

MEMORANDUM

To: Mayor and City Council

From: Community Development

Date: October 7, 2016

Subject: Perimeter Center Zoning Code Work Session #2

BACKGROUND

During the 2012-2013 Zoning and Land Development Code Rewrite process, City Council determined that the unique characteristics and needs of the Perimeter Center area required additional study through an independent process. The current study was initiated to review the area and establish PCID appropriate zoning districts that would ensure continued high quality development as the area transitions from a suburban commercial district into a more contemporary, urban center. As a function of this study, the city is considering development tools in the form of new zoning districts and an overlay district that will provide the proper regulations and incentives for future redevelopment. The draft documents propose the most intensive development in the PC-1 district located at the core of the PCID area, with less intensive development spreading through the PC-2, 3, and 4 districts. The proposed changes consider the most likely and appropriate areas for development and address protecting surrounding single family residential neighborhoods.

This work session, the third presentation of the study to the City Council, is to determine if the proposed development policies and scenarios in the draft documents are consistent with Council's vision for the area. In an effort to bring closure to the study which was initiated in late 2014, by the end of this year, staff has advertised the zoning changes for consideration by the Planning Commission on October 20th. Comments from the last work session and other significant revisions have been summarized in the tables below.

Chapter 27 – Perimeter Center Overlay	
Comment	Action
Reclassify Westside Connector. P. 7	Changed from Secondary Rd. to Minor Parkway
Question on Shared-use Paths on Figure 27-98-2. P. 7	Shared Paths removed. Covered on other city plans/maps with more comprehensive information
Sustainable Requirements too stringent.	Sustainable requirements moved to Districts (P. 18). Now apply only to rezoned property. Bike parking moved to parking section of Zoning Ordinance.
Concern that typical unbuffered dedicated bike lanes are allowed. (P. 11, 14, 17, 22)	#4 in Overlay for Frontage Options changed to make a general statement regarding different bike facility options at the discretion of the Public Works Director.
Question re: Allowable turn lanes and parking lanes. (P. 10,13, 16, 21)	Typical wording was amended in Tables of Street Requirements to allow parking lanes and turn lanes at discretion of Public Works Director.

Staff: Parking deck design.	Inserted requirement for aesthetic design of decks next to highways. Already addressed other road. P. 30
Staff: (c) Streets & Blocks. Applicability.	Inserted language to address “new” development. Aligned “redevelopment” and parking lots to a 50% standard. P. 24

Chapter 27 – Perimeter Center Districts	
Section	Action
Question regarding Food Truck parking requirements. P. 17	Note added to explain there are no parking requirements for ‘temporary’ locations.
Question re: “attached” houses and rentals. P. 17	Note [1] added to require a SLUP if >10% of units are rentals.
Question about age restricted rentals vs. owner occupied. P. 17	Modified to require a SLUP for age restricted multi-unit rentals.
Question re: DRI’s. P. 16	(f)(2) on page 16 addresses major modifications to an existing DRI and any new application (for DRI) require a SLUP.
Request to make the residential buffer in PC-2 and PC-3 consistent. (P. 13, 14)	Page 13, 14 include consistent buffer, height restrictions. Page 23 also includes a reference to this buffer. <100’ max. 3 stories; <500’ max 5 stories. Reference to buffer included in ‘e’ on P. 23. Will revise to only apply to external boundary.
Bicycle Parking. (Chap. 27 amend)	Bike parking moved to Zoning Ordinance and very minimal requirements for parking provided for basic uses where applicable. Parking reductions still allowed for providing bike parking above and beyond bike space requirements.
Applicability.	Districts apply only once rezoned to PC district. Other applicability sections per activity; <u>Sustainability, Building Types, Open Space.</u>
Bike repair in Sustainability. (P. 18, 19)	Qualified requirement by inserting “basic” tools for minor repairs (flats, etc.)
Staff: Townhome façade offsets.	Inserted an off-set requirement for adjacent units. P. 30 Districts
Staff: PC-1 District. Site Coverage max. impervious coverage	Increased from 80 to 85%, urban core. P. 12
Staff: Sustainability “applicability” provision.	Old applicability of points based on a percentage of improvement or expansion have been removed. Sustainability now applies only to the complete replacement of an existing building or a new building. Also, by being in the Districts, properties must ‘rezone’ for it to apply. P. 18
Staff: Surface parking min. setback.	Reduced from 10’ to 5’. Buffers if required are not affected. P. 22

Staff: Accessory Buildings.	Inserted "k" on P. 24 to address siting of such buildings.
Staff: Side and Rear Setbacks.	Previous drafts did not address side and rear setbacks. Provisions were added to the building regulation tables starting on P. 26.
Staff: Townhome parking/garages.	Clarification was added addressing front and rear loaded parking from an internal driveway. P. 30

PUBLIC OUTREACH

- Two Sounding Board Meetings
- DHA Meeting
- City Council Presentation
- City Council Work Sessions (2)
- Two Public Meetings
- Stakeholder Meetings

RECOMMENDATION

The three sets of zoning amendments have been advertised for consideration by the Planning Commission on October 20, 2016 with the goal of formally completing the process by the end of 2016.

Staff recommends initiation of the code adoption process.

DRAFT



Perimeter Center OVERLAY

City of Dunwoody, Georgia



DRAFT

SEPTEMBER 20, 2016

Dunwoody 
Smart people – Smart city 

City of Dunwoody

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Dunwoody GA 30346

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CODAMETRICS**duncan** associates

27-98 Perimeter Center Overlay

SECTIONS

- (a) Overlay Generally
- (b) Street Types
- (c) Streets & Blocks
- (d) General Building Design Criteria

(a) Overlay Generally

(1) Intent. The regulations of the Perimeter Center Overlay are intended to recognize and foster the transformation of Perimeter Center into a pedestrian-friendly, livable center that ensures long term economic, social, and environmental sustainability.

(2) Purpose. The Perimeter Center Overlay has been created to further the following public purposes.

- a. Plan Compatibility.** To guide new development and redevelopment as defined by the **City of Dunwoody Comprehensive Plan** (adopted 2010) and as amended.
- b. Perimeter Community Improvement Districts (PCIDs).** To provide implementation guidance for the general vision defined in **Perimeter @ The Center - Future Focus, 2011 - A Ten Year LCI Update**, dated November 2011, as amended (past, current, and future), and adopted by City Council.

(3) Applicability. The regulations within this section, 27-98 Perimeter Center Overlay, apply to all buildings, structures, land and uses within the Perimeter Center Overlay, as illustrated on the City's zoning map and in Figure 27-98-1 Perimeter Center Overlay Map.

(4) Existing Land Development Regulations. Refer to chapter 16 Land Development Regulations for additional requirements. When there is a conflict between the following regulations and chapter 16, the Perimeter Center Overlay regulations govern.

(5) Review and Approval Procedures. Unless otherwise expressly stated, all uses and development in Perimeter Center Overlay are subject to the applicable review and approval procedures of Article V, as well as any applicable procedures in chapter 16 of the municipal code (land development regulations).

(6) Site Plan Review. All applications for development permits within the Perimeter Center Overlay must be accompanied by a site plan, which is subject to review and approval in accordance with Article V, Division 10.

(7) Relief. In some instances, unique site conditions, existing building circumstances or other constraints related to the subject property may make strict compliance with the Perimeter Center Overlay regulations impossible or highly impractical. In such instances, an applicant may seek relief from applicable regulations. Any relief granted and conditions imposed

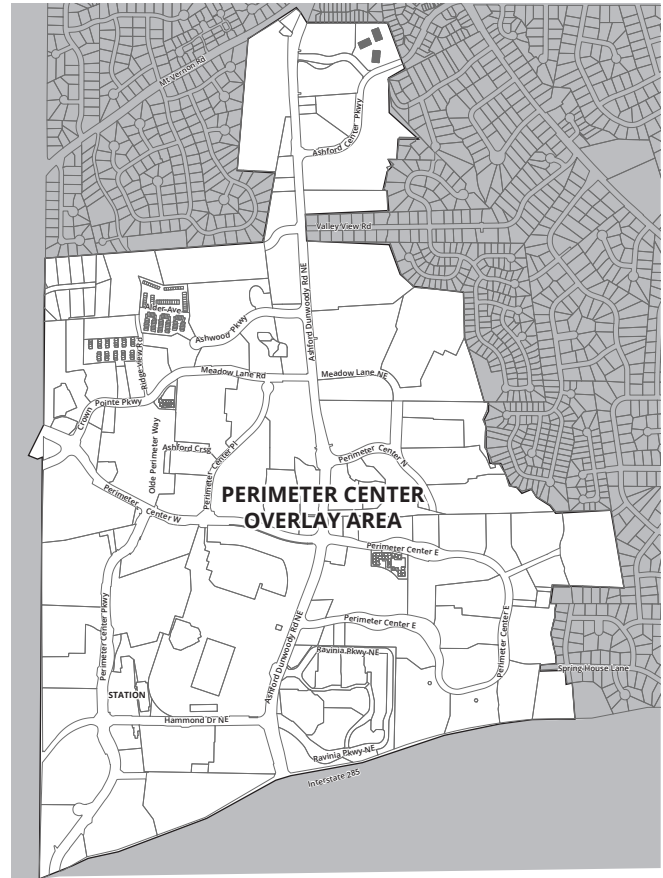


Figure 27-98-1 Perimeter Center Overlay Map

run with the land and are binding on the subject property owner and all future property owners.

a. Minor Exception—Administrative Relief. The community development director or public works director, as noted, is authorized to approve the following minor exceptions to strict compliance in accordance with the special administrative permit procedures of Article V, Division 7:

1. A change to any street or frontage option component width or depth (requires approval by the public works director);
2. A reduction in required landscape or yard depth by no more than 10 percent;
3. Establishment of a public access easement in lieu of right-of-way dedication for street type frontage options, per the public works director.
4. Retaining walls in street type frontages up to 5 feet in height when extreme grades prevent the use of shorter walls and landscaping is used to mitigate the visual impacts of the taller retaining wall at the sidewalk edge; and
5. Any other minor exception expressly authorized under the Perimeter Center Overlay regulations.

b. Major Exceptions—Special Land Use Permit. Major exceptions to strict compliance with the Perimeter Center Overlay regulations include any exception to strict compliance that is not expressly authorized as a minor exception under the provisions of subsection 27-98(a)(7). Major exceptions require review and approval in accordance with the special land use permit procedures of Article V, Division 3.

(b) Street Types

(1) Intent

Street Types are established for all of the existing and new streets in the Perimeter Center Overlay area. Frontage options are defined for the space between the building or parking and the curb per street type. Street Types and Frontage Options are intended to address the comfort, convenience, and safety of pedestrians and bicyclists; to develop a network of streets with identifiable character; and to provide an aesthetically pleasing edge to all development.

(2) Applicability

All new development and redevelopment within the Perimeter Center Overlay shall meet the Street Type frontage requirements under any one of the following circumstances:

- a. New Structure.** Development of a new principal structure on a lot or a portion of a lot.
- b. Redevelopment or Renovation.** Redevelopment or renovation of an existing structure or site that increases the gross building square footage by more than 50 percent.
- c. Parking Lots.** Redevelopment or revision to 50 percent or more of an existing parking lot or development of a new parking lot consisting of 15 or more spaces, not including resurfacing or repairing an existing layout.

(3) Street Types Map

Figure 27-98-2 Street Types Map defines street types in Perimeter Center for existing and new streets. Refer to 27-98(c) Streets & Blocks for more information about new streets and their designated locations.

Locations illustrated on the Street Types map are approximate. The requirements and locations of all streets shall be determined by and approved by the public works director during the Site Plan review process.

a. Street Types. The following Street Types are established for the existing streets and any new streets adjacent to and within the Perimeter Center Overlay. Refer to Figure 27-98-2 Street Types Map for the typologies of existing streets and proposed new streets.

1. **Major Parkway Street.** The Major Parkway Street Type is intended to create a parkway effect along the major transportation corridors within Perimeter Center through the use of wide landscape areas heavily planted with native or naturalized trees. Major Parkways require designated bicycle facilities. Refer to 27-98(b)(5) Major Parkway Street Type for requirements.
2. **Minor Parkway Street.** The Minor Parkway Street Type is intended to create a parkway effect along transportation corridors within Perimeter Center through the use of wide landscape areas heavily planted with trees. Minor Parkways require designated bicycle facilities. Refer to 27-98(b)(6) Minor Parkway Street Type for requirements.
3. **Primary Street.** The Primary Street Type is intended to establish a series of comfortably scaled streets with continuous building frontage and a limited number of drives interrupting the sidewalk. Primary Streets require a balance between bicyclists and pedestrians and automobiles by providing designated bicycle facilities and a robust pedestrian realm, and by prioritizing bicyclists and pedestrians for a more balanced transportation route. Refer to 27-98(b)(7) Primary Street Type for requirements.
4. **Secondary Street.** The Secondary Street Type is meant to establish a designated street for parking lot and structure access, while still providing a safe and comfortable pedestrian realm. Refer to 27-98(b)(8) Secondary Street Type for requirements.

b. Street Type Graphics. The graphics provided in this section illustrate the preferred configuration of each Street Type. By applying the standards defined, other configurations are permitted.

c. Redesignating Street Types. Existing street designations are set to match the existing character to the extent possible. New streets have been designated with the intent to provide an appropriate mix of Primary and Secondary Streets, establishing comfortable pedestrian streets while providing for parking access. Re-designation of existing streets may occur per the following:

1. **Process.** Parkways shall not be re-designated, except by a Text Amendment to this code. All other streets may be redesignated using the minor exception process.
2. **Entire Blocks.** When redesignating street types, the entire street between intersecting streets shall be redesignated.
3. **Primary Streets.** The intent is to maintain a similar amount of Primary Streets as established by the map. Primary Streets may be re-designated as Parkways or Secondary Streets only when a new or existing street, currently designated as Secondary, within or abutting the same parcel is redesignated as a Primary Street.
4. **Secondary Streets.** Existing Secondary Streets may be re-designated as Parkways or Primary Streets as long as vehicular access to parking for all surrounding sites has been addressed.

d. Shared-Use Paths. Trail locations are required per Figure 27-98-2 Street Types Map and the department of public works director. Refer to the City's current comprehensive plan or other City-approved cycle/pedestrian plan and coordinate with public works director for locations for other trails.

1. **Additional Easement.** Additional easement or right-of-way space may be required when trails do not follow a street or are along Primary or Secondary Streets.
2. **Trail Width.** Trails in locations other than within Street Type frontages shall be a minimum of 12 feet in width with a minimum of 10 feet of landscape on both sides.

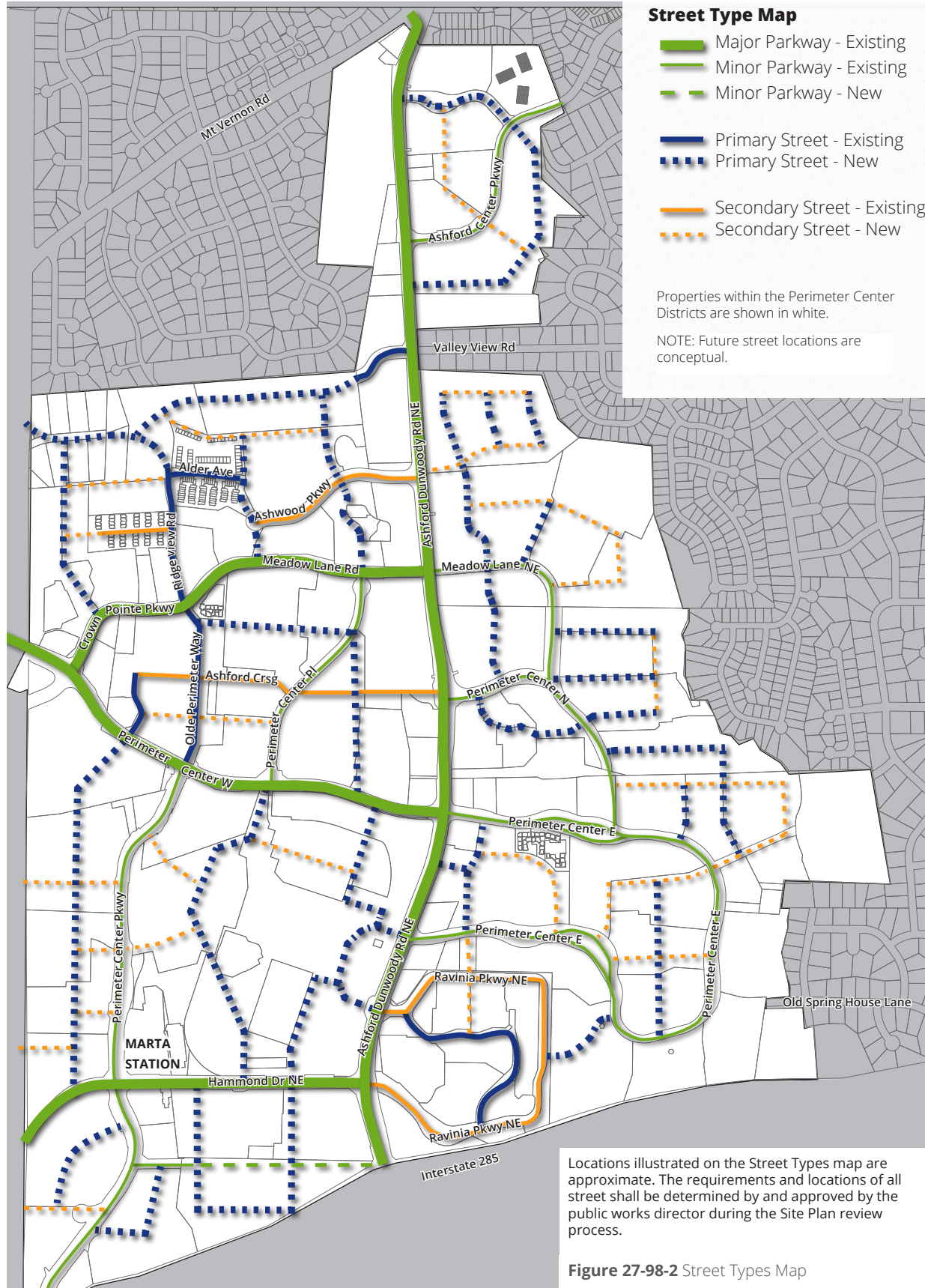


Figure 27-98-2 Street Types Map

3. **Trail Materials.** Trails shall be constructed primarily of concrete. Brick pavers are acceptable as accent material.
4. **Trees.** One canopy tree is required per every 60 linear feet of trail.
5. **Ground Plane Vegetation.** Trail easements are to be landscaped with a combination of planting beds that may include shrubs, ornamental grasses, ground cover, vines, or perennials; and seeded, plugged, or sodded grass. Nonliving materials, such as pine straw or mulch are permitted for up to 50 percent of the buffer. Additional mulch area may be approved by the community development director when significant tree cover exists.
6. **Existing Vegetation.** The community development director may credit buffer and screen landscape when existing vegetation and tree plantings meet the same requirements.

(4) Streetscape Furnishings & Hardscape Design

In addition to the Street Type frontage requirements that include minimum tree plantings and walk/trails, a street furnishings and hardscape design is required along all new and existing street frontages per the following.

- a. **Definition.** The streetscape furnishings and hardscape design shall include the frontage area as defined by the Street Types plus any hardscaped building setback area less than 25 feet deep from the frontage edge.
- b. **Design Submittal.** A consistent design shall be submitted for approval with Site Plan Review for all streets within and adjacent to the development.
- c. **Standard Specifications.** The design shall meet any standards defined by the City for sidewalk, curb, access, lighting, landscape, and other applicable construction details. Refer to Chapter 16, Land Development Regulations.
- d. **Perimeter Community Improvements Districts (PCID) Public Space Standards.** Refer to the current PCID public space standards document for recommended streetscape component and material specifications.
- e. **Submittal Requirements.** At a minimum, the design submittal shall include the following:

1. **Trees.** Trees meeting the minimum requirements of 27-98(b) Street Types shall be included in the design, with details related to tree pits, tree grates, and tree planting to meet the landscape installation requirements of 27-364 Landscape.
2. **Pavement Design.** Paving materials and pattern is required for each sidewalk and any other hardscape proposed. Pavement design shall address all walks or trails, including the minimum sidewalk or trail widths required by the frontage.
3. **Street Furnishings.** Benches and/or seatwalls, planters, planter fences, tree grates, and trash receptacles shall be specified and quantities and locations listed for each street. For each block face, a minimum of two benches, one trash receptacle, and one recycling receptacle are required.
4. **Bus Stop Facilities.** Coordinate with the public works director for the location of bus stops and the required facilities. Facilities may include benches, pavement, shelters, signs, and other improvements.
5. **Landscape Design.** Ground plane vegetation shall be designated for any landscape bed areas, planter areas, and open tree wells, including shrubs, perennials, annuals, and grasses.
6. **Lighting.** Pedestrian and vehicular lighting shall be specified with locations and quantities noted. All lighting shall meet any requirements of the public works director. Cut sheets and samples shall be submitted upon request.
7. **Stormwater Facilities.** Any stormwater facilities proposed for the right-of-way shall be included in the streetscape design. Facilities such as stormwater planters or parkway swales may be included. Maintenance responsibilities and processes shall be included.
8. **Identity Elements.** Any other elements designed to establish the identity of each street, such as banners mounted on light poles, pavement markers, artwork, or wayfinding signage, shall be included in the design submittal.

9. **Streetscape Maintenance.** Property owner is responsible for regular maintenance and emptying of trash and recycling receptacles, sweeping of sidewalks and seating areas, and tree maintenance. The City may request additional maintenance procedures.
10. **Streetscape Design Continuation.** The approved streetscape design for each street may be utilized by the City for the extension of any street outside the development to provide continuity.

(5) Major Parkway Street Type

The Major Parkway Street Type applies to existing streets per Figure 27-98-2 Street Types Map. Refer to Figure 27-98-3. Table of Major Parkway Street Type Requirements and Figure 27-98-4 Existing Major Parkway Street Section.

- a. New Major Parkway Streets.** New Major Parkway streets are not permitted, unless otherwise required by the public works director. This street type is applicable only to existing streets per Figure 27-98-2 Street Types Map.
- b. Frontage Options.** Frontages address the pedestrian and bicycle realm as well as street & parking buffers and public space options. Parcels that are adjacent to a Major Parkway as designated on the Figure 27-98-2 Street Types Map, shall apply one or more of the Major Parkway Street Frontage Options along the entire

length of the lot line abutting the Major Parkway. Refer to Figure 27-98-5 Table of Major Parkway Street Type Frontage Options.

- c. Dedication.** When the sidewalk is fully or partially located on private property, right-of-way dedication is required, so that the sidewalk is fully located in the right-of-way. The public works director may approve an easement in lieu of right-of-way dedication based on the geometry of the site, topography, or other site-specific considerations.
- d. Curb Movement.** The existing street curb may be required to move, based on the selection of the appropriate context-sensitive bicycle facility, per public works director.

FIGURE 27-98-3: TABLE OF MAJOR PARKWAY STREET TYPE REQUIREMENTS

Location	O-I, OCR, O-D, C-1, CR-1, PC-1, PC-2, and PC-3 Districts
Right-of-Way Width	Varies; shall be wide enough to include curb to curb as well as frontage options.
Travel Lanes	4 to 8 lanes, 2 way
Lane Width	10' to 12' per public works director
Allowable Turn Lanes	Per public works director
Parking Lanes	Permitted only with public works director approval; then parallel only, 8' wide
Pavement Width	Existing, varies. Recommended median when crossing exceeds 32 feet in width.
Frontage Options	Refer to Figure 27-98-5 Table of Major Parkway Street Type Frontage Options.

**Figure 27-98-4** Existing Major Parkway Street Section

FIGURE 27-98-5: TABLE OF MAJOR PARKWAY FRONTAGE OPTIONS

	PARKING EDGE refer to FIGURE 27-98-6	ACTIVE EDGE refer to FIGURE 27-98-7	BUILDING EDGE refer to FIGURE 27-98-8
1 Minimum Depth of Frontage	The minimum depth of the frontage shall be 34 feet measured from existing curb. Minimum depths of components shall be met; additional areas shall be added to any component to meet the minimum 34 feet.		
2 Street Buffer	<p>Minimum Depth: Street buffer depth may be varied, at a minimum of 8 feet with an average depth of 12 feet.</p> <p>Tree Plantings: Informal groupings of trees shall be spaced intermittently along the parkway with the goal of shading the parkway sidewalk at maturity. Trees shall be spaced between 12 feet and 40 feet on center. The buffer shall consist of at least 5 canopy trees per every 100 feet of buffer.</p> <p>Grade: Grade changes may be accommodated within the street buffer with slopes no greater than 1 to 4 ratio of rise to run. Retaining walls may be incorporated to meet these grades, but shall be no taller than 3 feet.</p>		
3 Sidewalk	A minimum 8 foot wide sidewalk is required and may meander between trees.		
4 Bicycle Facilities	A separated, dedicated bicycle facility is required consisting of an on-street buffered bicycle lane, dedicated cycle track, or shared-use path (a minimum width of 12') per public works director.		
5 Yard Treatment	Minimum Depth: minimum 6 foot deep landscape or patio area is required.		
	Parking Screen is required. Refer to Figure 27-229 Landscape Screen Requirements.	Patio shall be clearly differentiated from sidewalk. Fencing may be required for alcohol service per city ordinance. For PC Districts, the portion of the building facade adjacent to the patio shall meet the primary street facade requirements.	no additional requirements
6 Building & Parking Location	Refer to Article II. Zoning Districts for setback line or 27-105 Building Types for Parkway Frontage requirements for allowable widths of parking along the frontage and required building facades along the frontage.		

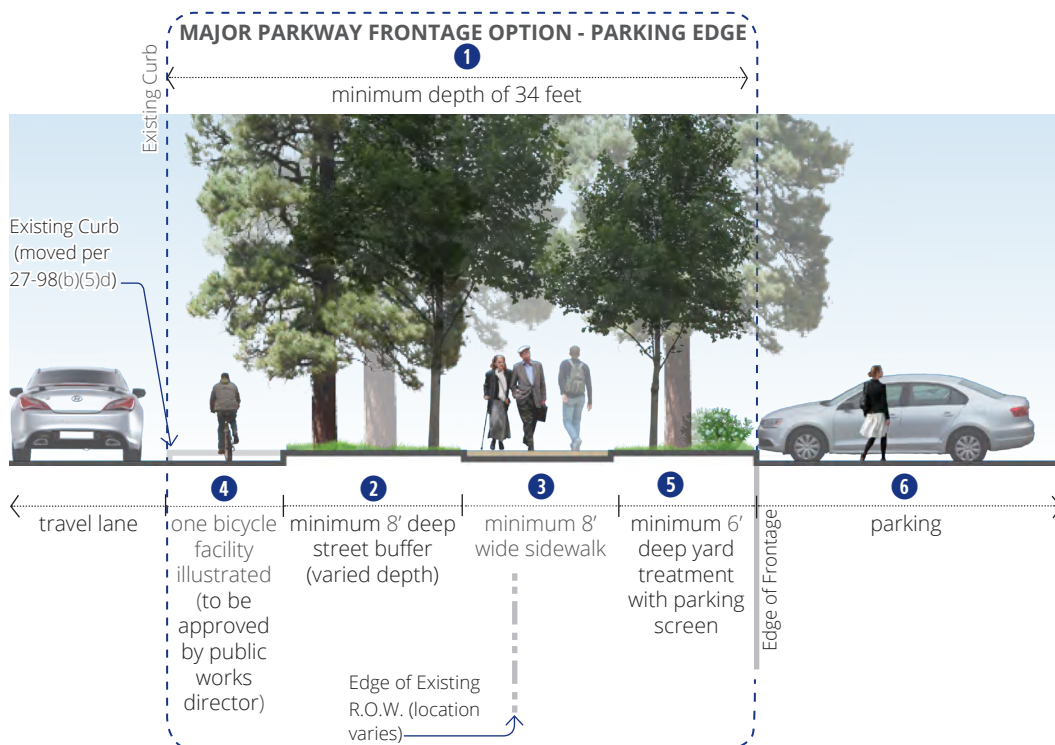


Figure 27-98-6 Major Parkway Frontage Option - Parking Edge

27-98 Perimeter Center Overlay

Street Types

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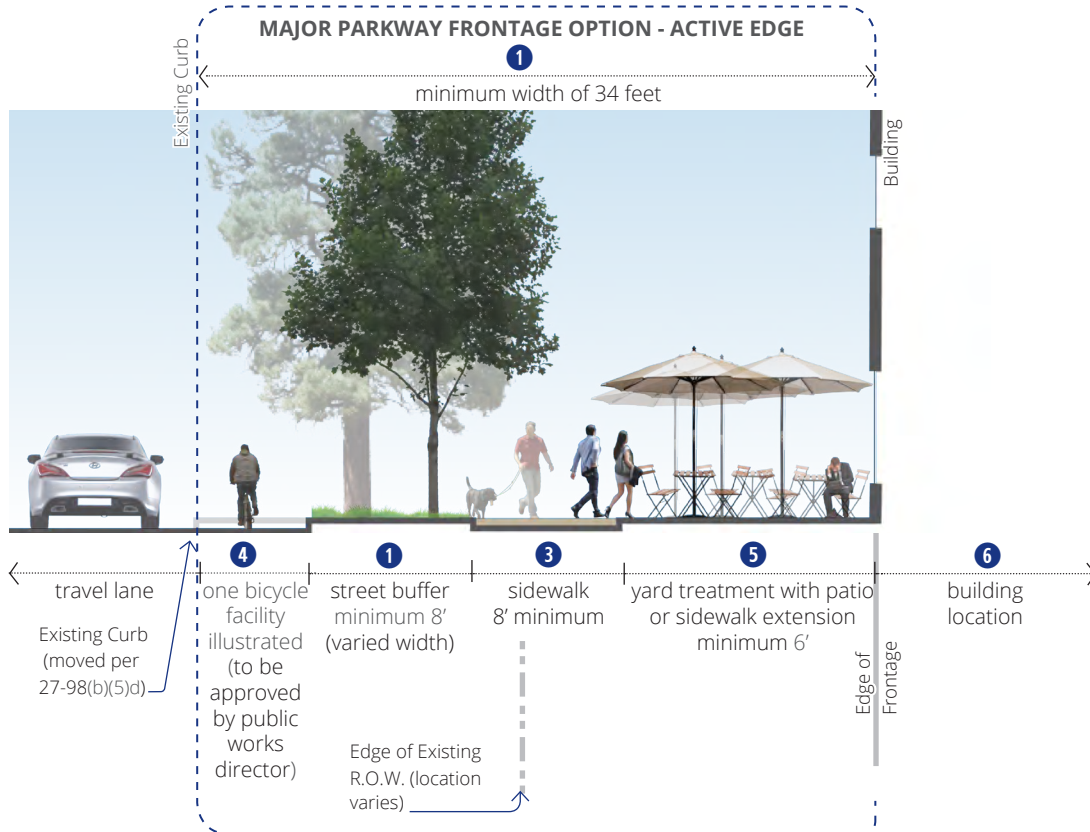


Figure 27-98-7 Major Parkway Frontage Option - Active Edge

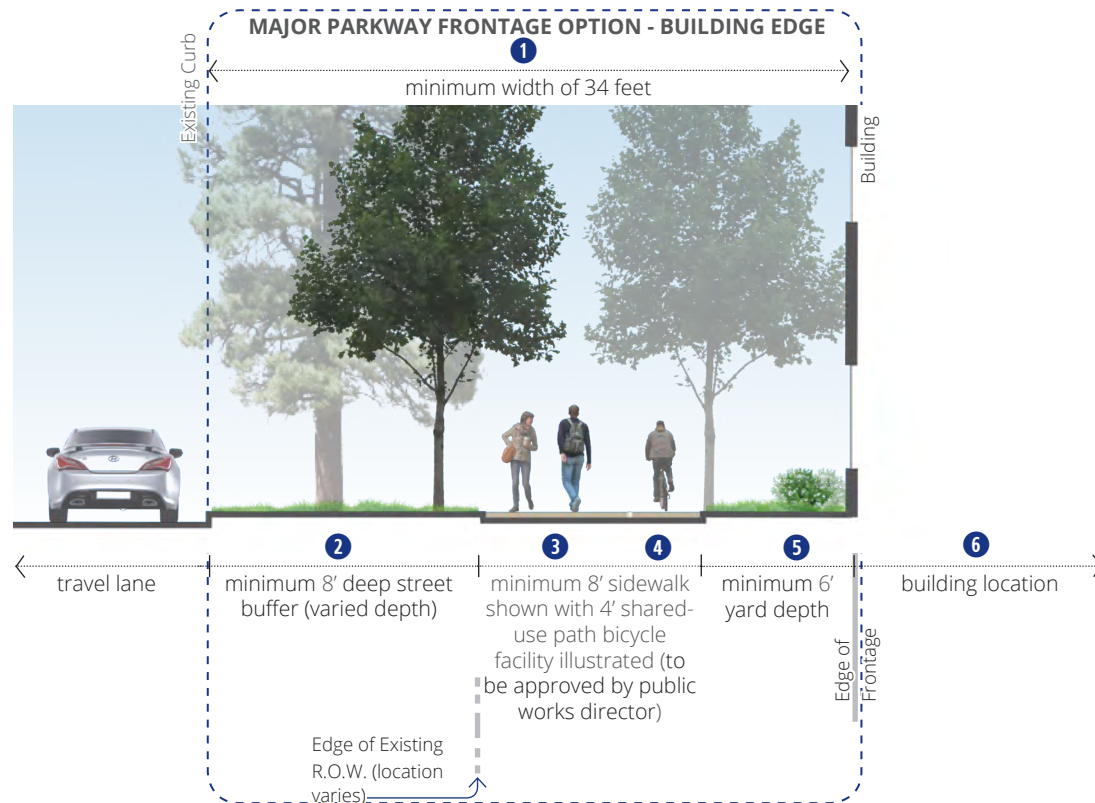


Figure 27-98-8 Major Parkway Frontage Option - Building Edge

(6) Minor Parkway Street Type

The Minor Parkway Street Type applies to existing streets per Figure 27-98-2 Street Types Map and is available for new streets per public works director. Refer to Figure 27-98-9. Table of Minor Parkway Street Type Requirements and Figure 27-98-10 Minor Parkway Street Section - Existing and New.

- a. New Minor Parkway Streets.** New Minor Parkway streets are not permitted, unless a minor exception is approved by the public works director.
- b. Frontage Options.** Frontages address the pedestrian and bicycle realm as well as street & parking buffers and public space options. Parcels that are adjacent to a Minor Parkway as designated on Figure 27-98-2 Street Types Map shall apply one or more of the Minor Parkway Street Frontage Options along the entire length

of the lot line abutting the Minor Parkway. Refer to Figure 27-98-11 Table of Minor Parkway Street Type Frontage Options.

- c. Dedication.** When the sidewalk is fully or partially located on private property, right-of-way dedication is required, so that the sidewalk is fully located in the right-of-way. The public works director may approve an easement in lieu of right-of-way dedication based on the geometry of the site, topography, or other site-specific considerations.
- d. Curb Movement.** The existing street curb may be required to move, based on the selection of the appropriate, context sensitive bicycle facility, per the public works director.

FIGURE 27-98-9: TABLE OF MINOR PARKWAY STREET TYPE REQUIREMENTS

Location	O-I, OCR, O-D, C-1, CR-1, PC-1, PC-2, and PC-3 Districts
Right-of-Way Width	Existing varies; 100' to 120' per public works director
Travel Lanes	2 to 4 lanes, 2 way
Lane Width	10' to 12' per public works director
Allowable Turn Lanes	Per public works director
Parking Lanes	Permitted only with public works director approval; Parallel only, 8' wide
Pavement Width	Existing, varies. Recommended median when crossing exceeds 32 feet in width.
Frontage Options	Refer to Figure 27-98-11 Table of Minor Parkway Street Type Frontage Options



Figure 27-98-10 Minor Parkway Street Section - Existing and New

FIGURE 27-98-11: TABLE OF MINOR PARKWAY FRONTAGE OPTIONS

	PARKING EDGE refer to FIGURE 27-98-12	ACTIVE EDGE refer to FIGURE 27-98-13	BUILDING EDGE refer to FIGURE 27-98-14
1 Minimum Depth of Frontage	The minimum depth of the frontage shall be 34 feet measured from existing curb. Minimum depths of components shall be met; additional areas shall be added to any component to meet the minimum 34 feet.		
2 Street Buffer	<p>Minimum Depth: Street buffer depth may be varied, at a minimum of 6 feet wide with average of 10 feet.</p> <p>Tree Plantings: Informal groupings of trees shall be spaced intermittently along the parkway, with the goal of shading the parkway sidewalk at maturity. Trees shall be spaced between 12 feet and 40 feet on center. The buffer shall consist of at least 5 canopy trees per every 100 feet of buffer.</p> <p>Grade: Grade changes may be accommodated within the street buffer with slopes no greater than 1 to 4 ratio of rise to run. Retaining walls may be incorporated to meet these grades, but shall be no taller than 3 feet.</p>		
3 Sidewalk	A minimum 6 foot wide sidewalk is required and may meander between trees.		
4 Bicycle Facilities	A separated, dedicated bicycle facility is required consisting of an on-street buffered bicycle lane, dedicated cycle track, or shared-use path (a minimum width of 12') per public works director.		
5 Yard Treatment	Minimum Depth: minimum 6 foot deep landscape or patio area is required.		
	Parking Screen is required. Refer to Figure 27-229 Landscape Screen Requirements.	Patio shall be clearly differentiated from sidewalk. Fencing may be required for alcohol service per city ordinance. For PC Districts, the portion of the building facade adjacent to the patio shall meet the primary street facade requirements.	no additional requirements
6 Building & Parking Location	Refer to Article II. Zoning Districts for setback line or 27-105 Building Types for Parkway Frontage requirements for allowable widths of parking along the frontage and required building facades along the frontage.		

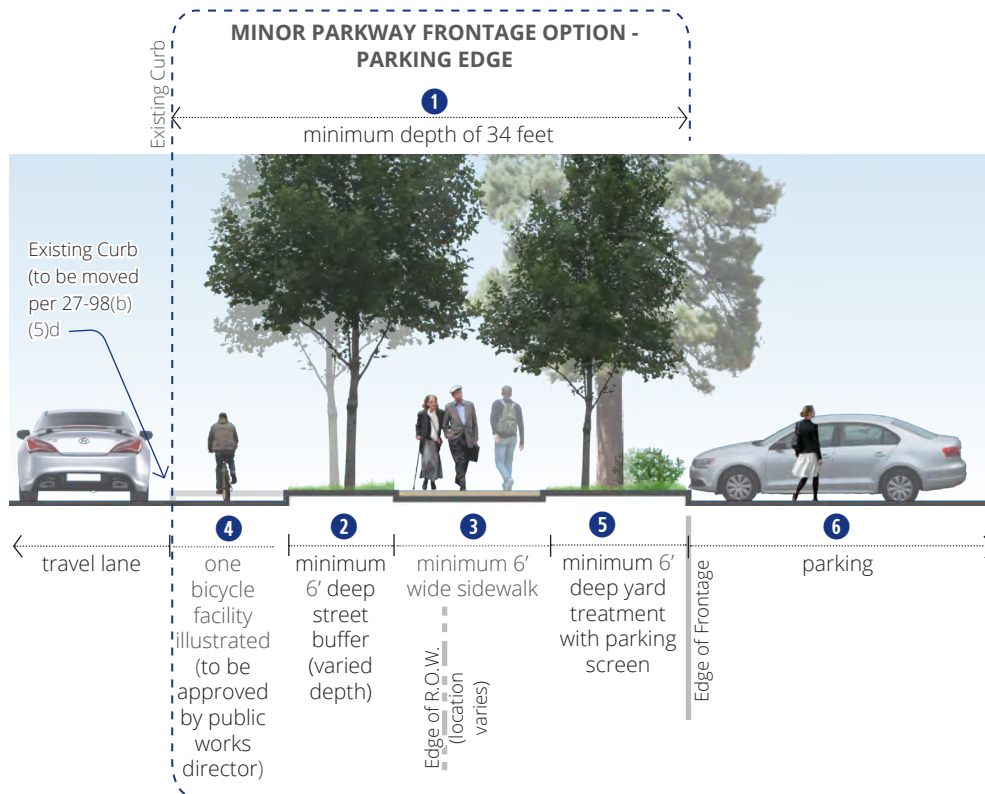


Figure 27-98-12 Minor Parkway Frontage Option - Parking Edge

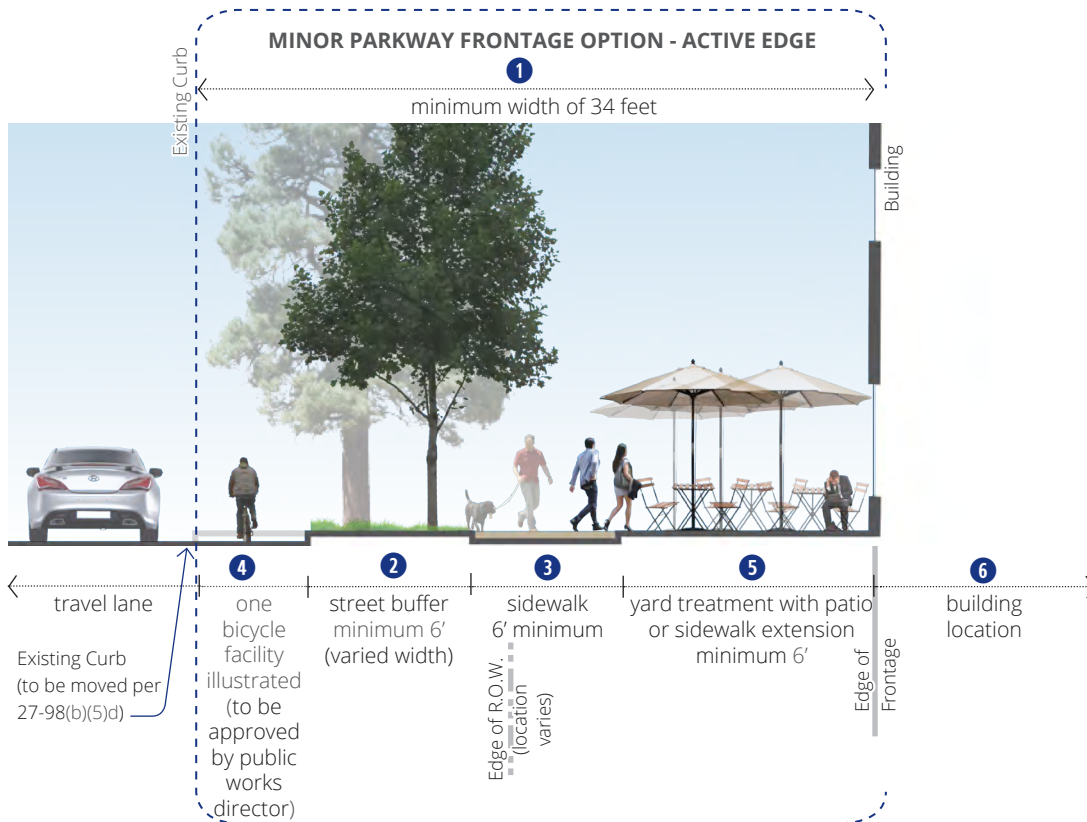


Figure 27-98-13 Minor Parkway Frontage Option - Active Edge

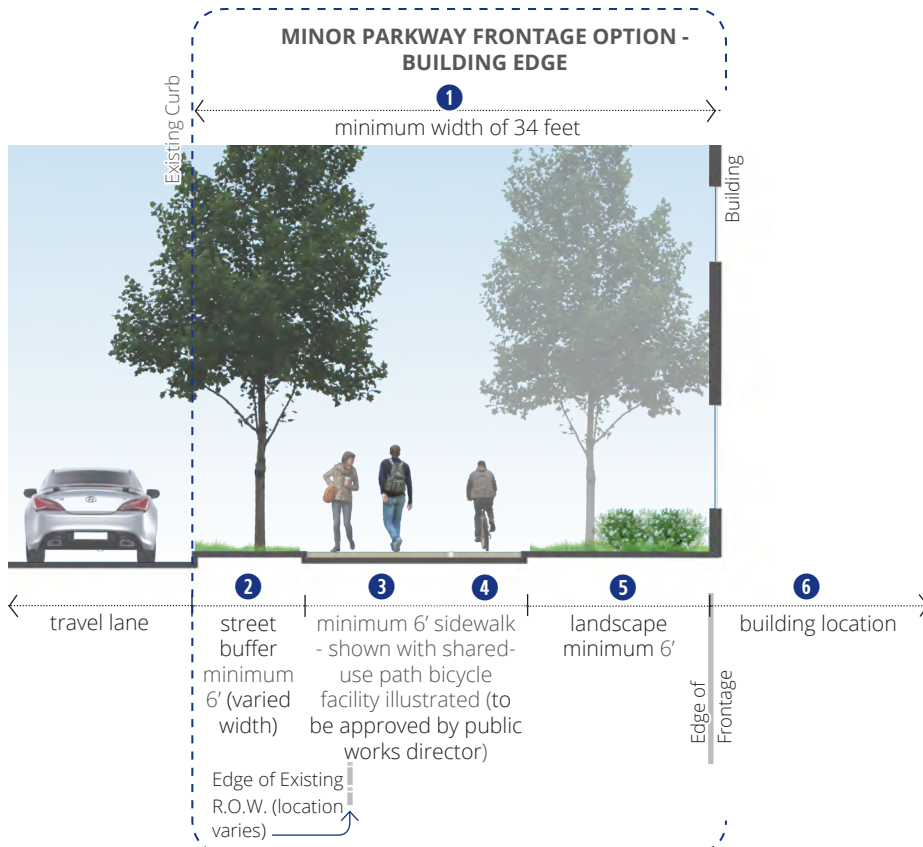


Figure 27-98-14 Minor Parkway Frontage Option - Building Edge

(7) Primary Street Type

The Primary Street Type applies to new and existing streets per Figure 27-98-2 Street Types Map.

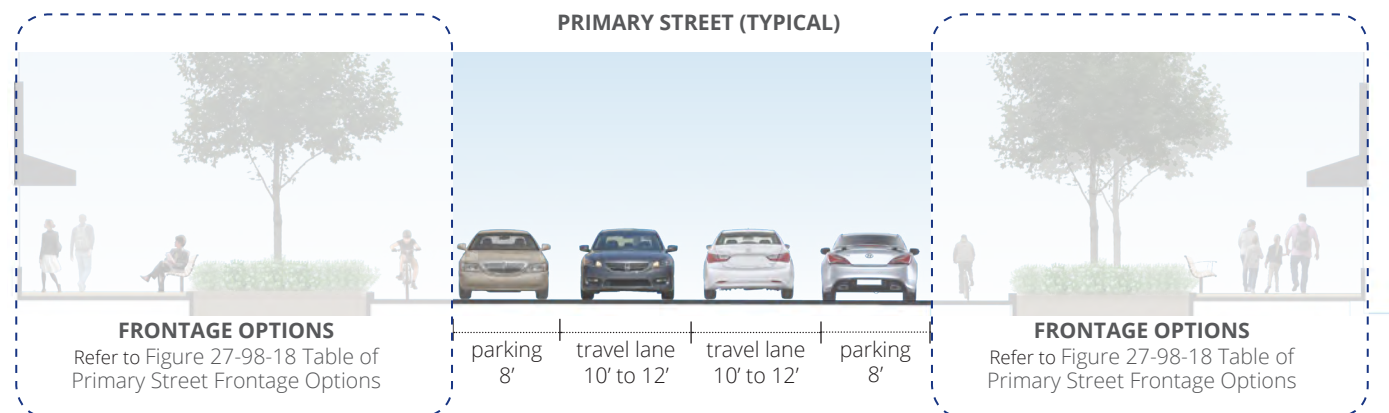
- a. New Primary Streets.** Refer to Figure 27-98-15 Table of Primary Street Type Requirements for primary street type requirements for new streets, Figure 27-98-16 Primary Street Type Section: Typical illustrates the typical street section. A narrow version is available for limited application (refer to Figure 27-98-17 Primary Street Type Section: Narrow).
- b. Frontage Options.** Frontages address the pedestrian and bicycle realm as well as street & parking buffers and public space options. Parcels that are adjacent to a Primary Street as

designated on Figure 27-98-2 Street Types Map shall apply one or more of the Primary Street Frontage Options along the entire length of the lot line abutting the Minor Parkway. Refer to Figure 27-98-18 Table of Primary Street Frontage Options.

- c. Dedication.** When the sidewalk is fully or partially located on private property, right-of-way dedication is required, so that the sidewalk is fully located in the right-of-way. The public works director may approve an easement in lieu of right-of-way dedication based on the geometry of the site, topography, or other site-specific considerations.
- d. Curb Movement.** The existing street curb may be required to move, based on the selection of the appropriate, context sensitive bicycle facility, per the public works director.

FIGURE 27-98-15: TABLE OF PRIMARY STREET TYPE REQUIREMENTS

CONFIGURATION	TYPICAL refer to Figure 27-98-16	LIMITED NARROW refer to Figure 27-98-17
Location	O-I, OCR, O-D, C-1, CR-1, NS, PC-1, PC-2, PC-3, and PC-4 Districts	RM-HD, PC-4 Districts; use requires approval of public works director/community development director
Right-of-Way Width	66' to 100' per public works director	60' to 75' per public works director
Travel Lanes	2 lanes, 2 way	1 yield lane
Lane Width	10' to 12' per public works director	16' to 18' per public works director
Allowable Turn Lanes	Per public works director	Per public works director
Parking Lanes	Per public works director	Per public works director
Pavement Width	34' to 40'	32'
Frontage Options	Refer to Figure 27-98-18 Table of Primary Street Frontage Options	Yard Edge Frontage Option only

**Figure 27-98-16** Primary Street Type Section: Typical

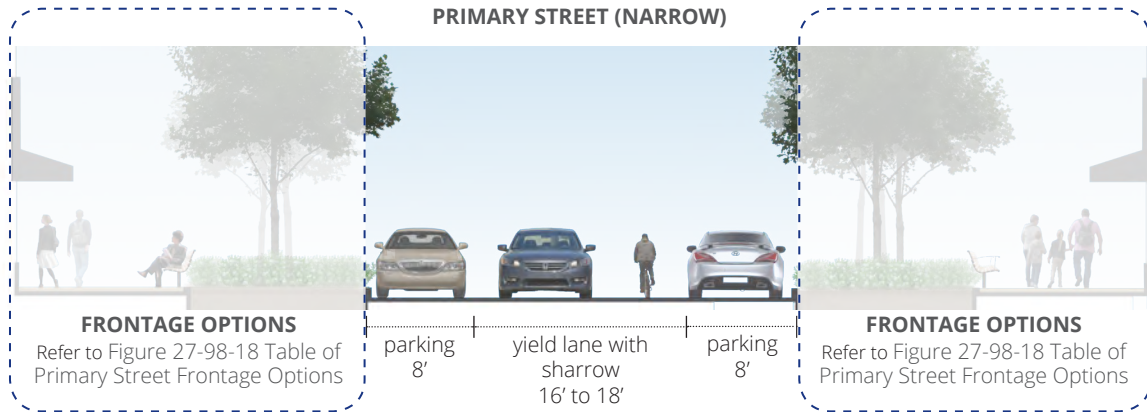


Figure 27-98-17 Primary Street Type Section: Narrow

FIGURE 27-98-18: TABLE OF PRIMARY STREET FRONTAGE OPTIONS

	BUILDING EDGE refer to FIGURE 27-98-19	PARKING EDGE refer to FIGURE 27-98-20	YARD EDGE refer to FIGURE 27-98-21	COURTYARD/ PLAZA A OR B refer to FIGURE 27-98-22 OR FIGURE 27-98-23	DROP-OFF DRIVE EDGE refer to FIGURE 27-98-24
1 Minimum Depth of Frontage	24 feet	30 feet	16 feet	20 feet	20 feet
	The minimum depth of the frontage shall be measured from existing curb. Minimum depths of components shall be met; additional areas shall be added to any component to meet the minimum overall depth defined above.				
2 Street Buffer	Minimum Depth: Street buffer depth shall be a minimum of 6 feet wide.				
	Tree Planting: Canopy trees spaced minimum 35' on center in planters or wells, either raised or at grade, with access through planters to street, minimum every 60'.		Minimum of one tree per 30 linear feet of frontage, spaced equally or informally		
	Grade changes may be accommodated within the street buffer with slopes no greater than 1 ft of rise in 3 ft run. Retaining walls may be incorporated to meet these grades, but shall be no taller than 18 inches. Steps may also be incorporated.				
3 Sidewalk	Minimum 10' wide clear paved sidewalk with furnishings		Minimum 6' wide sidewalk		
4 Bicycle Facilities	A separated, dedicated bicycle facility is required consisting of an on-street buffered bicycle lane, dedicated cycle track, or shared-use path (a minimum width of 12') per public works director.				
5 Yard Treatment	Build-to Zone is directly adjacent to sidewalk	Minimum 5' landscape area is required to screen parking. Minimum 24" wide shrubs required 30" o.c.	yard depth determined PC Districts: by build-to zone of Building Type. Zoning District: by building setback	30' to 60' deep courtyard or plaza permitted with maximum of 30% of frontage in PC-2 and PC-3, maximum of 10% of frontage in PC-1	minimum 5' buffer; maximum 30' deep drop-off drive is permitted. maximum of 20% of frontage
6 Building & Parking Location	Article II. Zoning Districts for setback line or 27-105 Building Types for Primary Street Frontage requirements for allowable widths of parking along the frontage and required building facades along the frontage.				

27-98 Perimeter Center Overlay

Street Types

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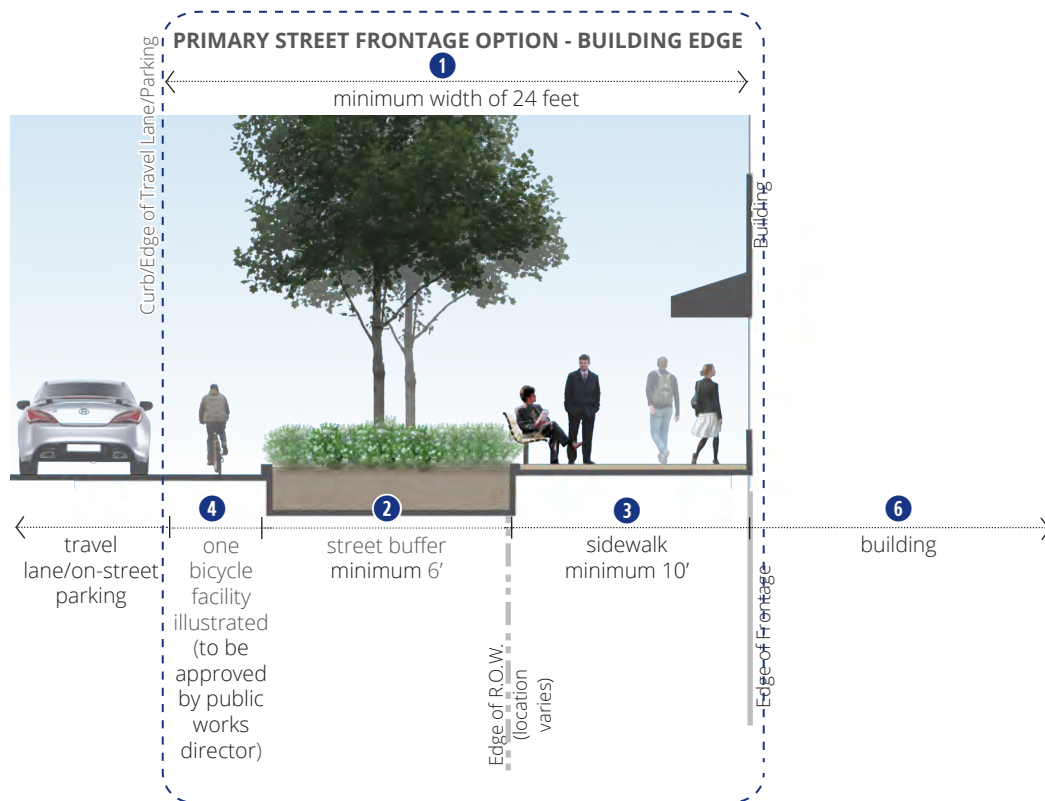


Figure 27-98-19 Primary Street Frontage Option - Building Edge

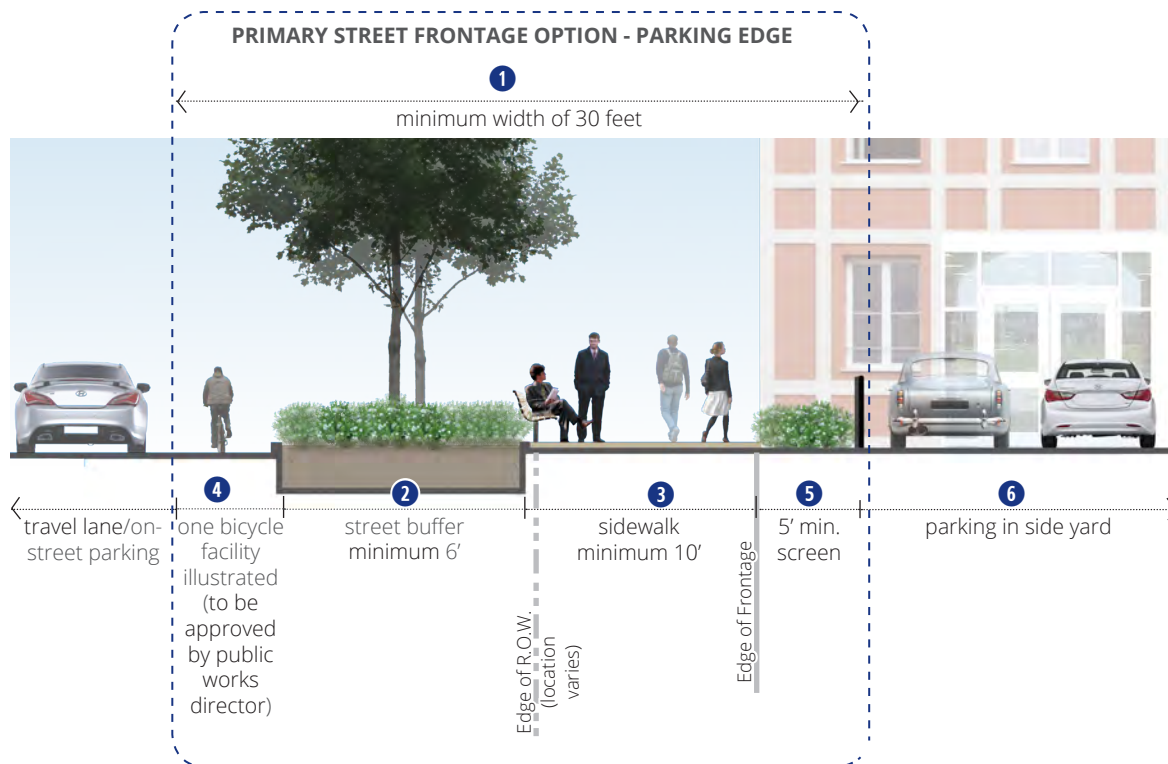


Figure 27-98-20 Primary Street Frontage Option - Parking Edge

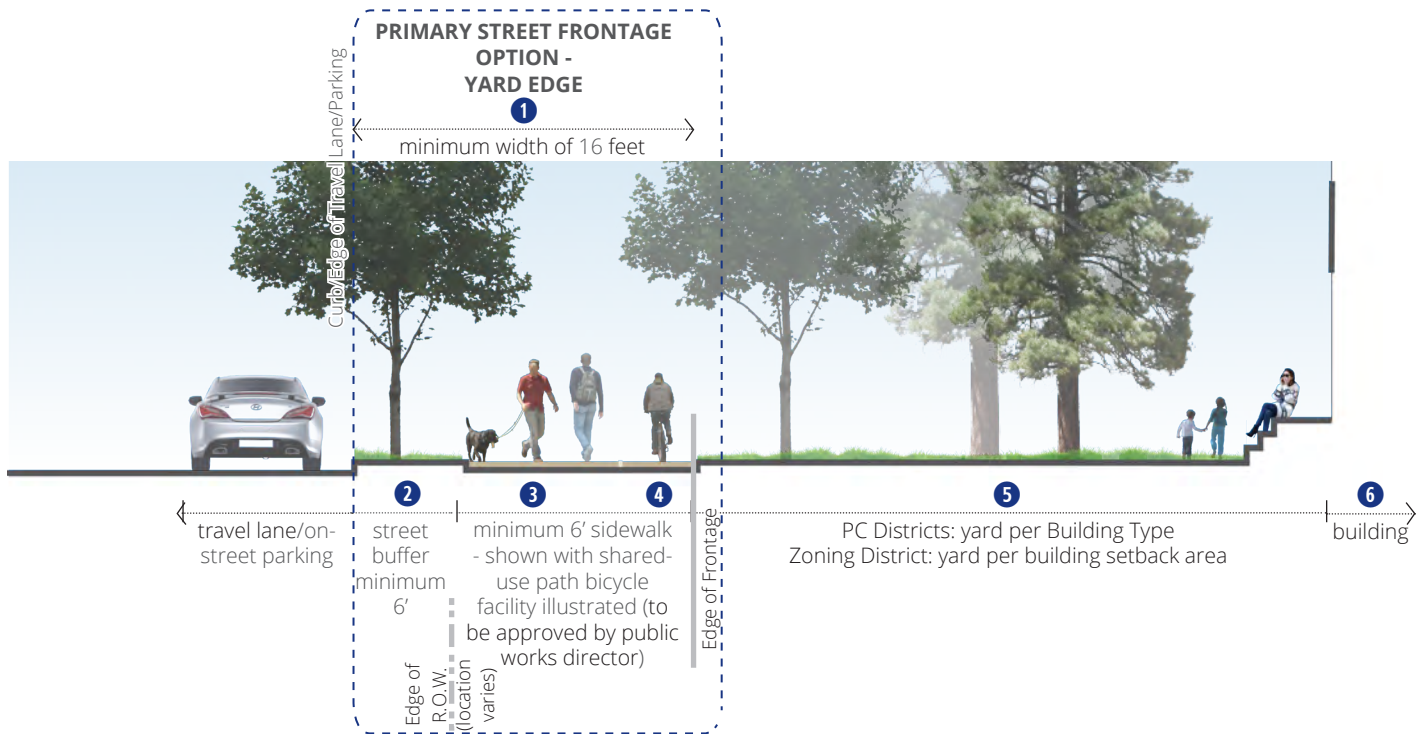


Figure 27-98-21 Primary Street Frontage Option - Yard Edge

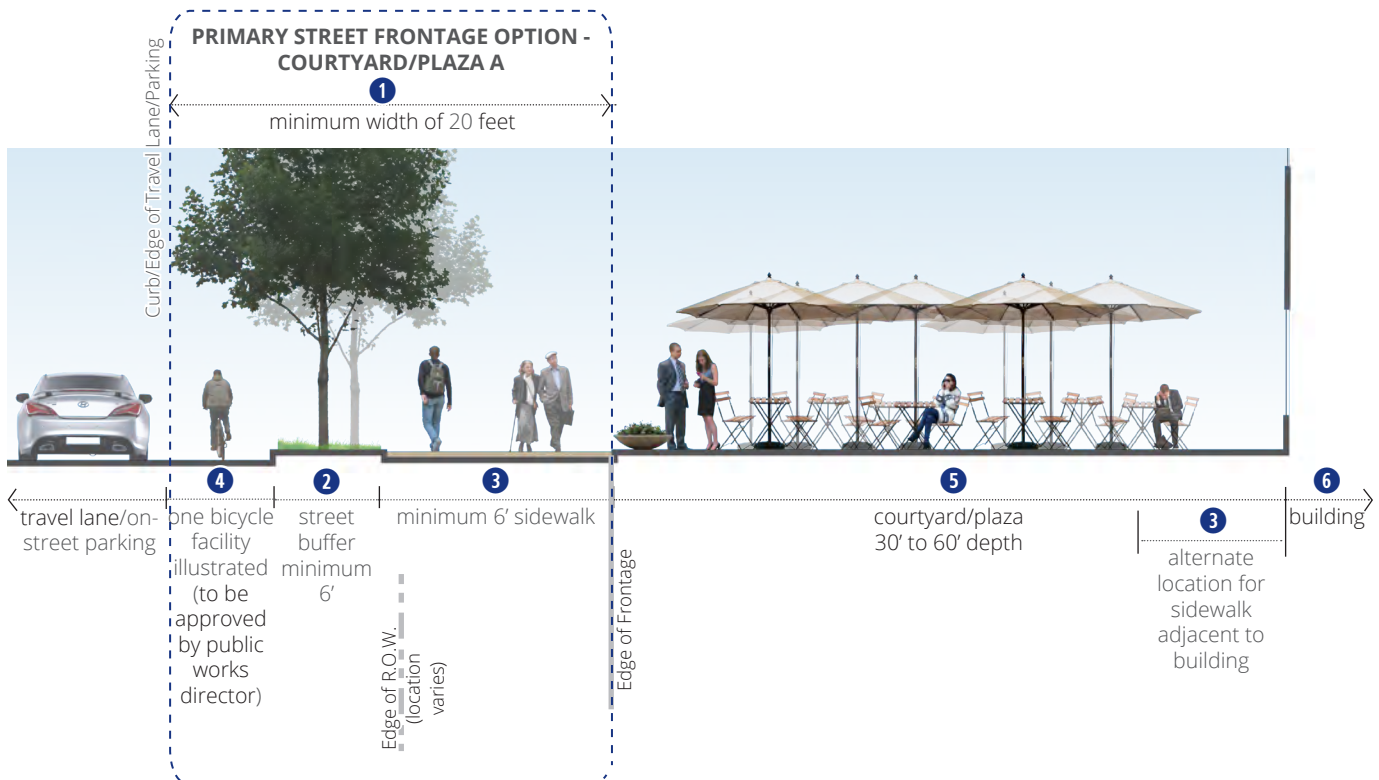


Figure 27-98-22 Primary Street Frontage - Courtyard/Plaza A

27-98 Perimeter Center Overlay

Street Types

DRAFT

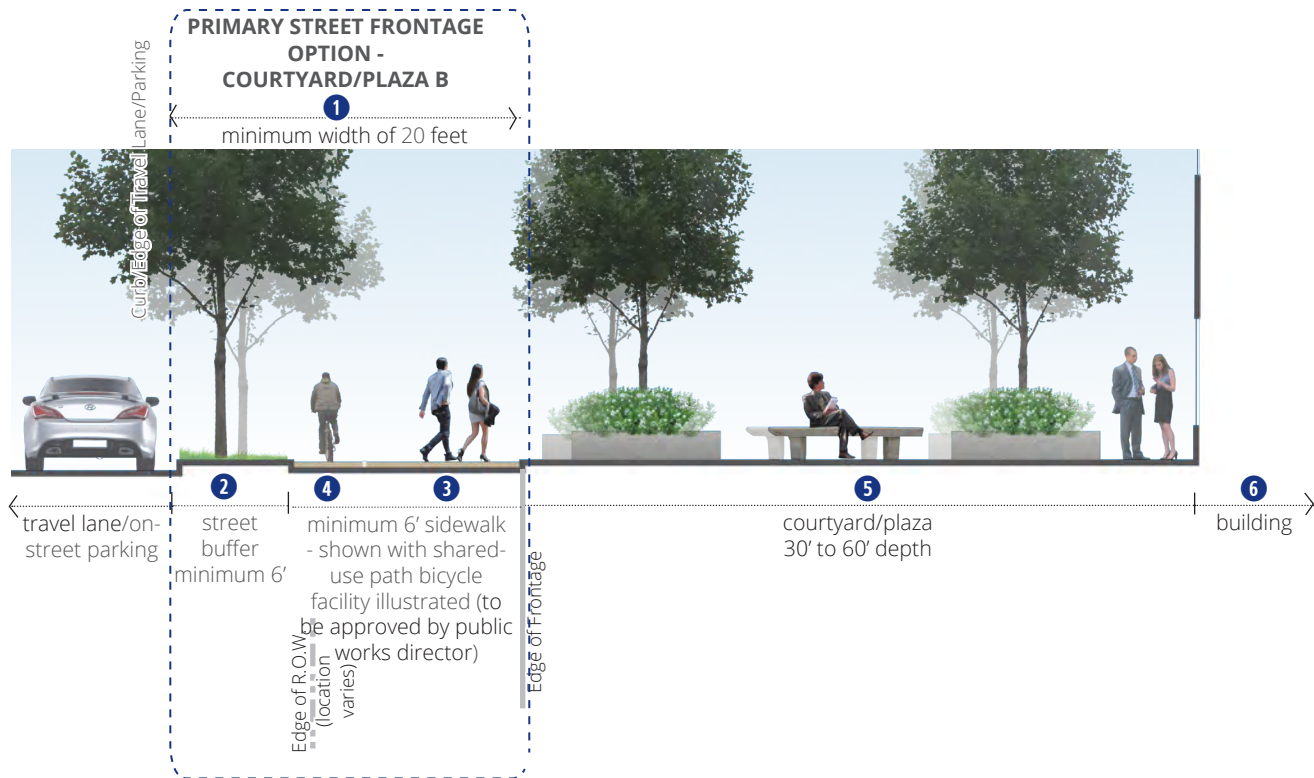


Figure 27-98-23 Primary Street Frontage Option - Courtyard/Plaza B

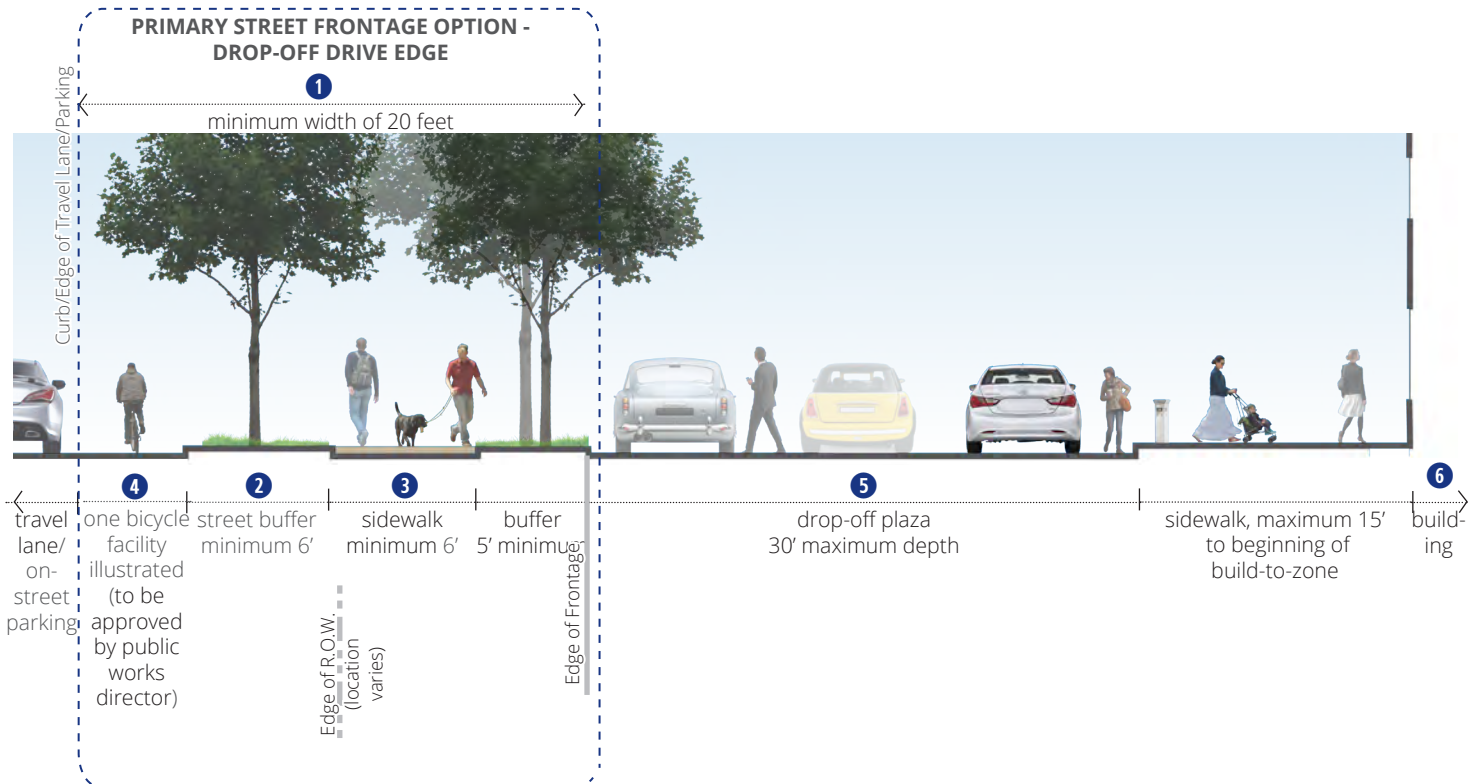


Figure 27-98-24 Primary Street Frontage Option - Drop-off Drive Edge

27-98 Perimeter Center Overlay

Street Types

(8) Secondary Street Type

The Secondary Street Type applies to new and existing streets per Figure 27-98-2 Street Types Map.

a. New Secondary Streets. Refer to Figure 27-98-25 Table of Secondary Street Type Requirements for Secondary Street Type requirements for new streets, Figure 27-98-26 Secondary Street Type Section - Typical illustrates the typical street section. A narrow version is available for limited application (refer to Figure 27-98-27 Secondary Street Type Section - Narrow).

b. Frontage Options. Frontages address the pedestrian and bicycle realm as well as street & parking buffers and public space options. Parcels that are adjacent to a Secondary Street as designated on Figure 27-98-2 Street Types Map, shall apply one or more of the Secondary

Street frontage options along the entire length of the lot line abutting the Secondary Street. Refer to Figure 27-98-28 Table of Secondary Street Frontage Options.

c. Dedication. When the sidewalk is fully or partially located on private property, right-of-way dedication is required, so that the sidewalk is fully located in the right-of-way. The public works director may approve an easement in lieu of right-of-way dedication based on the geometry of the site, topography, or other site-specific considerations.

d. Curb Movement. The existing street curb may be required to move, based on the selection of the appropriate, context sensitive bicycle facility, per the public works director.

FIGURE 27-98-25: TABLE OF SECONDARY STREET TYPE REQUIREMENTS

CONFIGURATION	TYPICAL refer to Figure 27-98-26	LIMITED NARROW refer to Figure 27-98-27
Location	O-I, OCR, O-D, C-1, CR-1, NS, PC-1, PC-2, PC-3, and PC-4 Districts	RM-HD, PC-4 Districts; use requires approval of public works director/community development director
Right-of-Way Width	66' to 100' per public works director	60' to 75' per public works director
Travel Lanes	2 lanes, 2 way	1 yield lane
Lane Width	10' to 12' per public works director	16' to 18' per public works director
Allowable Turn Lanes	Per public works director	Per public works director
Parking Lanes	Per public works director	Per public works director
Pavement Width	34' to 40'	32'
Frontage Options	Refer to Figure 27-98-28 Table of Secondary Street Frontage Options	

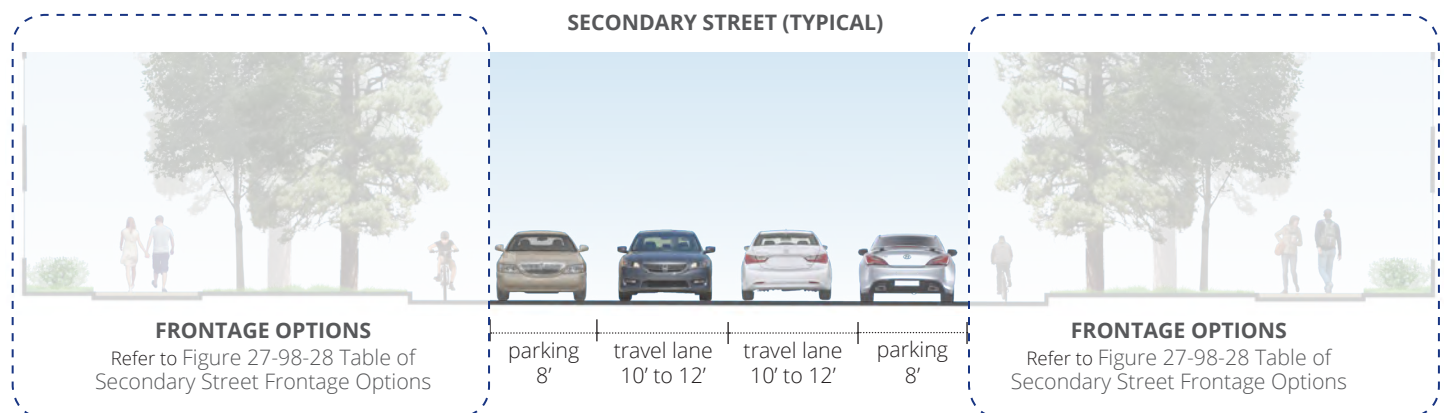


Figure 27-98-26 Secondary Street Type Section - Typical

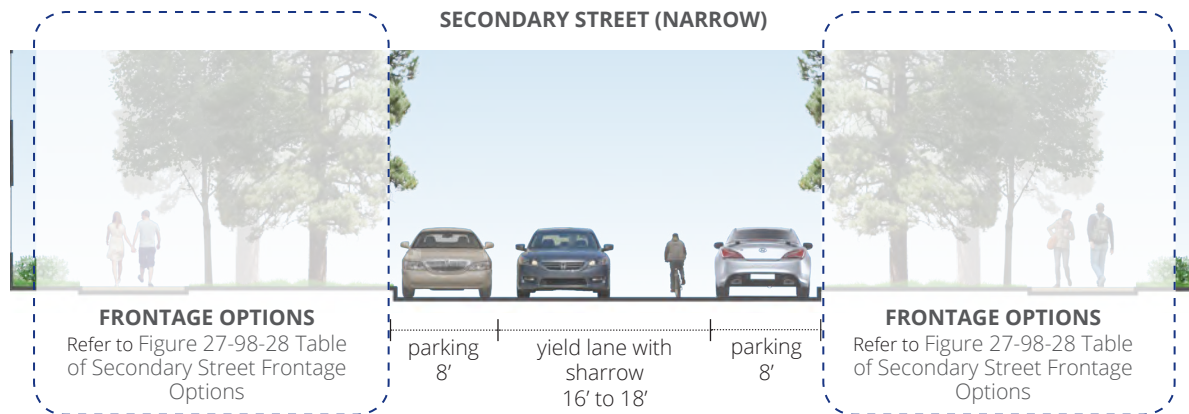


Figure 27-98-27 Secondary Street Type Section - Narrow

FIGURE 27-98-28: TABLE OF SECONDARY STREET FRONTAGE OPTIONS		
	PARKING EDGE refer to FIGURE 27-98-29	YARD EDGE refer to FIGURE 27-98-30
1 Minimum Depth of Frontage	The minimum depth of the frontage shall be 20 feet measured from existing curb. Minimum depths of components shall be met; additional areas shall be added to any component to meet the minimum overall depth.	
2 Street Buffer	<p>Minimum Depth: Street buffer width shall be a minimum of 6 feet wide.</p> <p>Tree Plantings: A minimum of one tree for every 300 square feet of buffer shall be provided, spaced evenly or informally.</p> <p>Grade: Grade changes may be accommodated within the street buffer with slopes no greater than 1 ft of rise in 4 ft run. Retaining walls may be incorporated to meet these grades, but shall be no taller than 3 feet.</p>	
3 Sidewalk	A minimum 6 foot wide sidewalk is required and may meander between trees.	
4 Bicycle Facilities	Shared, on-street bicycle facilities	
5 Yard Treatment	A minimum 8' screen is required between parking and the sidewalk. The screen shall include a canopy tree every 30 feet of frontage, spaced evenly or informally, and a double row of shrubs, minimum 24" in height and width at maturity, staggered and spaced 24" on center.	A minimum 5' deep landscape yard is required, with a minimum of 30 shrubs, perennials, and/or grasses for every 500 square feet of yard area.
6 Building & Parking Location	Refer to Article II. Zoning Districts for setback line or 27-105 Building Types for Secondary Frontage Options requirements for allowable widths of parking along the frontage and required building facades along the frontage.	

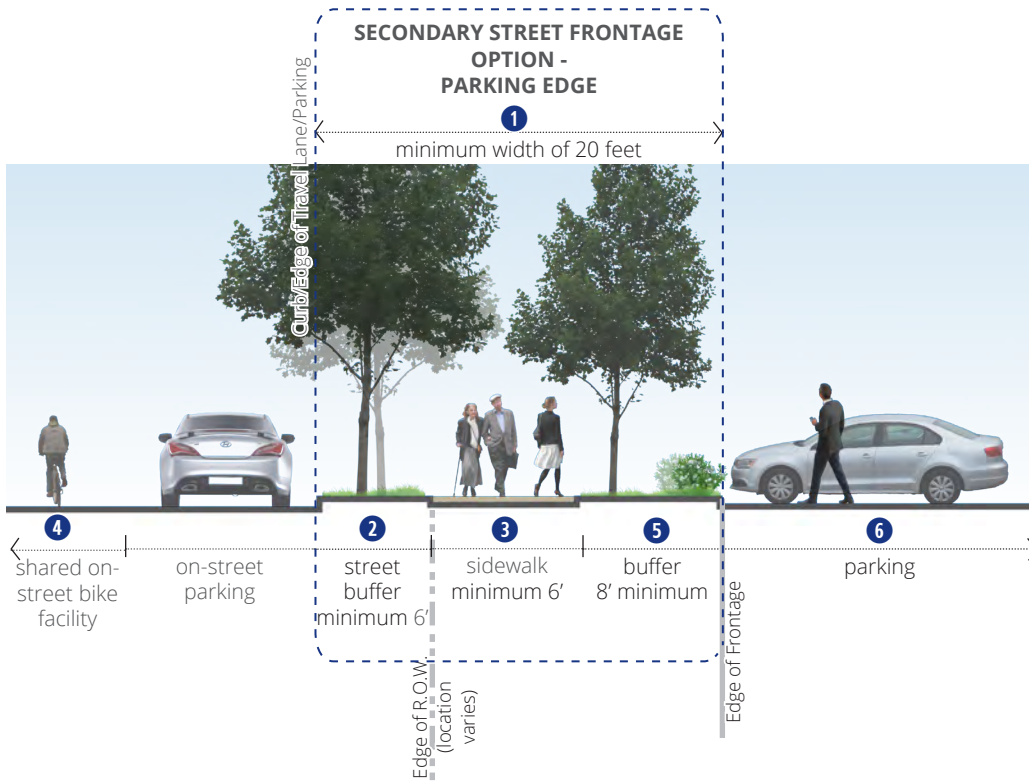


Figure 27-98-29 Secondary Street Frontage Option - Parking Edge

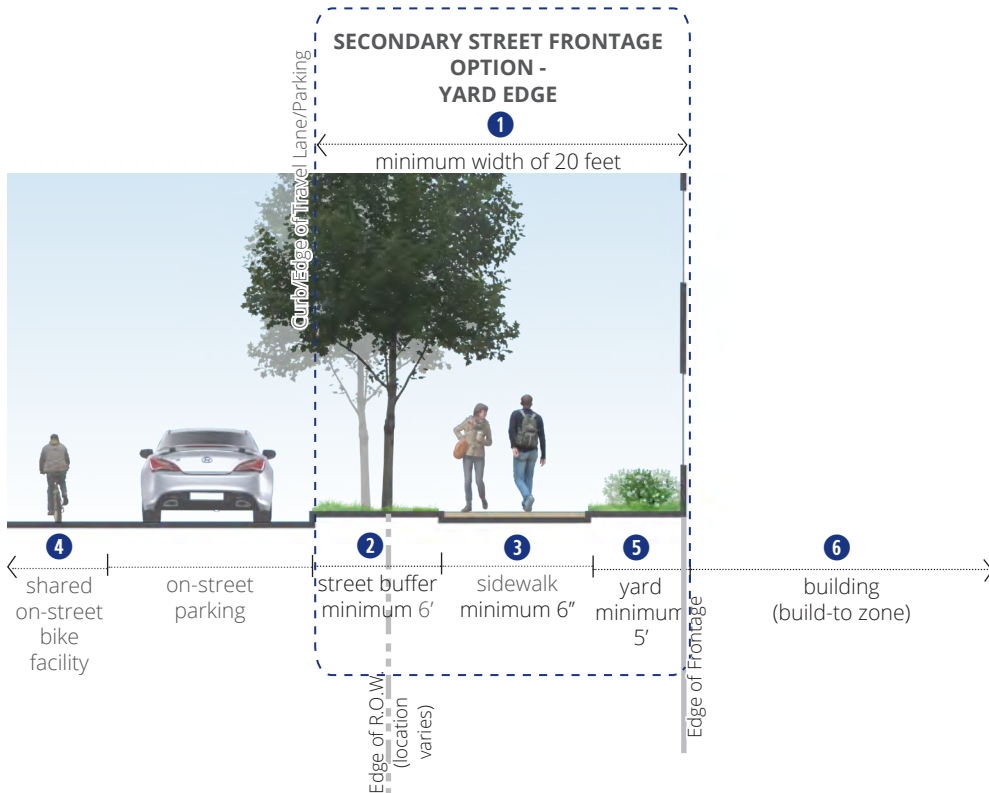


Figure 27-98-30 Secondary Street Frontage Option - Yard Edge

(c) Streets & Blocks**(1) Intent**

The intent of the following requirements is to form an interconnected network of streets with multiple intersections and block sizes scaled to support multiple modes of transportation, including walking, biking, transit use, and driving, within the Perimeter Center Overlay area. Also refer to Chapter 16 for additional regulations.

(2) Applicability

All new development and redevelopment on site 5 acres or larger within the Perimeter Center Overlay shall meet the general block, lot, and street design requirements under any of the following circumstances:

- a. **New Structure.** Development of a new principal structure on a lot or portion of a lot.
- b. **Redevelopment or Renovation.** Redevelopment or renovation to an existing structure or site that increases the gross building square footage by more than 50 percent.
- c. **Parking Lots.** Redevelopment or revision to 50 percent or more of an existing parking lot or development of a new parking lot consisting of 15 or more spaces, not including resurfacing or repairing an existing layout.

(3) New Street Locations.

- a. **Street Types Map.** Proposed street locations are illustrated on Figure 27-98-2 Street Types Map, defined to establish a network that would fulfill the Streets & Blocks intent and regulations.
- b. **Public Works Director.** Locations illustrated on the Street Types map are approximate. The requirements and locations of all streets shall be determined by and approved by the public works director during the Site Plan review process.

(4) Street Connectivity.

The following provides requirements and guidance for locating new streets and connecting to surrounding context.

- a. The arrangement of streets shall provide for the continuation of existing streets from adjoining areas into new developments.
- b. Cul-de-sac and dead end streets should be avoided and utilized only when topography and

other existing barriers, such as highways, rail lines, or waterways, prevent street connectivity.

- c. Streets should follow natural features rather than interrupting or dead-ending at the feature.
- d. Streets should terminate at another street with either open space or a building facade across from the termination.
- e. When adjacent developments do not provide connectivity, coordinate with the community development director and public works director to determine the potential for future connections and provide for those connections.

(5) Block Configuration

Refer to Figure 27-98-31 for an illustration of Typical Block Elements and section 16-240 for Blocks.

- a. Blocks should be deep enough to accommodate buildings facing streets with parking located in the interior. Refer to 27-98(b) Street Types for minimum building frontages required for all streets.
- b. Blocks may be configured to include existing lots within an existing zoning district that is outside of the Perimeter Center Overlay.

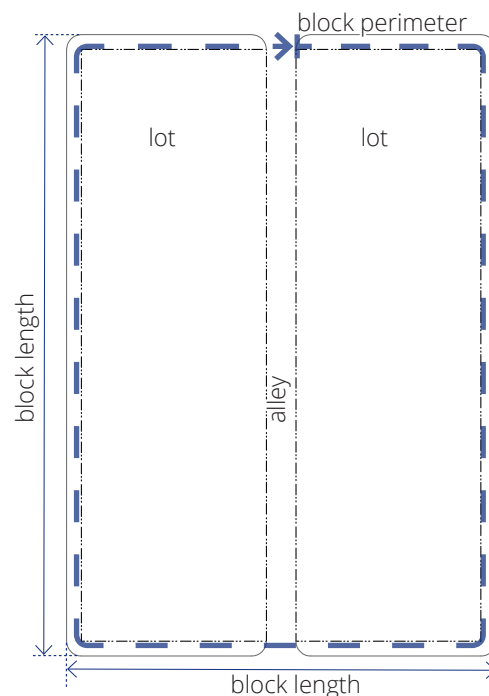


Figure 27-98-31 Typical Block Elements

- c. A network of streets is required to meet the maximum block size requirement. Proposed new streets to meet these sizes are illustrated on Figure 27-98-2 Street Types Map.
- d. **Block Sizes for Zoning Districts.** Block sizes for residential and commercial development and redevelopment are designated in section 16-240.
- e. **Block Sizes for Perimeter Center (PC) Districts.** Maximum block perimeter for all PC Districts is 2400 feet. Recommended block perimeter is a maximum of 1800 feet.
- f. **Exceptions.** Exceptions to block sizes include locations adjacent to natural features, such as steep grades and drainage areas, and other existing barriers, such as an inability to connect to adjacent parcels.

(6) Block Driveway Access Configurations

- a. Blocks may include alleys, drives, or driveway entrances for service, parking accessibility, and other routes internal to the development.
- b. Access to blocks should be aligned across the street from access to other blocks.
- c. In the Perimeter Center districts, driveway locations are defined in the general Building Type requirements. Refer to 27-105(a)(10) Driveways.
- d. **Mid-Block Pedestrianways.** Mid-Block pedestrianways are required through blocks longer than 800 feet and at locations that connect public rights-of-way with other public facilities such as parks and transit.
 - 1. When combined with mid-block street crossings, these pathways should align to facilitate easy pedestrian movements.
 - 2. Mid-block pedestrianways should be located in the middle third of a block face.
 - 3. Minimum width for mid-block pedestrianways rights-of-way or easements is 20 feet.
 - 4. A minimum of 1 canopy tree per 600 square feet of area is required.
 - 5. Mid-block pedestrianways should be treated with the minimum design requirements per 27-98(b)(4) Streetscape Furnishings & Hardscape Design.
- 6. Shared-use paths through blocks per 27-98(b)(3)d may fulfill the requirements for Mid-Block Pedestrianways.

(7) Street Types

Refer to 27-98(b) Street Types for requirements for streets.

- a. **Street Type.** All new and existing streets shall utilize one of the permitted Street Types per Figure 27-98-2 and the requirements of 27-98(b) Street Types.
- b. **Frontage Options.** For each side of a street adjacent to or within the development, one of the frontage options defined by Street Type shall be utilized.
- c. **Other Internal Street.** Other street types completely internal to the development may be approved by the public works director and the community development director through minor exception.
- d. **Public Use.** All streets shall be available for public use at all times. Gated streets and streets posted as private are not permitted.
- e. **Dimensions.** The dimensions defined in each Street Type provide the acceptable ranges. The public works director shall determine the appropriate configuration and dimensions for each street. Minor exceptions may be approved by the public works director during the approval process.
- f. **Additional Director Requirements.** During the Pre-Submittal Conference, the community development director and/or public works director may adjust requirements for right-of-way, pavement width, or street elements depending on unique site locations and characteristics.
- g. **Street Construction.** All street construction, whether publicly dedicated or privately held, shall follow Street Type and Frontage requirements and any other specifications defined by the City.

(d) General Building Design Criteria

(1) Intent. These criteria are intended to address each building's appearance and cohesiveness within the overall Perimeter Center Overlay area. Further, the criteria are intended to elicit high quality buildings, enhance the pedestrian experience, maintain an appropriate scale, and implement the vision for the area as defined in current plans.

(2) Applicability. The following outlines the general design criteria applicable to all buildings within the Perimeter Center Overlay.

- a. Applicable Facades.** These criteria apply to all facades visible from the street, facing streets, facing main parking lots, and adjacent to open spaces, unless otherwise noted.
- b. Applicable Development Sites.** All new development and redevelopment within the Perimeter Center Overlay shall meet the general building design requirements under any of the following circumstances:
 1. **New Structures & Additions.** Development of a new principal structure or additions to a principal structure on a lot or a portion of a lot.
 2. **Existing Façade Renovation.** An alteration to an existing structure that includes renovation of more than 50 percent of an applicable facade.

(3) Materials.

- a. Major Facade Materials.** Major facade materials include high quality, durable, finish materials, such as brick, stone, and glass. Other high quality materials may be approved by the Director of Community Development during the site plan process. A minimum of 80 percent of each facade shall be constructed of major materials. Street facade materials shall continue around the corner of the building to non-street-facing facades a minimum depth of 20 feet. Refer to Figure 27-98-32 Major Materials.
- b. Minor Materials.** Acceptable high quality minor facade materials include all major materials; cement-based stucco; wood lap siding and shingles; architectural metal siding; architecturally finished concrete; fiber cement siding or shingles (such as Hardie Company products or similar); synthetic stucco/EIFS (see restrictions below); glass block; split-faced, burnished, glazed, or honed concrete masonry



Major Materials: Brick, stone



Major Materials: Glass, concrete

Figure 27-98-32 Major Materials

units (CMU), cast stone concrete elements. No more than 20 percent of each facade shall consist of minor materials.

c. Accent Materials. The following materials may be used for trim, details, soffits, eaves, and other accent areas: vinyl; aluminum and other durable metals; and metal for beams, lintels, trim, and ornamentation. Other materials may be approved by the Director of Community Development during the site plan process.

d. Restrictions.

1. The use of plain CMU block and vinyl are prohibited as a facade material.
2. Synthetic stucco (EIFS) used on the first or second floor of a building shall be a "high-impact" system. Conventional EIFS may be used above the second floor.

e. Roof Materials. Wherever asphalt shingles are used, they shall be a minimum 3-dimensional architectural type.

(4) Windows, Awnings, and Shutters

a. Quantity of Windows. A minimum of 15 percent window coverage is required per story on all street facing facades and any facade visible from a street.

1. **Measurement.** Window requirements are measured as a percentage of the facade per floor.
2. **Blank Walls.** Windows shall be distributed so that no rectangular area greater than 30 percent of any story's facade, as measured from floor to floor, and no horizontal segment of a story's facade greater than 30 feet in width is windowless and violates the minimum percentage requirements.

b. Transparent Glass. All window glass shall be highly transparent with low reflectance. Light transmission should be approximately 60 percent for ground story windows and a minimum of 55 percent for upper story windows.

c. Awnings. All awnings shall be canvas or metal. Plastic awnings are prohibited. Awnings shall not be translucent. Refer to Figure 27-98-33 Awnings.



Permitted Awnings: Metal



Permitted Awnings: Canvas

Figure 27-98-33 Awnings



Figure 27-98-34 Examples of Ground Story along Slope.

d. Shutters. If installed, shutters, whether functional or not, may be designed to the following standards:

1. All shutters should be sized for the windows, so that if the shutters were to be closed, they would not be too small for complete coverage of the window.
2. Shutters should be wood, metal, or fiber cement. Other “engineered” woods may be approved during the site plan process with an approved sample and examples of successful, high quality local installations.

(5) Garage Doors. The following requirements apply to garage doors provided on any street facade.

- a. Location.** Garage doors are not permitted on Primary Street facades unless not utilized for vehicular access (but for patio access, open air dining). Garage doors are permitted on Secondary Street Facades with direct access to the street. The preferred location is on interior lot facades.
- b. Recessed from Facades.** Garage Doors located on street facing facades shall be recessed a minimum of 3 feet from the dominant facade of the principal building facing the same street.
- c. Design.** Garage doors facing a Secondary Street and intended to be closed during business hours shall be clad with materials consistent with the design of the building. Carriage-style doors are required on the Townhouse and Detached House Building Type.

(6) Ground Story at Sloping Facades

- a. Intent.** Given the slopes in many parts of the Perimeter Center area, building design must accommodate grade changes along the sidewalk without creating tall, out-of-scale blank walls. Large, unarticulated building facades signal to pedestrians that an area is not intended for walking, reducing activity in the area and creating dead zones. Refer to Figure 27-98-34 Examples of Ground Story along Slope.
- b. Regulations for Shopfronts.**
 1. Grade transitions on the building along the sidewalk should be designed to maximize active pedestrian-scale frontages between waist and eye level while minimizing blank walls.

2. When possible, the interior floor level should step to match the exterior grade.
3. If it is necessary for the interior floor to remain constant along the grade, changes may be accommodated by a storefront window display space.
4. Knee wall and retaining walls shall not exceed 30 inches in height except along a maximum 15 foot section of facade length.
5. If grade change is more than 12 feet along a single block face, entrance requirements may be increased to one entrance per 80 feet of building frontage.
6. Building entrances adjacent to the street should be within 3 feet of the elevation of the adjacent sidewalk, unless utilizing the PC District building types.

c. Regulations for Residential and other Building Frontages.

1. Grade transitions at the building along the sidewalk should be designed to minimize blank walls.
2. When possible, the interior floor level should step to match the exterior grade.
3. Multiple front entrances along the street activate each segment of building section at each grade.
4. Transition zones between the sidewalk and building facade of porches, terraces, and landscape areas may be used assist with grade changes.
5. If it is necessary for the interior floor to remain constant along the grade, changes can be accommodated by terraced planters and retaining walls.
6. Retaining walls shall not exceed 30 inches in height except along a maximum 15 foot section of frontage.
7. When the elevation of the first floor is more than 3 feet above grade, windows should be provided into the basement or lower floor elevations.

(7) Fueling Stations

Refer to Figure 27-98-35 Recommended Gas Station Layout for one illustration of the following criteria.

- a. **Building Location.** A principal building should be built up to any corner with the longest facade along any Primary Street. If no Primary Street abuts the parcel, along the Parkway Facade.
- b. **Side Yard Parking.** One double loaded aisle of parking is permitted on the interior side along a Primary Street, perpendicular to the street.
- c. **Pumps and Canopy Location.** The pumps and canopy may be located on the interior of the parcel, but may have driveways to a street.
- d. **Building Build-to Zone.** A building should be built to within 5 feet to 15 feet of the edge of the Street Type frontage.
- e. **Building Facade Design.** The facade of all buildings shall meet all general design criteria within this section, 27-98(d) General Building Design Criteria.
- f. **Canopy Design.** The canopy, columns, and structure shall be constructed substantially of the same materials utilized on the building.
- g. **Pavement.** Site paving shall consist of maximum 22 foot driveways and lanes along parking and along pumps. All other areas shall be landscaped.
- h. **Frontage.** A maximum of 50 percent of the frontage may be used for parking and fuel areas.

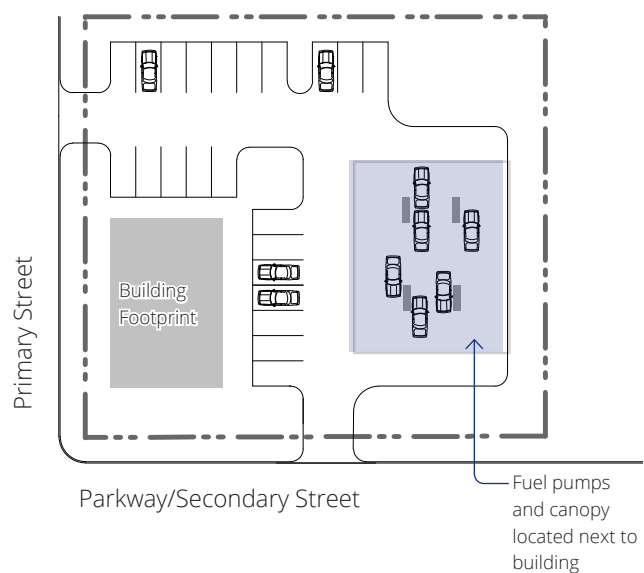


Figure 27-98-35 Recommended Gas Station Layout

(8) Accessory Drive-through Structures

Refer to Figure 27-98-36 Recommended Drive-Through Facility Layout for one illustration of the following requirements.

- a. Structure/Canopy.** Drive-through structures or canopies shall be located on the rear or side facade of the building or in the rear of the lot behind the building, where permitted by use. The structure should not be visible from any Primary Street.
- b. Stacking Lanes.** Stacking lanes shall be located perpendicular to the Primary Street or behind the building.
- c. Canopy Design.** The canopy, columns, and structure shall be constructed substantially of the same materials utilized on the building.

(9) Parking Structures Fronting a Street

The following requirements are provided for parking structures fronting a Secondary Street or Parkway.

- a. Primary Streets.** Parking structures, whether accessory or principal use, shall not front a Primary Street, unless otherwise approved with Special Land Use Permit per Article V, Division 3.
- b. Perimeter Center Districts.** In the Perimeter Center (PC) Districts, parking structures as the principal use on the lot require a special land use permit (refer to Article V, Division 3 Special Land Use Permits).
- c. Parking Structure Design Requirements.**
 1. **Blank Wall Limitations.** On any street facade, no rectangular area greater than 30 percent of any story's facade, as measured from floor to floor, and no horizontal segment of a story's facade greater than 15 feet in width may be solid, blank wall.
 2. **Pedestrian Entry.** A defined pedestrian entrance/exit is required separate from the vehicular entrance and directly accessing the sidewalk. If the entry is enclosed, windows are required to meet a transparency rate of 65 percent.
 3. **Structures along Highways.** Structured parking located along highways shall incorporate architectural elements (e.g. trellises, planters, landscape, panels) that provide visual screening of vehicles and interior lighting.

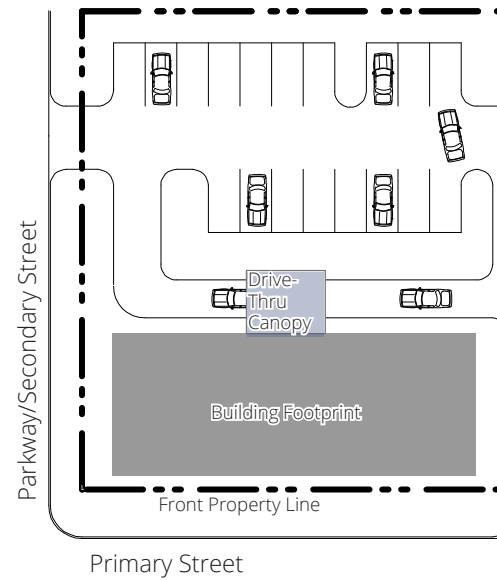


Figure 27-98-36 Recommended Drive-Through Facility Layout

DRAFT



Perimeter Center DISTRICTS

City of Dunwoody, Georgia



DRAFT

SEPTEMBER 20, 2016

City of Dunwoody

41 Perimeter Center East
Suite 250
Dunwoody GA 30346

City Mayor

Denis Shortal

City Council

Lynn Deutsch
John Heneghan
Terry Nall
Jim Riticher
Pam Tallmage
Douglas Thompson

Planning Commission

Bob Dallas, Chair
Kirk Anders
Richard Grove
Bill Grossman
Renate Herod
Paul Player
Heyward Wescott

City Staff

Rebecca Keefer, City Planner/Director of Sustainability & Project Manager
Eric Linton, City Manager
Jessica Guinn, Assistant City Manager
Steve Foote, Community Development Director
Michael Starling, Economic Development Director
Michael Smith, Public Works Director
Andrew Russell, Planning Coordinator
Eric Bosman, City Consultant

Sounding Board

Robert Miller
Alex Chambers
Bob Dallas
Jennifer Harper
Stacey Harris
Cheryl Spitalnick
Tony Torbert

CODAMETRICS**duncan** associates

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27-103 General

SECTIONS

- (a) Intent & Applicability
- (b) Administration & Procedures
- (c) How to Use this Code

How To Use The PC Districts

1

Locate parcel on
Regulating Map Figure 27-104-1
to determine which district is designated



27-104(a)

PC Districts

2

Review
District Requirements
to determine permitted Building Types, Uses, Height, and other requirements



27-104(b) to (g)

PC Districts

If the development is for a site where
New Streets
are shown, go to Street Types and Streets & Blocks



27-98(b) & (c)

Perimeter Center Overlay

If the development is for a site that is
3 acres or greater in size
go to Open Space Types



27-106

PC Districts

3

Review
Building Types
to comply with requirements for Building Type(s) and their site(s)



27-105

PC Districts

4

Locate parcel on
Street Types Map Figure 27-98-1
for Street Type designation for use with Building Types and Frontage requirements



27-98(b)

Perimeter Center Overlay

5

Review
Street Type Frontage Requirements
to determine street buffer, sidewalk/trail, and frontage landscape



27-98(b)

Perimeter Center Overlay

If the development is for a site where
New Shared Paths
are shown, go to Shared Paths



27-98(b)

Perimeter Center Overlay

If the development
Borders a different district
(or single-family neighborhood as noted), go to Transitional Buffers



27-230

Zoning Ordinance

6

Determine the applicable
Development Approval Process
to prepare all required application materials

27-103(b)

Perimeter Center Districts

Figure 27-103-1 How to Use the Code

(a) Intent & Applicability

(1) Intent. The Perimeter Center (PC) district regulations are intended to promote the same purposes identified for the Perimeter Center Overlay, as stated in 27-98(a)(2).

(2) Applicability. The PC zoning district regulations apply as shown on the Regulating Map (Figure 27-104-1), but only to lots that have been rezoned to the applicable PC zoning district in accordance with the amendment procedures of Article V, Division 2.

(b) Administration & Procedures**(1) Establishment of Perimeter Center Districts.**

Perimeter Center (PC) zoning district classifications may be applied to property and PC district boundaries may be amended only in accordance with the amendment procedures of Article V, Division 2 and the Regulating Map of Figure 27-104-1.

(2) Effect of Establishment. Once property has been classified in a Perimeter Center district, that property is subject exclusively to the applicable Perimeter Center district regulations and any applicable overlay district regulations.

(3) Review and Approval Procedures. Unless otherwise expressly stated, all uses and development in Perimeter Center districts are subject to the applicable review and approval procedures of Article V, as well as any applicable procedures in chapter 16 of the municipal code (land development regulations).

(4) Site Plan Review. All applications for development permits within a Perimeter Center district must be accompanied by a site plan, which is subject to review and approval in accordance with Article v, Division 10.

(5) Relief. The Perimeter Center districts are intended to accommodate development as-of-right when such development occurs in strict conformance with applicable regulations. In some instances, however, unique site conditions, existing building circumstances or other constraints related to the subject property may make strict compliance impossible or highly impractical. In such instances, an applicant may seek relief from applicable regulations. Any relief granted and conditions imposed run with the land and are binding on the subject property owner and all future property owners.

a. Minor Exception-Administrative Relief. The community development director is authorized to approve the following minor exceptions in

accordance with the special administrative permit procedures of Article v, Division 7:

1. The location of a building relative to any minimum setback requirement or build-to zone width/location, provided that location deviates from strict compliance by no more than 5 feet;
2. An increase in total impervious coverage by no more than 5 percent, not to exceed the total amount of permitted impervious plus semi-pervious coverage;
3. A decrease in build-to zone coverage by no more than 10 percent;
4. A reduction in the amount of required Shopfront building frontage along Primary Streets, by no more than 10 percent;
5. Additional height of any building story by no more than 2 feet, provided that the overall building height does not exceed the maximum permitted height;
6. An increase in the maximum permitted height of a retaining wall by up to 5 feet; and
7. Any other minor exception expressly authorized under the Perimeter Center district regulations.

b. Major Exceptions-Special Land Use Permit.

Major exceptions to strict compliance with the Perimeter Center district regulations include any exception to strict compliance that is not expressly authorized as a minor exception under the provisions of subsection 27-103(B)(5) a. Major exceptions require review and approval in accordance with the special land use permit procedures of Article v, Division 3.

(c) How to Use this Code

Refer to Figure 27-103-1 How to Use the Code for an overview of code use.

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27-104 Districts

SECTIONS

- (a) Districts & Regulating Map
- (b) PC-1 District
- (c) PC-2 District
- (d) PC-3 District
- (e) PC-4 District
- (f) Uses
- (g) Sustainability Measures

(a) Districts & Regulating Map

(1) District Map. The Perimeter Center districts are designated in Figure 27-104-1 Regulating Map.

(2) District Requirements. The requirements in this subsection apply to the Perimeter Center districts as follows.

- a. Requirements Specific to each District.** Refer to 27-104(b) through 27-104(e) for specific descriptions and requirements for each district.
- b. Use Requirements for all Districts.** Refer to 27-104(f) Uses for use requirements applicable to all districts.
- c. Sustainability Measures for all Districts.** Refer to 27-104(g) Sustainability Measures for sustainable development practice requirements applicable to all districts.

(3) Street Types. Refer to section 27-98(b) Street Types within the Perimeter Center Overlay for information on Street Types and street frontage requirements applicable to all Perimeter Center districts.

(4) Transition Yards. Refer to section 27-230 Transition Yards for information on buffer and screen requirements between different districts and uses.

(5) New Streets & Blocks. Refer to section 27-98(c) Streets & Blocks within the Perimeter Center Overlay for information on new street design requirements and block configurations.

(6) Other Perimeter Center Overlay Requirements. Refer to sections 27-98(d) General Building Design Criteria within the Perimeter Center Overlay.

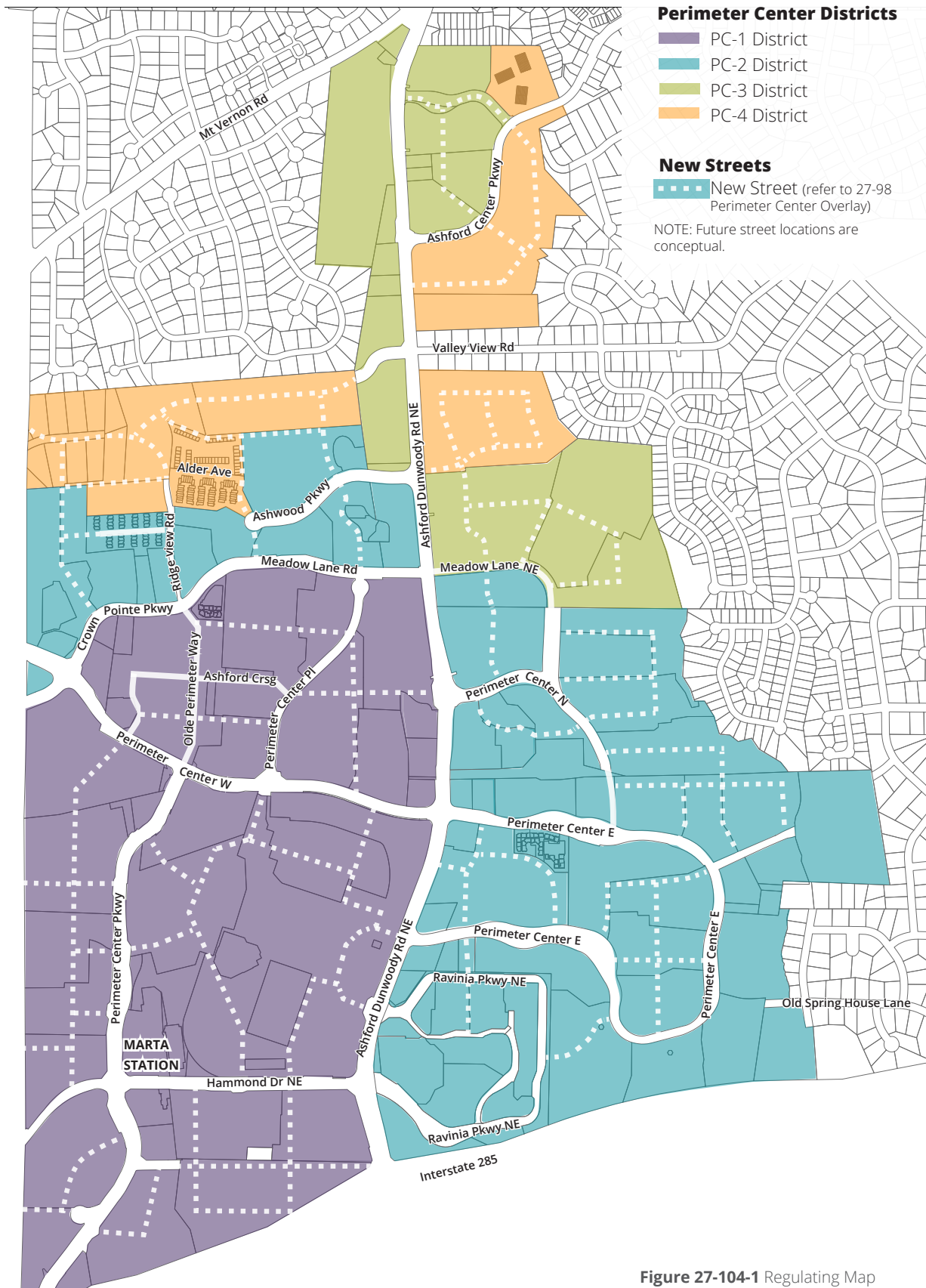


Figure 27-104-1 Regulating Map

(b) PC-1 District

Requirements for this district are provided in Figure 27-104-2. PC-1 District Requirements.

(1) Description & Intent. The PC-1 District is intended to apply to the central core area of Perimeter Center, including the area directly surrounding the Dunwoody MARTA train station. This district allows for the highest intensity of buildings, a high level of employment uses, and active ground story uses and design that support pedestrian mobility.

PC-1 DISTRICT REQUIREMENTS			Reference
BUILDING TYPES	Shopfront	● Minimum 60% of the length of all Primary Street frontages shall be fronted with the Shopfront Building. Refer to Figure 27-98-2 Street Types Map.	27-105 Building Types
	General	●	
	Townhouse		
	Detached House		
	Civic	●	
USE MIX	Minimum Mix of Uses	For all developments over 3 acres in size, a minimum of 2 use categories shall be accommodated.	27-104(f) Uses
HEIGHT	Minimum	For parcels larger than 3 acres, buildings shall be a minimum of 18 feet in height.	27-105(b)(2) for floor-to-floor heights per Building Type;
	Maximum	30 stories or 370 feet, whichever is less	
STREETS & BLOCKS	Streets & Block Configurations	Required per Figure 27-104-1 Regulating Map & Figure 27-98-2 Street Types Map	27-98(c) Streets & Blocks
	Minor Parkway	●	27-98(b) Street Types
	Primary Street	●	
	Secondary Street	●	
	Secondary Street Narrow		
SITE COVERAGE	Maximum Impervious Cover	85 percent	27-621 Terms Defined
	Maximum Impervious plus Semi-Pervious Cover	95 percent	
OPEN SPACE REQUIREMENTS	For all developments 3 acres or larger, one permitted Open Space Type is required within 1/8 of a mile of the principal entrance(s) to each building. For developments over 15 acres, at least one Open Space Type shall be a minimum of 1 contiguous acre in size.		27-106 Open Space Types
	When multiple open spaces are required, no more than 2 of any one type may be utilized to meet the required open space.		
	Plaza	●	
	Green	●	
	Commons	● Permitted but shall not be used to meet open space requirement.	
	Park	●	

Figure 27-104-2. PC-1 District Requirements

● = permitted

(c) PC-2 District

Requirements for this district are provided in Figure 27-104-3. PC-2 District Requirements.

(1) Description & Intent.

The PC-2 District is made up primarily of employment uses, residential buildings, and limited shopfront retail and services.

PC-2 DISTRICT REQUIREMENTS			Reference
PERMITTED BUILDING TYPES	Shopfront	● Permitted on Primary Street frontages only. Refer to Figure 27-98-2 Street Types Map.	27-105 Building Types
	General	●	
	Townhouse	●	
	Detached House		
	Civic	●	
USE MIX	Minimum Mix of Uses	No mix required	27-104(f) Uses
HEIGHT	Minimum	1 story; single story buildings shall be 18 to 24 feet in height.	27-230 for transition yards; 27-105(a)(9)e for perimeter buffer; 27-105(b)(2) for floor-to-floor heights per Building Type
	Maximum: less than 100 feet from a lot line adjacent to single family zoning district	3 stories or 42 feet, whichever is less	
	Maximum: between 100 feet and 500 feet to a lot line adjacent to single family zoning district	5 stories or 70 feet, whichever is less	
	Maximum: greater than 500 feet to a lot line adjacent to single family zoning district	14 stories or 180 feet, whichever is less	
STREETS & BLOCKS	Streets & Block Configurations	Required per Figure 27-104-1 Regulating Map & Figure 27-98-2 Street Types Map	27-98(c) Streets & Blocks
	Minor Parkway	●	27-98(b) Street Types
	Primary Street	●	
	Secondary Street	●	
	Secondary Street Narrow		
SITE COVERAGE	Maximum Impervious Cover	65 percent	27-621 Terms Defined
	Maximum Impervious plus Semi-Pervious Cover	75 percent	
OPEN SPACE REQUIREMENTS	For all developments 3 acres or larger, one Open Space Type is required within 1/4 of a mile of the principal entrance(s) to each building. For developments over 18 acres, at least one Open Space Type shall be a minimum of 1 contiguous acre in size.		
	When multiple open spaces are required, no more than 2 of any one type may be utilized to meet the required open space.		
	Plaza	●	27-106 Open Space Types
	Green	●	
	Commons	●	
	Park	●	

● = permitted

Figure 27-104-3. PC-2 District Requirements

(d) PC-3 District

Requirements for this district are provided in Figure 27-104-4. PC-3 District Requirements.

(1) Description & Intent. The PC-3 District is a smaller scale less intensive commercial district, permitting both shopfront buildings and office buildings.

PC-3 DISTRICT REQUIREMENTS			Reference
PERMITTED BUILDING TYPES	Shopfront	●	Minimum 80% of the length of all Primary Street frontages shall be fronted with Shopfronts. Refer to Figure 27-98-2 Street Types Map.
	General	●	27-105 Building Types
	Townhouse	●	
	Detached House		
	Civic	●	
USE MIX	Minimum Mix of Uses	No mix required	27-104(f) Uses
HEIGHT	Minimum	1 story; single story buildings shall be 18 to 24 feet in height.	27-230 for transition yards;
	Maximum: less than 100 feet from a lot line adjacent to single family zoning district	3 stories or 42 feet, whichever is less	27-105(a)(9)e for perimeter buffer;
	Maximum: greater than 100 feet to a lot line adjacent to single family zoning district	5 stories or 70 feet, whichever is less	27-105(b)(2) for floor-to-floor heights per Building Type
STREETS & BLOCKS	Streets & Block Configurations	Required per Figure 27-104-1 Regulating Map & Figure 27-98-2 Street Types Map	27-98(c) Streets & Blocks
	Minor Parkway	●	
	Primary Street	●	27-98(b) Street Types
	Secondary Street	●	
	Secondary Street Narrow		
SITE COVERAGE	Maximum Impervious Cover	75 percent	27-621 Terms Defined
	Maximum Impervious plus Semi-Pervious Cover	85 percent	
OPEN SPACE REQUIREMENTS	For all developments 3 acres or larger, one Open Space Type is required within 1/4 of a mile of the principal entrance(s) to each building. For developments over 18 acres, at least one Open Space Type shall be a minimum of 1 contiguous acre in size.		
	When multiple open spaces are required, no more than 2 of any one type may be utilized to meet the required open space.		
	Plaza	●	27-106 Open Space Types
	Green	●	
	Commons	●	Permitted but shall not be used to meet open space requirement.
	Park	●	

Figure 27-104-4. PC-3 District Requirements

● = permitted

(e) PC-4 District

Requirements for this district are provided in Figure 27-104-5. PC-4 District Requirements.

(1) Description & Intent. The PC-4 District is made up primarily of residential uses at a scale that provides a transition between the intensity of Perimeter Center and the surrounding single-family residential neighborhoods.

PC-4 DISTRICT REQUIREMENTS			Reference
PERMITTED BUILDING TYPES	Shopfront		
	General	●	
	Townhouse	●	27-105 Building Types
	Detached House	●	
	Civic	●	
USE MIX	Minimum Mix of Uses	No mix required	27-104(f) Uses
HEIGHT	Minimum	1 story	
	Maximum	3 stories or 42 feet, whichever is less	27-105(b)(2) for floor-to-floor heights per Building Type
STREETS & BLOCKS	Streets & Block Configurations	Required per Figure 27-104-1 Regulating Map & Figure 27-98-2 Street Types Map	27-98(c) Streets & Blocks
	Minor Parkway		
	Primary Street	●	
	Secondary Street	●	27-98(b) Street Types
	Secondary Street Narrow	●	
SITE COVERAGE	Maximum Impervious Cover	60 percent	
	Maximum Impervious plus Semi-Pervious Cover	70 percent	27-621 Terms Defined
OPEN SPACE REQUIREMENTS	For all developments 3 acres or larger, one Open Space Type is required within 1/8 of a mile of the principal entrance(s) to each building. For developments over 18 acres, at least one Open Space Type shall be a minimum of 1 contiguous acre in size.		
	When multiple open spaces are required, no more than 2 of any one type may be utilized to meet the required open space.		
	Plaza		27-106 Open Space Types
	Green	●	
	Commons	●	
	Park	●	

● = permitted

○ = permitted under listed conditions

Figure 27-104-5. PC-4 District Requirements

(f) Uses

The following applies to all Perimeter Center districts.

(1) Use Table. The following apply to the uses outlined in this section. Refer to Figure 27-104-6. Table of Permitted Uses.

- a. Use Categories.** Refer to 27-111 through 27-116.
- b. Permitted and Special Uses.** Each use may be permitted as-of-right, permitted with a special administrative permit, permitted as a special exception, or permitted with a special land use permit. Refer to 27-111(4) Use Tables.
- c. Number of Uses.** A lot may contain more than one use.
- d. Principal and Accessory Uses.** Each of the uses may function as either a principal use or accessory use on a lot, unless otherwise specified.
- e. Building Type.** Each use shall be located within a permitted Building Type (Refer to 27-105 Building Types), unless otherwise specified.

(2) Developments of Regional Impact. Any project that constitutes a major modification to an existing Development of Regional Impact or a new application for a Development of Regional Impact requires review and approval through the Special Land Use Permit

process. See also 27-303 for additional process requirements.

(3) Use Sub-Categories. For the purposes of fulfilling the Use Mix requirements defined in each district table (refer to 27-104(b) PC-1 District through 27-104(e) PC-4 District), utilize the following sub-categories of uses, consisting of those uses listed in the table, right, that may contribute to the mix. Some permitted uses are not included.

a. Lodging & Residence Sub-Category

Household Living
Group Living
Lodging

b. Civic Sub-Category

Club or Lodge, Private
Cultural Exhibit
Educational Services
Hospital
Place of Worship

c. Office Sub-Category

Construction and Building Sales and Service
Medical Service
Office or Consumer Service
Research and Testing Services

d. Retail Sales Sub-Category

Retail Sales
Food and Beverage Retail Sales

e. Service Use Sub-Category

Animal Services
Day Care
Repair or Laundry Services, Consumer
Personal Improvement Services
Eating and Drinking Establishments
Financial Services
Entertainment and Spectator Sports
Sports and Recreation, Participant

USES	DISTRICTS				
	PC-1	PC-2	PC-3	PC-4	Reference
RESIDENTIAL					
Household Living					
Detached house	—	—	—	P	
Attached house	—	P[1]	P[1]	P[1]	
Multi-unit building, rental	S	S	S	S	
Multi-unit building, owner- occupied	P	P	P	P	
Age-Restricted Multi-unit building, rental	S	S	S	S	
Age-Restricted Multi-unit building, owner-occupied	P	P	P	P	
Group Living	—	S	S	S	
QUASI-PUBLIC & INSTITUTIONAL					
Ambulance Service	S	S	S	S	
Club or Lodge, Private	P	P	P	—	
Cultural Exhibit	P	P	P	P	
Day Care	P	P	P	P	
Educational Services	S	S	S	S	
Hospital	S	S	S	—	
Place of Worship	P	P	P	P	27-146
Utility Facility, Essential	P	P	P	E	27-151
COMMERCIAL					
Animal Services	P	P	S	—	27-131
Communication Services					
Telecommunications antenna mounted to building or similar structure	A	A	A	A	27-150
All other	P	P	P	P	
Standalone tower	S	S	S	S	
Construction and Building Sales and Services	P	P	P	—	
Eating and Drinking Establishments					
Food truck	P	P	P	—	27-138

USES	DISTRICTS				Reference
	PC-1	PC-2	PC-3	PC-4	
Other eating or drinking establishment	P	P	P	—	
Drive-through Facility	S	S	P	—	27-98(d)(8)
Entertainment and Spectator Sports					
Indoor	P	P	P	—	
Outdoor	S	S	S	—	
Financial Services					
Banks, credit unions, brokerage and investment services	P	P	P	—	
Food and Beverage Retail Sales	P	P	P	—	
Funeral and Interment Services	P	P	P	—	
Lodging	P	P	P	S	
Medical Service	P	P	P	—	
Office or Consumer Service	P	P	P	—	
Parking, Non-accessory	S	S	S	S	27-98(d)(9)
Personal Improvement Service	P	P	P	—	
Repair or Laundry Service, Consumer	P	P	P	—	
Research and Testing Services	P	P	P	—	
Retail Sales	P	P	P	—	
Sports and Recreation, Participant					
Indoor	P	P	P	—	
Outdoor	S	S	S	—	
Vehicle and Equipment, Sales and Service					
Gasoline sales	—	—	S	—	27-98(d)(7)
Vehicle sales and rental (Indoor only)	P	P	P	—	27-154
Vehicle repair, minor	—	—	S	—	27-153

P = use permitted as of right
A = special administrative permit required
E = special exception required
S = special land use permit required

[1] Where more than 10% of the units are rental, a special land use permit is required.

Figure 27-104-6. Table of Permitted Uses

(g) Sustainability Measures

(1) Intent. The following requirements are intended to further the sustainability goals of the City defined in the most current Sustainability Plan adopted by the City.

(2) Applicability. The sustainable development measures in this section shall be addressed by all new developments in a Perimeter Center (PC) district that involve the complete replacement of an existing building or construction of a new building.

(3) Calculation & Evaluation

- a. Minimum Points Required.** The applicant shall achieve no fewer than 7 points from any combination of the sustainable development measures as valued in Figure 27-104-7 Sustainability Measures & Values. No partial points will be accepted.
- b. Minimum Requirements of the Measure.** All points shall be awarded based on meeting the minimum requirements of each sustainability measure, as indicated in this section.
- c. Newly Constructed Measures.** Measures count only if they are part of the new development application; measures already in place at the time of application do not count, unless otherwise approved by the community development director.
- d. Required Documentation.** The following documentation is required:
 1. Documentation of which measures and total number of points the applicant will achieve shall be indicated on the Development Permit application submitted to the City (refer to Article V, Division 9).
 2. Documentation is required to clearly illustrate the extent to which the minimum requirements of each of the selected measures is to be met through permanent construction or policies.

	MEASURES	VALUE
ENERGY CATEGORY	Building Energy Efficiency	3 points
	Renewable Energy Sources	5 points
	Green Roof	4 points
	Heat Island Reduction	2 points
WATER CATEGORY	Building Water Efficiency	2 points
	Water-Efficient Landscaping	1 points
	Pervious Pavement	2 points
TRANS-PORTATION CATEGORY	Enhanced Bicycle Amenities Measure	1 to 2 points
	Transportation Access Measure	4 points
ALTERNATIVE	Alternative Measure	1 to 3 points

Figure 27-104-7 Sustainability Measures & Values

(4) Energy Category Measures

- a. Energy Efficiency.** Newly constructed buildings must demonstrate an average 10 percent improvement over the energy code currently in effect in the city.
- b. Renewable Energy.** Incorporate renewable energy generation on-site with production capacity of at least 5 percent of the building's annual electric or thermal energy, established through an accepted building energy performance simulation tool. The following renewable energy generation sources are applicable: solar thermal or photovoltaics, ground-sourced heating or cooling, fuel cells and microturbines using non-fossil fuel, wind energy conversion. Other means of generating electricity without using a fuel, such as kinetic, heat exchange, approved by the community development director.
- c. Green Roof.** Install a vegetated roof for at least 50 percent of any building roof area or roof deck; a minimum of 2,500 square feet is required to receive credit.

d. Heat Island Reduction. Use any combination of the following strategies for 35 percent of all on-site, non-roof hardscape areas, including sidewalks, plazas, courtyards, parking lots, parking structures, and driveway: coverage of the surface at canopy tree maturity in 15 years, and/or solar reflective paving & roofing with a SRI (solar reflectance index) of at least 29.

(5) Water Category Measures

- a. Building Water Efficiency.** Indoor water use in new buildings and major renovations must be an average 20 percent less than in baseline buildings. Baseline water usage shall be determined based on fixtures per the Energy Policy Act of 1992 and subsequent rulings by the United States Department of Energy or a similar method approved by the community development director.
- b. Water-Efficient Landscaping.** Reduce potable water used for landscape irrigation by 50 percent from a calculated midsummer baseline case by using either one of the following methods: utilizing all xeriscape plant materials and providing no permanent irrigation system, or using only captured rainwater with an irrigation system.
- c. Pervious Pavement.** Install an open grid or pervious pavement system that is at least 40 percent pervious on 65 percent of all hardscape surface areas, including sidewalks, plazas, courtyards, parking lots, and driveways. The water shall be directed into the groundwater or other acceptable storm accommodation per the public works director.

(6) Transportation Category Measures

- a. Enhanced Bicycle Amenities Measure.** Inclusion of 2 of the following earns 1 point. Inclusion of 3 of the following earns 2 points.
- 1. Lockable enclosed bicycle storage.** Provide 1 secure, enclosed bicycle storage space for 10 percent of planned employee occupancy with no more than 10 spaces required.
 - 2. Employee shower facilities.** Provide a minimum of one shower facility per 150 employees, minimum of one total.
 - 3. Repair Center.** Provide a designated bicycle repair center open to the public and consisting of, at least one air pump, water, and basic tools for minor repairs.

b. Transportation Access Measure. Site must be within 1/4 mile of existing or proposed transit stop with provision of enhanced access to transit and shall include one or more of the following.

1. Construction of a bus turnout on development property or in adjacent street right-of-way.
2. Construction of additional shared use paths other than those required per 27-98(b)(3)d Shared Use Paths.
3. Provision of direct platform connection to rail station.
4. Additional easement for provision or enhancement of transit.
5. Other items approved by the public works director.

(7) Alternative Measure. The applicant may submit an alternative sustainable development measure for approval by the community development director. The measure shall further a sustainability goal and shall not be considered standard practice for current developments. The measure shall be unrelated to any of the other measures defined in this Section. Based upon their review, the community development director shall approve for the number of points to be awarded. Required documentation shall clearly illustrate that the measure furthers a sustainability goal.

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27-105 Building Types

SECTIONS

- (a) Requirements for all Building Types
- (b) Regulations Specific to Building Types
- (c) Explanation of Specific Building Type Requirements

27-105 Building Types

Requirements for all Building Types

DRAFT

(a) Requirements for all Building Types

The Building Types detailed in this section outline the required building forms for new construction and renovated structures within the Perimeter Center districts defined in 27-104 Districts.

(1) Applicability. The requirements of this section apply to all new development, or new structures or buildings, established in 27-103(a)(2). All structures must meet the requirements of one of the Building Types permitted within the Zoning District.

(2) Districts, Building Types, and Street Types. For each lot, a designated Perimeter Center district is found on Figure 27-104-1 Regulating Map.

a. District. Each district, per 27-104 Districts, has a set of requirements related to streets, open space, uses, and permitted Building Types.

b. Building Types. Each Building Type, as established in this section 27-105, has a set of requirements, including requirements related to the facades facing different Street Types.

c. Street Types. Street Types, per section 27-98(b) Street Types of the Perimeter Center Overlay, include frontage requirements for the space between the build-to zone of the Building Type and the curb of the street.

(3) Uses. Uses are permitted by District, per 27-104(f) Uses. Each Building Type can house a variety of uses depending on the district in which it is located. Some Building Types have additional limitations on permitted uses as located within the building.

(4) General Building Design Requirements. Refer to section 27-98(d) General Building Design Criteria within the Perimeter Center Overlay for information on building design requirements additional to and applicable to all Building Types.

(5) Multiple Principal Structures. Multiple structures are permitted on all lots in the Perimeter Center districts. All structures shall meet the requirements of one of the permitted Building Types for the district.

(6) Permanent Structures. All buildings constructed shall be permanent construction, unless otherwise noted.

(7) Other Structures. Utility structures and towers permitted in the district are exempted from the Building Type standards.

(8) Build to the Corner. The corner of the lot is defined as the intersection of the two build-to zones of

each street. Refer to Figure 27-105-1 Corner and Build-to Zones on Different Street Types.

a. Corners at Primary Street and Parkways.

Buildings shall be built up to the corner, occupying the intersection of the two build-to zones of any intersection of two Primary Streets, a Primary Street at any Parkway, or two Parkways. Courtyards shall not be located in this area.

b. Corners involving Secondary Streets. Buildings should be built to the corner, occupying the intersection of the two build-to zones of any intersection involving a Secondary Street. Courtyards should not be located in this area.

(9) Treatment of Build-to Zones, Setbacks, & Transition Yards. Refer to 27-621 Terms Defined for definition of build-to zones and setbacks.

a. Landscape Areas. All build-to zones and setbacks shall only contain courtyards (as permitted by Street Type frontage), plazas (as permitted by Street Type frontage), patios (as permitted by Street Type frontage), sidewalk or trail extensions, buffers, and/or landscape yards, unless otherwise specifically noted.

b. Transition Yards. Refer to 27-230 for size and landscape requirements.

c. Surface Parking. Surface parking lot yard location is determined by Building Type and shall only be permitted in setbacks when the setback

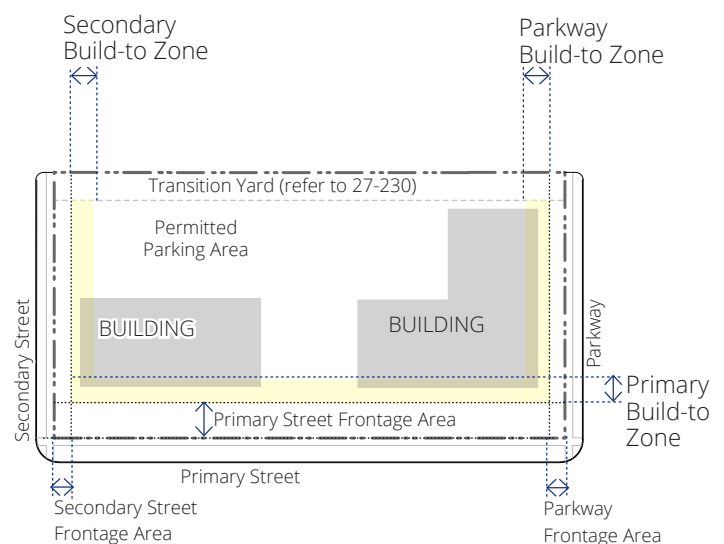


Figure 27-105-1 Corner and Build-to Zones on Different Street Types

is labeled as a “building setback”. Parking shall be set back from a lot line abutting another property a minimum of 5 feet. Refer to 27-105(b) Regulations Specific to Building Types.

- d. Driveways.** Driveways may cross perpendicularly through build-to zones and setbacks, when permitted per 27-105(a)(10) Driveways, below, or to connect to a parking lot on an abutting lots.
- e. Perimeter Buffer.** On all PC district lots, a required transition yard of 100 feet is established at the lot line of a PC district and any other non-PC zoning district within the city of Dunwoody. Within the perimeter buffer, building height is restricted to 3 stories or 42 feet, whichever is less. Refer to 27-104 Districts.

(10) Driveway Access. Location of vehicular driveway access is based on the Major or Minor Parkway, Primary Street, and Secondary Street designations in Figure 27-98-2 Street Types Map per the hierarchy as follows. Refer to Figure 27-105-2 Driveway Locations by Street Type.

- a. Public Works Director.** The public works director shall determine the quantity of driveways, need for closure of existing driveways, width of driveways, and proximity of driveways to other driveways or streets.

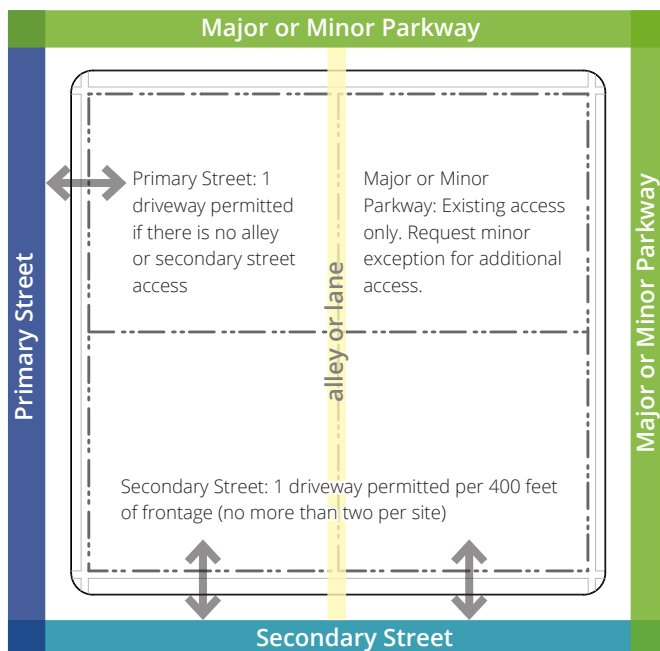


Figure 27-105-2 Driveway Locations by Street Type

- b. Alleys/Lanes.** Access from an adjacent alley or lane is unlimited. Construction of new private or public alleys/lanes through blocks is encouraged. Typically, alleys or lanes cut through a block and provide vehicular access to multiple garages, parking lots, or service areas. Minimum pavement width for an alley or lane is 16 feet and maximum pavement width is 24 feet.
- c. Secondary Street Access.** If no alley exists, one driveway access per 200 linear feet of street frontage or 2 total driveway access points, whichever is greater, are permitted from each Secondary Street per site.
- d. Primary Street.** If no alley or Secondary Street exists, one driveway access point is permitted off each Primary Street.
 - 1. Side Parking off Primary Street.** When side parking with the drive located perpendicular to the street centerline, is utilized as permitted by the Building Type, one driveway access is permitted off the Primary Street. This driveway counts towards entrances permitted by 27-105(a)(10)d, above. The sidewalk paving shall extend across the driveway, signaling priority to the pedestrian.
- e. Parkways.** If no alley, Secondary Street, or Primary Street exists, existing driveways may be utilized off Parkways. New driveways off Minor or Major Parkways are permitted only if no other option exists and a minor exception is granted by the public works director.
- f. Shared Driveways.** Shared driveways are encouraged for all other adjacent developments.
- g. Inter-Lot Drives.** When two or more parking lots are located adjacent to each other and each lot contains the same Building Type, the parking lots shall be connected with a drive perpendicularly crossing the Transition Yard. Other parking lots should be connected wherever practical.
- h. Pedestrian Facilities.** At-grade, designated pedestrian routes, including sidewalks and crosswalks, shall be supplied connecting each parking area to either the Primary sidewalk (and front entrance) or a rear public entrance.
 - 1.** Crosswalks shall include pavers or colored concrete pavement across driveways.
 - 2.** Vehicular areas, such as driveways, parking drive lanes, loading lanes, bike lanes/facilities,

and parking areas, shall not be utilized as designated pedestrian routes.

3. Pedestrian connections are desirable between all adjacent sites and land uses.

i. Driveway Width. Driveways shall be no greater than 22 feet in width at the right-of-way line. Driveways wider than 22 feet shall include landscaped, pedestrian refuge islands with a walk area to reduce the crossing to no more than 22 feet.

j. Loading Facilities. Loading facilities for all Building Types in all Perimeter Center districts shall be located as follows:

1. Loading facilities shall not face Primary Streets.
2. Loading facilities shall not be located in any required building setback area.
3. Preferred location for loading facilities is on the interior of the lot, not visible from any streets.
4. Loading facilities visible from a Parkway or Secondary Street shall be screened from the sidewalk by a fence/wall or a combination of fence/wall and landscape screening.
 - i. Fence/Wall. Fences and walls shall be no taller than 6 feet and shall have a minimum overall opacity of 80 percent. Materials shall meet the requirements of 27-98(d)(3)a Major Facade Materials, in the Perimeter Center Overlay.
 - ii. Landscape. Tree canopies shall be used to screen above the 6 foot wall or fence.
 - iii. Shrubs and Hedges. Shrubs and hedges may be utilized to fulfill no more than one third of the screening, as measured along the screen length. A double row of 5 gallon shrubs, with mature height of a minimum 6 feet, is required.

k. Accessory Buildings. The following applies to all accessory buildings or structures in any PC district:

1. Attached accessory structures are considered part of the principal structure and shall meet all requirements of the principal structure.

2. Detached accessory structures shall comply with all building and district requirements unless otherwise modified in the following:

- i. Detached accessory structures are not permitted between the face of the building and any street right-of-way line.
- ii. Detached accessory structures shall be a maximum of 10 feet lower in height than the overall building height or 15 feet in height, whichever is greater.

(b) Regulations Specific to Building Types

(1) Building Type Descriptions & Intentions

- a. Shopfront Building.** The Shopfront Building is intended for use as a mixed use building located close to the sidewalk with parking typically in the rear or side of the lot.

The most distinctive element of this Building Type is the ground floor front facade with large amounts of storefront glass and regularly spaced entrances, typically for retail and service uses.

- b. General Building.** The General Building is intended to be built close to the sidewalk, but may also allow for a landscape yard and drop off drives. This building can be structured to house offices or residential uses including multi-family, and may have limited amounts of accessory retail and service uses in the ground floor.

Parking is typically provided in structures at the rear of the lot, internally in the rear of the building, but may also have limited surface parking along the side of the building.

- c. Townhouse Building.** The Townhouse Building is a building typically comprised of several multi-story single-family units, located adjacent to each other, each with its own entrance to the street.

- d. Detached House Building.** The Detached House Building is a building surrounded by landscape yard, with an entrance facing the street, and typically housing single family uses.

Parking is required to be located in the rear, either incorporated into a detached garage or in an attached garaged accessed from the rear of the building.

- e. Civic Building Description.** The Civic Building is the most flexible Building Type limited to civic and institutional types of uses.

These buildings are distinctive within the urban fabric created by the other Building Types and could be designed as iconic structures.

(2) Building Type Regulations

The following defines and illustrates the regulations for each Building Type.

a. SHOPFRONT BUILDING REGULATIONS

	PC-1 DISTRICT	PC-2 DISTRICT	PC-3 DISTRICT	
BUILDING SITING Refer to Figure 27-105-3 Shopfront Building: Building Siting and 27-105(c)(1) Building Siting by Street Types				
Along Primary Street Frontage Edge				
1	Minimum Build-to Zone Coverage	90%	80%	80%
2	Build-to Zone (Measured from Street Type Frontage edge. Refer to Figure 27-105-21)	0' to 10'	0' to 15'	0' to 15'
3	Off-Street Parking along Frontage	one double-loaded aisle, perpendicular to street per 27-105(c)(1)c.1. Side Parking	permitted (refer to 27-105(a)(8) Build to the Corner for buildings required on corners)	
Along Parkway Frontage Edge				
4	Minimum Build-to Zone Coverage	60%	30%	30%
5	Build-to Zone (Measured from Street Type Frontage edge. Refer to Figure 27-105-21)	0' to 15'	0' to 15'	0' to 15'
6	Off-Street Parking along Frontage	permitted (refer to 27-105(a)(8) Build to the Corner for buildings required on corners)		
Along Secondary Street Frontage Edge				
7	Minimum Build-to Zone Coverage	30%	none required	none required
8	Build-to Zone (Measured from Street Type Frontage edge. Refer to Figure 27-105-21)	0' to 30'	0' to 50'	0' to 50'
9	Off-Street Parking along Frontage	permitted	permitted	permitted
At Adjacent Lot Lines				
10	Minimum Side Building Setback	15'; 0' abutting other PC-1 districts	15'	15'
11	Minimum Rear Building Setback	30'	30'	30'
FLOOR-TO-FLOOR HEIGHT Refer to Figure 27-105-4 Shopfront Building: Floor-to-Floor Height & Use Requirements and 27-105(c)(2) Floor-to-Floor Height.				
12	Ground Story: Minimum Height Maximum Height	14' 30' ¹	14' 30' ¹	14' 24' ¹
13	Ground Story Elevation	80% between 0' to 2' above adjacent sidewalk; visible basement not permitted (refer to 27-98(d)(6) Ground Story at Sloping Facades for stepping facades at sloped sidewalks)		
USES Refer to Figure 27-105-4 Shopfront Building: Floor-to-Floor Height & Use Requirements and 27-105(c)(3) Uses within Building.				
14	Ground Story	any retail subcategory, service subcategory, lobbies for lodging (refer to 27-104(f) Uses) permitted in the district		
15	Upper Story	any use permitted in the district		
16	Parking within Building	permitted fully in any basement and in rear of ground and upper floors; entrance to parking should be off an interior facade		
17	Required Occupied Space	30' deep from any Primary Street facade on all ground and upper stories		
STREET FACADE DESIGN REQUIREMENTS Refer to Figure 27-105-5 Shopfront Building: Street Facade Design Requirements and 27-105(c)(4) Street Facade Design Requirements				
18	Ground Story Fenestration Measured between 2' and 10' above grade	75% minimum	60% minimum	60% minimum
19	Entry Area	recessed between 3' and 8' from the facade closest to the street		
20	Entrance Elevation	within 2' of average grade at the sidewalk adjacent to entrance		
21	Required Number of Primary Street Entrances	1 per each 75' width of front facade		
22	Ground Story Vertical Facade Divisions	every 30' of facade width		
23	Horizontal Facade Divisions	required within 3' of the top of the ground story, and every 5th floor above the 1st floor		

Notes

¹ If 18 feet or more in height, ground story shall count as 2 stories towards maximum building height.

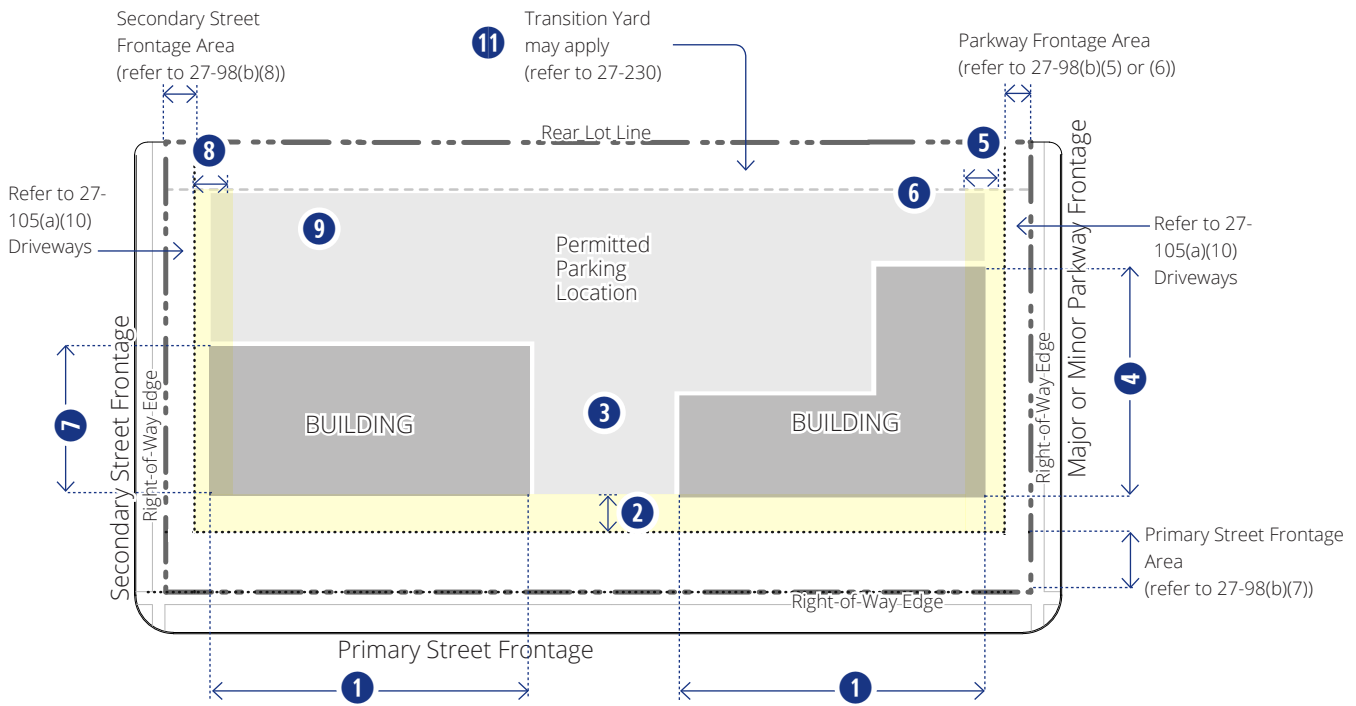


Figure 27-105-3 Shopfront Building: Building Siting

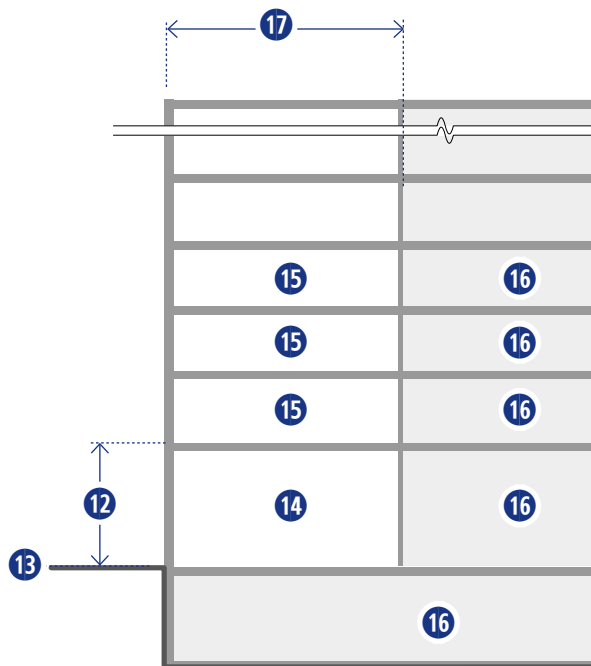


Figure 27-105-4 Shopfront Building: Floor-to-Floor Height & Use Requirements

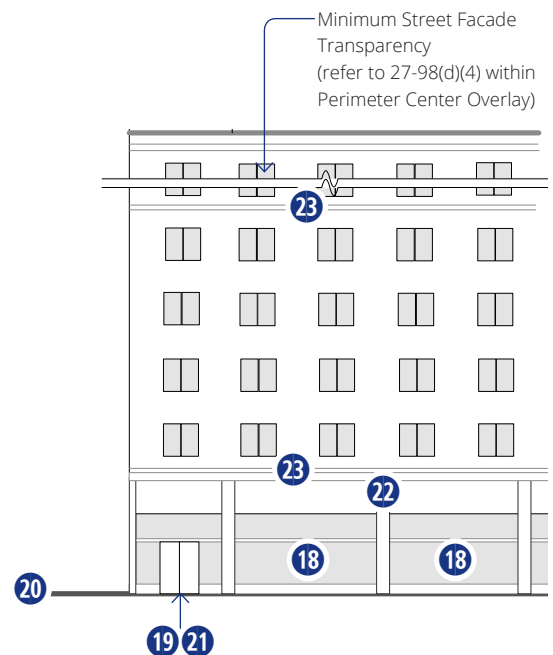


Figure 27-105-5 Shopfront Building: Street Facade Design Requirements

b. GENERAL BUILDING REGULATIONS

	PC-1 DISTRICT	PC-2 DISTRICT	PC-3 DISTRICT	PC-4 DISTRICT
BUILDING SITING Refer to Figure 27-105-6 General Building: Building Siting and 27-105(c)(1) Building Siting by Street Types				
Along Primary Street Frontage Edge				
1	Minimum Build-to Zone Coverage	80%	50%	60%
2	Build-to Zone (Measured from Street Type Frontage edge. Refer to Figure 27-105-21)	5' to 20'	10' to 30'	5' to 15'
3	Off-Street Parking along Frontage	one double-loaded aisle, perpendicular to street per 27-105(c)(1) c.1. Side Parking	permitted (refer to 27-105(a)(8) Build to the Corner for buildings required on corners)	
Along Parkway Frontage Edge				
4	Minimum Build-to Zone Coverage	60%	50%	50%
5	Build-to Zone (Measured from Street Type Frontage edge. Refer to Figure 27-105-21)	5' to 20'	10' to 30'	15' to 50'
6	Off-Street Parking along Frontage	permitted (refer to 27-105(a)(8) Build to the Corner for buildings required on corners)		
Along Secondary Street Frontage Edge				
7	Minimum Build-to Zone Coverage	30%	none required	
8	Build-to Zone (Measured from Street Type Frontage edge. Refer to Figure 27-105-21)	10' to 50'	10' to 50'	15' to 50'
9	Off-Street Parking along Frontage	permitted	permitted	permitted
At Adjacent Lot Lines				
10	Minimum Side Building Setback & Space Between Buildings on Same Lot	15'; minimum 15' between buildings on same lot		
11	Minimum Rear Building Setback	30'		
FLOOR-TO-FLOOR HEIGHT Refer to Figure 27-105-7 General Building: Floor-to-Floor Height & Use Requirements and 27-105(c)(2) Floor-to-Floor Height.				
12	Ground Story: Minimum Height Maximum Height	14' 30' ¹	14' 24' ¹	14' 24' ¹
13	Ground Story Elevation	maximum of 2'-6" above the sidewalk without visible basement and a maximum of 4'-6" above the sidewalk with a visible basement		
USES Refer to Figure 27-105-7 General Building: Floor-to-Floor Height & Use Requirements and 27-105(c)(3) Uses within Building.				
14	Ground Story	office category, residential category, and retail sales and service uses limited to 25% of gross ground floor area, as permitted by district		limited to certain uses per 27-104(f) Uses
15	Upper Story	any permitted use		limited to certain uses per 27-104(f) Uses
16	Parking within Building	permitted fully in any basement and in rear of ground and upper floors; entrance to parking should be off an interior facade		
17	Required Occupied Space	30' deep from any Primary Street facade on all ground and upper stories		
STREET FACADE DESIGN REQUIREMENTS Refer to Figure 27-105-8 General Building: Street Facade Design Requirements and 27-105(c)(4) Street Facade Design Requirements				
18	Ground Story Fenestration Measured between 2' and 10' above grade	20% minimum	20% minimum	20% minimum
19	Entry Area	recessed minimum 3' from the facade closest to the street		
20	Required Number of Primary Street Entrances	1 per each 150' of front facade		
21	Ground Story Vertical Facade Divisions	every 150' of facade width		
22	Horizontal Facade Divisions	required within 3' of the top of the ground story, the top of the visible basement, and any 5th floor above the 1st floor		

Notes

¹ If 18 feet or more in height, ground story shall count as two stories towards maximum building height.

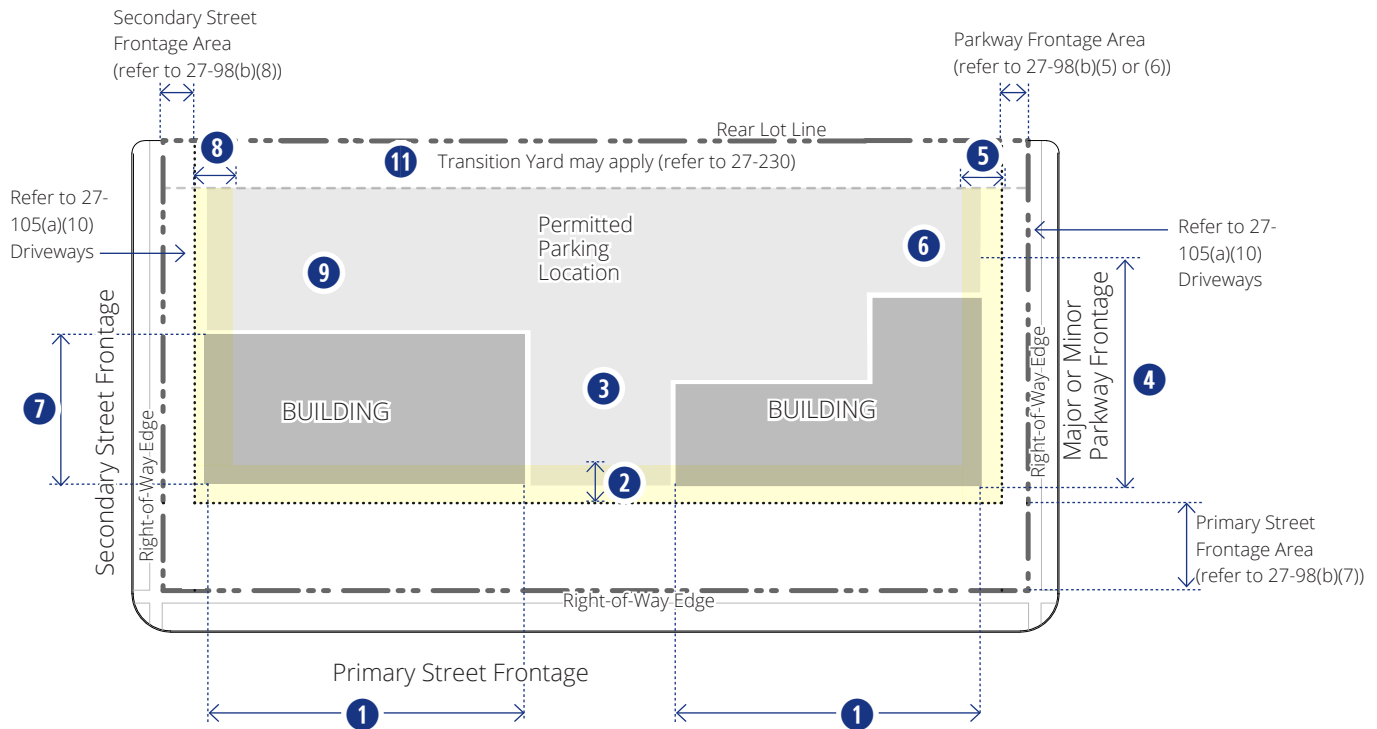


Figure 27-105-6 General Building: Building Siting

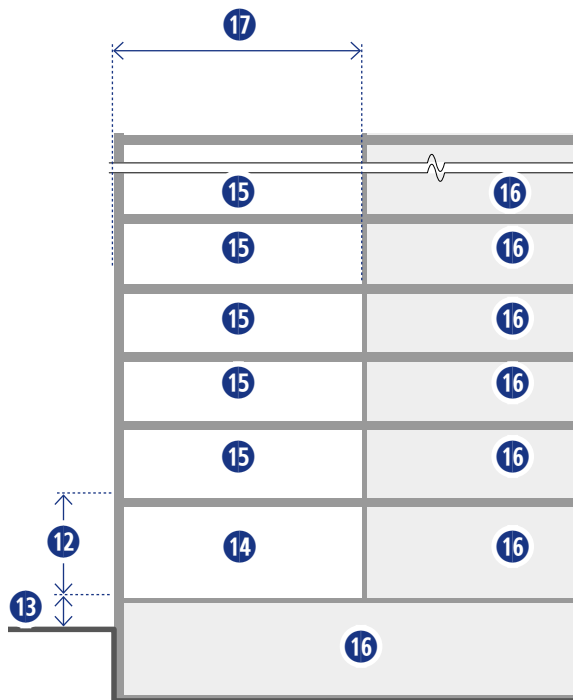


Figure 27-105-7 General Building: Floor-to-Floor Height & Use Requirements

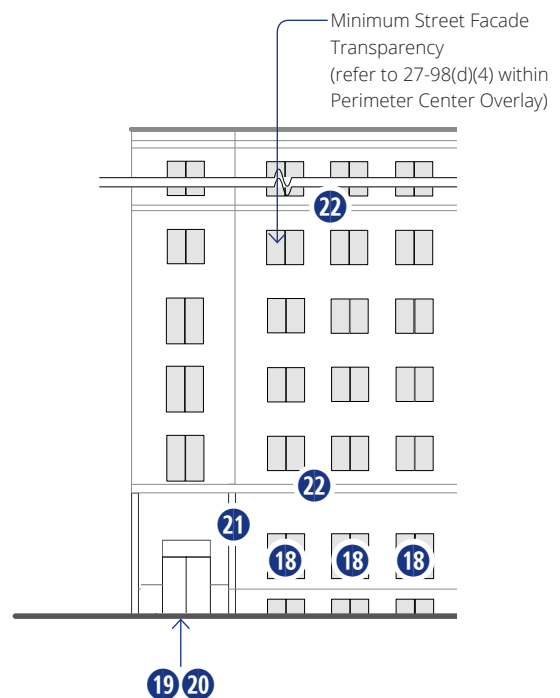


Figure 27-105-8 General Building: Street Facade Design Requirements

c. TOWNHOUSE BUILDING REGULATIONS

	PC-2 DISTRICT	PC-3 DISTRICT	PC-4 DISTRICT
BUILDING SITING ^{SEE NOTE 1} Refer to Figure 27-105-9 Townhouse Building: Building Siting and 27-105(c)(1) Building Siting by Street Types			
Along Primary Street Frontage Edge			
1	Minimum Build-to Zone Coverage ^{SEE NOTE 2}	65%	65%
2	Build-to Zone ^{SEE NOTE 2} (Measured from Street Type Frontage edge. Refer to Figure 27-105-21)	10' to 30'	15' to 30'
3	Off-Street Parking along Frontage	not permitted	not permitted
Along Parkway Frontage Edge			
4	Minimum Build-to Zone Coverage ^{SEE NOTE 2}	50%	50%
5	Build-to Zone ^{SEE NOTE 2} (Measured from Street Type Frontage edge. Refer to Figure 27-105-21)	25' to 50'	25' to 50'
6	Off-Street Parking along Frontage	permitted	
Along Secondary Street Frontage Edge			
7	Minimum Build-to Zone Coverage	none required	
8	Setback (Measured from Street Type Frontage edge. Refer to Figure 27-105-21)	15'	15'
9	Off-Street Parking along Frontage	permitted	
At Adjacent Lot Lines & Between Buildings			
10	Minimum Side Building Setback & Space Between Buildings on Same Lot	15'	
11	Minimum Rear Building Setback	30'; 10' for accessory garage building	
FLOOR-TO-FLOOR HEIGHT ³ Refer to Figure 27-105-10 Townhouse Building: Floor-to-Floor Height & Use Requirements and 27-105(c)(2) Floor-to-Floor Height.			
12	All Stories: Minimum Height Maximum Height Measured floor-to-floor	9' 14'	9' 14'
13	Ground Story Elevation	maximum of 2'-6" above the sidewalk without a visible basement and a maximum of 4'-6" above the sidewalk with a visible basement	
USES Refer to Figure 27-105-10 Townhouse Building: Floor-to-Floor Height & Use Requirements and 27-105(c)(3) Uses within Building.			
14	All Stories	attached house only	
15	Parking within Building	permitted fully in any basement and in rear of ground and upper floors; garage doors and access to structured parking should be off an interior side or rear facade (not a street facade), except parking may be either front or rear loaded from an internal driveway	
16	Required Occupied Space	30' deep from any Primary Street facade on all ground and upper stories	
17	Required Open Space	A minimum of 400 square feet of open space is required per unit, in the form of a rear yard space, courtyard, or open space type, within 100 feet of the building.	
STREET & COURTYARD FACADE DESIGN REQUIREMENTS Refer to Figure 27-105-11 Townhouse Building: Street Facade Design Requirements and 27-105(c)(4) Street Facade Design Requirements			
18	Ground Story Fenestration	15% minimum	15% minimum
19	Entry Area	Entrance shall be off a stoop or a porch. Refer to 27-621 Terms Defined. The porch shall be a minimum of 5 feet deep and 8 feet wide. The stoop shall be a minimum of 3 feet deep and 5 feet wide.	
20	Required Number of Street Entrances	1 per unit ²	1 per unit ²
21	Ground Story Vertical Facade Divisions	No more than two side by side units may share the same façade plane and then a 2' offset is required.	
22	Horizontal Facade Divisions	not required	

Notes

¹ For the purposes of the Townhouse building, a building consists of multiple units; minimum of 3 and maximum of 8 units.

² Each unit shall have a front facade fully located in a primary street build-to zone, except one of every five units may shall front a courtyard with a minimum width of 30 feet or a secondary street. The courtyard shall be defined on three sides by units.

³ A Townhouse building shall be a minimum of 2 stories and a maximum of 4 stories, independent of the district requirements.

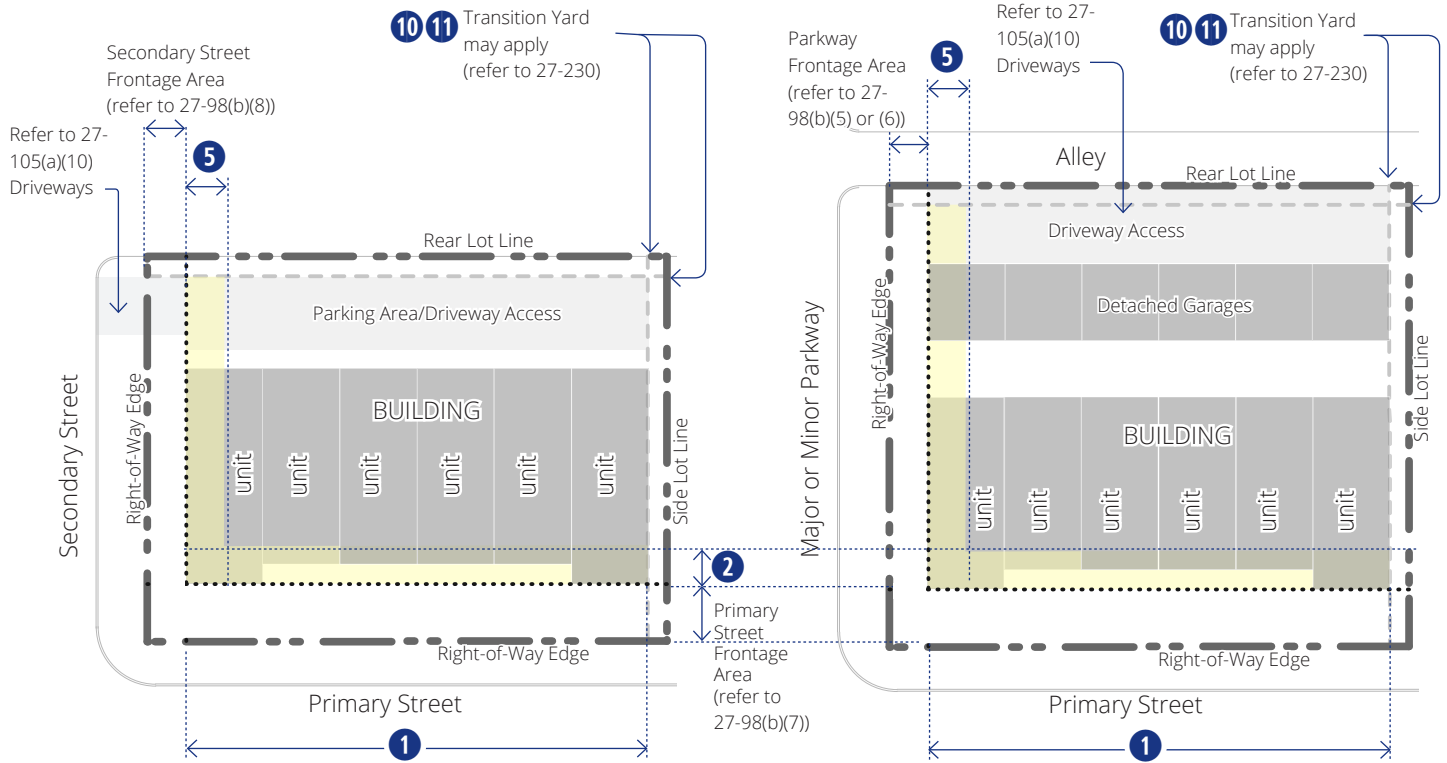


Figure 27-105-9 Townhouse Building: Building Siting

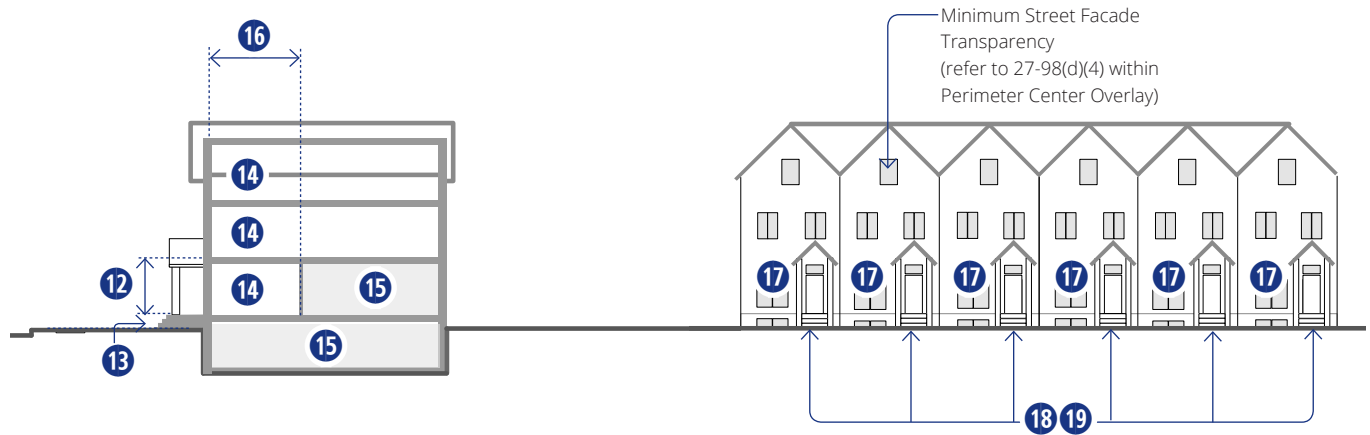


Figure 27-105-10 Townhouse Building: Floor-to-Floor Height & Use Requirements

Figure 27-105-11 Townhouse Building: Street Facade Design Requirements

d. DETACHED HOUSE BUILDING REGULATIONS

PC-4 DISTRICT

BUILDING SITING Refer to Figure 27-105-12 Detached House Building: Building Siting and 27-105(c)(1) Building Siting by Street Types

Along Primary Street Frontage Edge		
1	Minimum Build-to Zone Coverage ¹	65%
2	Build-to Zone ¹ (Measured from Street Type Frontage edge. Refer to Figure 27-105-21)	15' to 30'
3	Garage Entrance or Off-Street Parking along Frontage	not permitted
Along Parkway Frontage Edge		
4	Minimum Build-to Zone Coverage ¹	50%
5	Build-to Zone ¹ (Measured from Street Type Frontage edge. Refer to Figure 27-105-21)	25' to 50'
6	Garage Entrance or Off-Street Parking along Frontage	permitted, preferred off alley or lane
Along Secondary Street Frontage Edge		
7	Minimum Build-to Zone Coverage	none required
8	Setback (Measured from Street Type Frontage edge. Refer to Figure 27-105-21)	15'
9	Garage Entrance or Off-Street Parking along Frontage	permitted, preferred off alley or lane
At Adjacent Lot Lines		
10	Minimum Side Building Setback & Space Between Buildings on Same Lot	10'; minimum 15' between buildings on the same lot
11	Minimum Rear Building Setback	40'; 10' for accessory garage building

FLOOR-TO-FLOOR HEIGHT Refer to Figure 27-105-13 Detached House Building: Floor-to-Floor Height & Use Requirements and 27-105(c)(2) Floor-to-Floor Height.

12	All Stories: Minimum Height Maximum Height Measured floor-to-floor	9' 14'
13	Ground Story Elevation	maximum of 2'-6" above the sidewalk without a visible basement and a maximum of 4'-6" above the sidewalk with a visible basement

USES Refer to Figure 27-105-13 Detached House Building: Floor-to-Floor Height & Use Requirements and 27-105(c)(3) Uses within Building.

14	All Stories	household living
15	Parking within Building	permitted fully in any basement and in rear of ground and upper floors
16	Required Occupied Space	30' deep from any Primary Street facade on all ground and upper stories

STREET FACADE DESIGN REQUIREMENTS Refer to Figure 27-105-14 Detached House Building: Street Facade Design Requirements and 27-105(c)(4) Street Facade Design Requirements

17	Ground Story Fenestration Measured between 2' and 10' above grade	15% minimum
18	Entry Area	Entrance shall be off a stoop or a porch. Refer to 27-621 Terms Defined. The porch shall be a minimum of 5 feet deep and 8 feet wide. The stoop shall be a minimum of 3 feet deep and 5 feet wide.
19	Required Number of Street Entrances	1 per unit ¹
20	Ground Story Vertical Facade Divisions	not required
21	Horizontal Facade Divisions	not required

Notes

¹ Each building shall have a facade fully located in a primary street build-to zone, except one of every five units may front a courtyard with a minimum width of 30 feet or a secondary street. The courtyard shall be defined on three sides by units.

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27-105 Building Types

Detached House Building Regulations

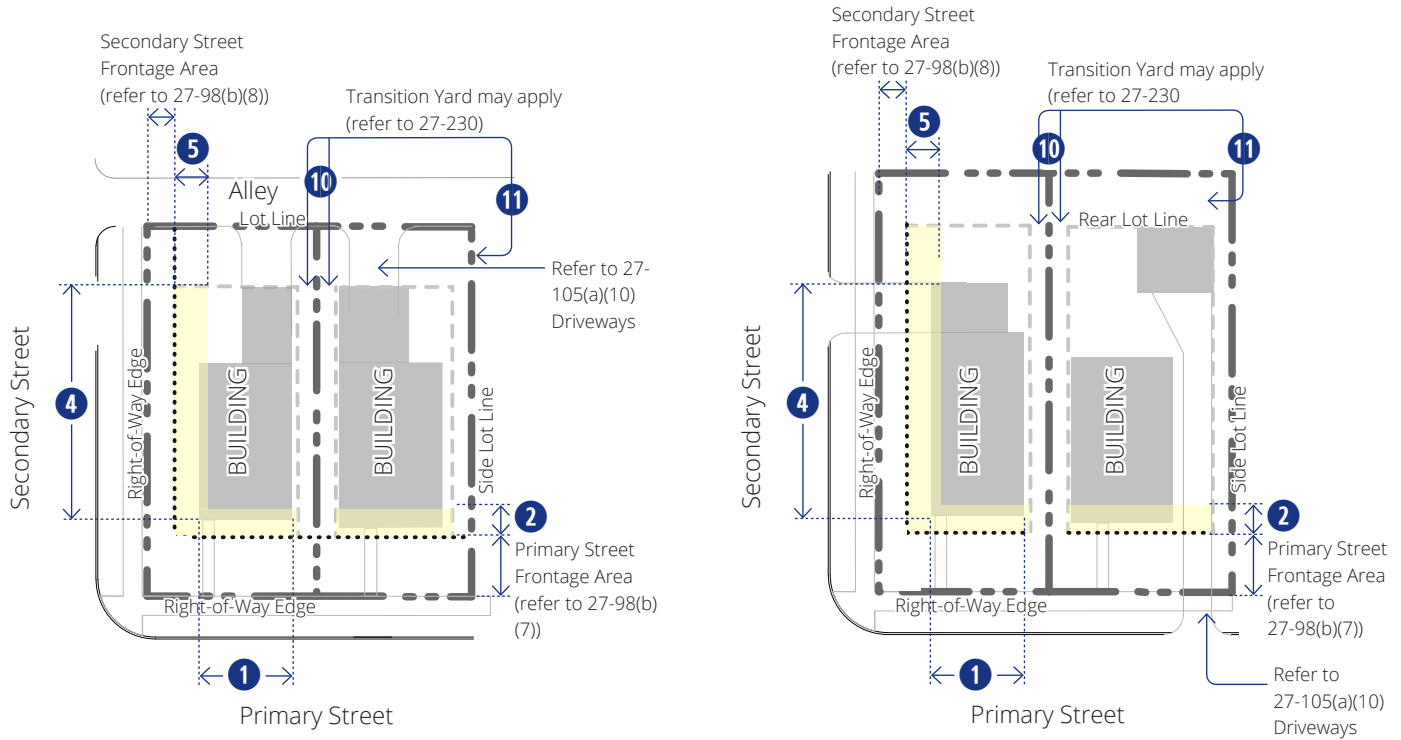


Figure 27-105-12 Detached House Building: Building Siting

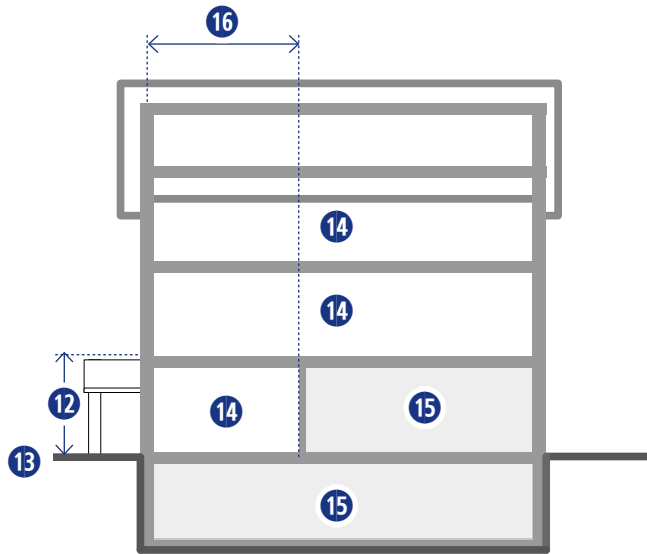


Figure 27-105-13 Detached House Building: Floor-to-Floor Height & Use Requirements

Minimum Street Facade Transparency (refer to 27-98(d)(4) within Perimeter Center Overlay)

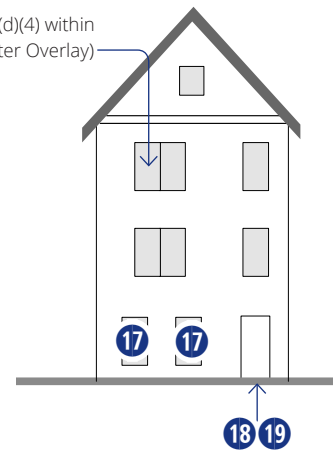


Figure 27-105-14 Detached House Building: Street Facade Design Requirements

e. CIVIC BUILDING REGULATIONS

PC-1 DISTRICT					PC-2 DISTRICT		PC-3 DISTRICT		PC-4 DISTRICT			
BUILDING SITING Refer to Figure 27-105-15 Civic Building: Building Siting and 27-105(c)(1) Building Siting by Street Types												
Along Primary Street Frontage Edge												
1	Minimum Build-to Zone Coverage				n/a							
2	Setback (Measured from Street Type Frontage edge. Refer to Figure 27-105-21)				15'							
3	Off-Street Parking along Frontage				maximum of one double-loaded aisle, perpendicular to street							
Along Parkway Frontage Edge												
4	Minimum Build-to Zone Coverage				n/a							
5	Setback (Measured from Street Type Frontage edge. Refer to Figure 27-105-21)				15'							
6	Off-Street Parking along Frontage				Permitted							
Along Secondary Street Frontage Edge												
7	Minimum Build-to Zone Coverage				n/a							
8	Setback (Measured from Street Type Frontage edge. Refer to Figure 27-105-21)				15'							
9	Off-Street Parking along Frontage				Permitted							
At Adjacent Lot Lines												
10	Minimum Side Building Setback & Space Between Buildings on Same Lot				15'		30'		30'		30'	
11	Minimum Rear Building Setback				30'		30'		30'		30'	
FLOOR-TO-FLOOR HEIGHT Refer to Figure 27-105-16 Civic Building: Floor-to-Floor Height & Use Requirements and 27-105(c)(2) Floor-to-Floor Height.												
12	Ground Story: Minimum Height Maximum Height Measured floor-to-floor				14' 30'							
USES Refer to Figure 27-105-16 Civic Building: Floor-to-Floor Height & Use Requirements and 27-105(c)(3) Uses within Building.												
13	All Stories				Limited to civic subcategory of uses							
14	Parking within Building				permitted fully in any basement and in rear of ground and upper floors; entrance to parking should be off an interior facade							
15	Required Occupied Space				30' deep from any Primary Street facade on all ground and upper stories							
STREET FACADE DESIGN REQUIREMENTS Refer to Figure 27-105-17 Civic Building: Street Facade Design Requirements and 27-105(c)(4) Street Facade Design Requirements												
16	Ground Story Fenestration Measured between 2' and 10' above grade				20% minimum							
17	Entry Area				Entrance shall be off a stoop or a porch. Refer to 27-621 Terms Defined. The porch shall be a minimum of 5 feet deep and 8 feet wide. The stoop shall be a minimum of 3 feet deep and 5 feet wide.							
18	Entrance Elevation				maximum of 2'-6" above the sidewalk without visible basement; maximum of 4'-6" above the sidewalk with a visible basement							
19	Required Number of Primary Street Entrances				one							
20	Ground Story Vertical Facade Divisions				none required							
21	Horizontal Facade Divisions				none required							

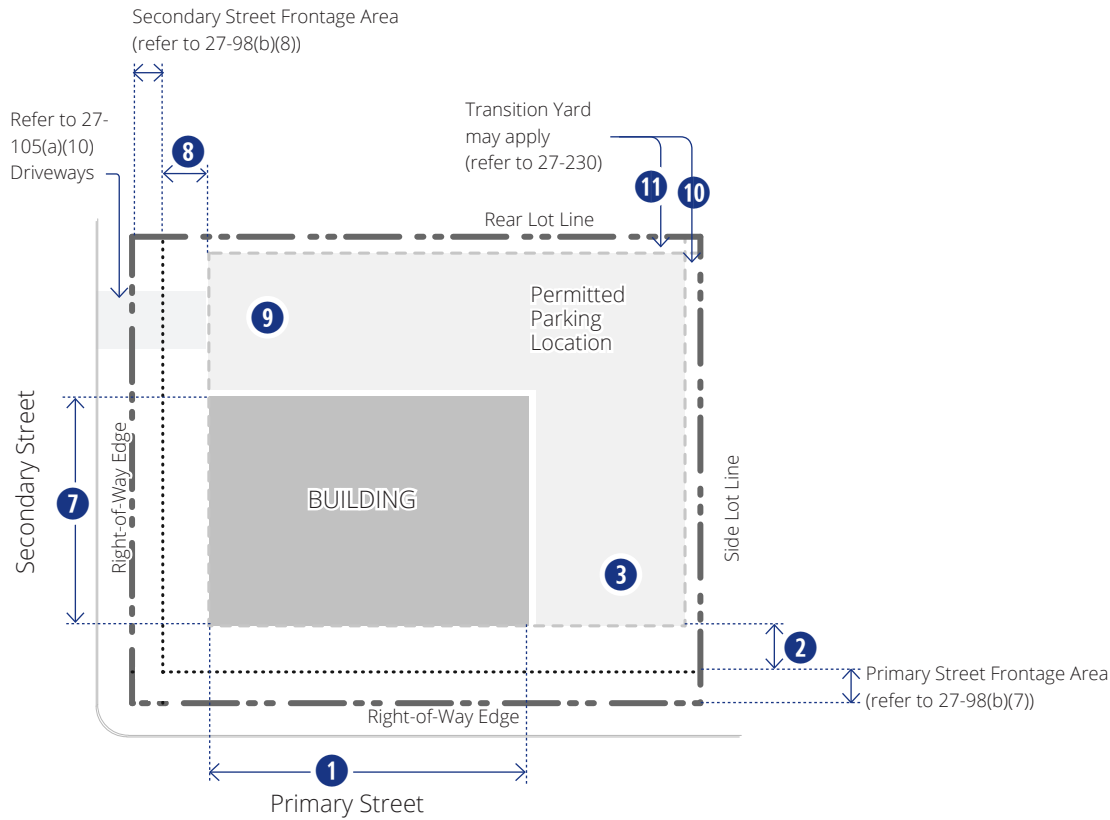


Figure 27-105-15 Civic Building: Building Siting

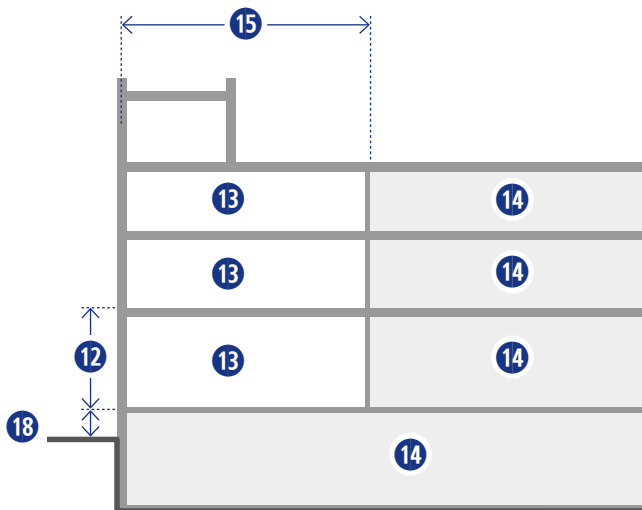


Figure 27-105-16 Civic Building: Floor-to-Floor Height & Use Requirements

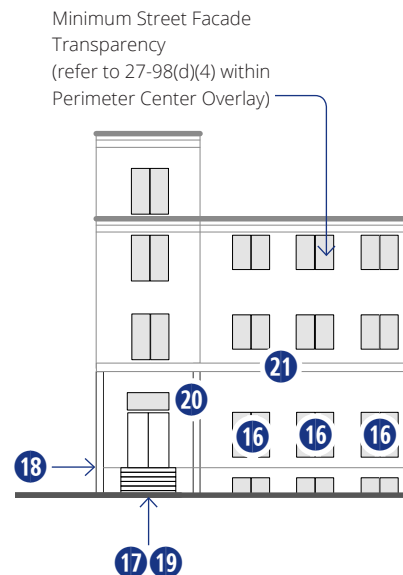


Figure 27-105-17 Civic Building: Street Facade Design Requirements

(c) Explanation of Specific Building Type Requirements

The following explains and further defines the standards outlined on the tables on the previous pages, specific to each Building Type, refer to 27-105(b)(2)a through 27-105(b)(2)e.

(1) Building Siting by Street Types

The following explains the line item requirements for each Building Type Table within the section entitled "Building Siting."

For each Building Type, requirements are defined for each Street Type frontage that may occur surrounding the site. Refer to Figure 27-98-2 Street Types Map for the type defined for each street in the Perimeter Center area. Each Street Type is defined with frontage options in 27-98(b) Street Types, including the Major and Minor Parkway, Primary Street, and the Secondary Street.

a. Build-to Zone Coverage. The minimum percentage of building wall or facade along the street for each Street Type (Parkway, Primary Street, Secondary Street per Figure 27-98-2 Street Types Map) is designated on each Building Type table. Refer to 27-105(b)(2)a through 27-105(b)(2)e.

1. **Measurement.** The width of the principal structures (as measured within the build-to zone along the frontage edge) is divided by the length of the frontage parallel to the property line following the street. Refer to Figure 27-105-18 Minimum Build-to Zone Coverage.
2. **Courtyards.** Courtyards, per 27-621 Terms Defined, located along the facade in the build-to zone count towards the minimum coverage. Refer to Street Types for limitations of Courtyards along some frontages.

b. Build-to Zone. The build-to zone is designated separately for each Street Type frontage on each Building Type table. Refer to Figure 27-105-1 Corner and Build-to Zones on Different Street Types and Figure 27-98-2 Street Types Map.

1. **Measurement.** The build-to zone for all districts and Street Type frontages is measured from the edge of the Street Type frontage area onto the site, along the frontage edge. Refer to Figure 27-105-19 Location of Build-to Zone in relation to Frontage.
2. **Encroachments.** Awnings and building mounted signage may extend beyond the build-to zone into the frontage area, but may not extend into the street right-of-way.
3. **Porches, Arcades, Galleries.** Porches, arcades and galleries may be included on the building facade design, utilizing the range of depth permitted by the build-to zone. If located outside the building coverage requirement, the depth of the arcade or gallery may exceed the build-to zone.

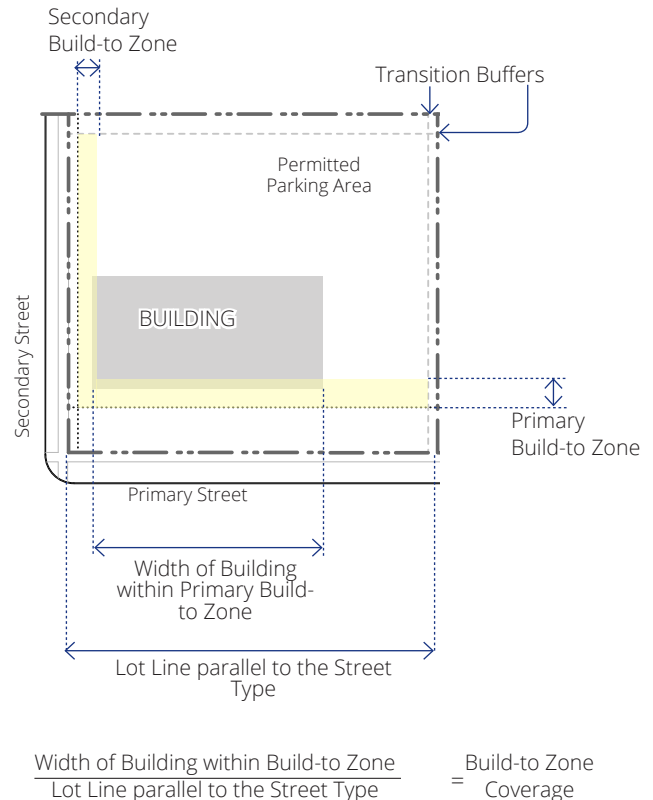


Figure 27-105-18 Minimum Build-to Zone Coverage

27-105 Building Types

Explanation of Specific Building Type Requirements

c. Off-Street Parking along Frontage. Off-street parking is permitted behind any buildings on the site, but is only permitted exposed along a frontage if designated on the table of requirements for each Building Type.

1. **Side Parking.** Where noted, side off-street parking along a Primary Street may be permitted with a maximum of one double loaded aisle of parking perpendicular to the street centerline and with a maximum width of 72 feet. Refer to the Street Type frontage parking edge for design requirements in front of the parking and Figure 27-105-20 Side Off-Street Parking.

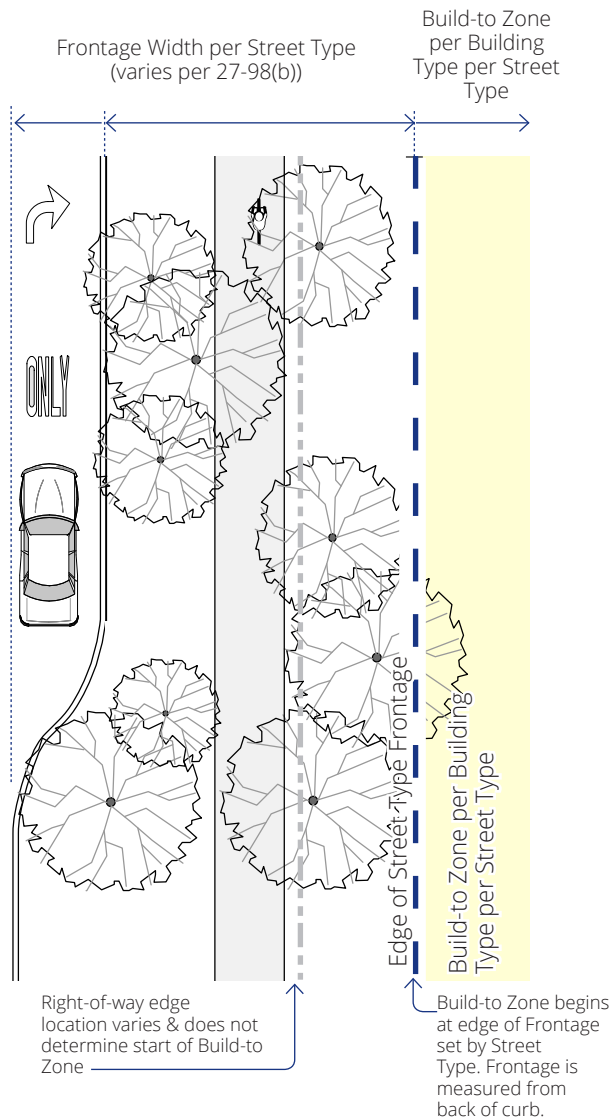


Figure 27-105-19 Location of Build-to Zone in relation to Frontage

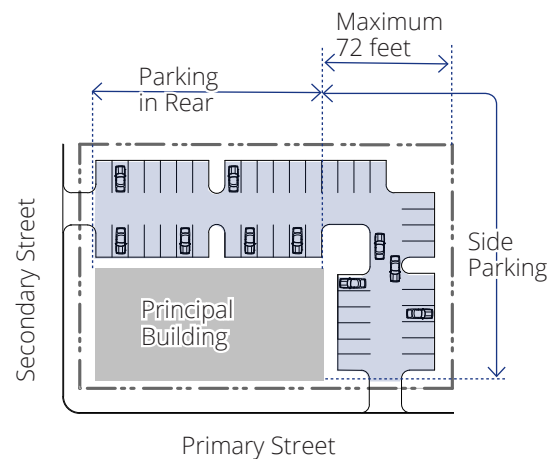


Figure 27-105-20 Side Off-Street Parking

27-105 Building Types

Explanation of Specific Building Type Requirements

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(2) Floor-to-Floor Height

The following explains the line item requirements for each Building Type Table within the section entitled "Floor-to-Floor Height."

Overall permitted building heights are designated by District in stories. Refer to 27-104 Districts.

a. Ground Story, Minimum and Maximum Height.

(Refer to Figure 27-105-21 Measuring Story Height). Each Building Type indicates a permitted range of height in feet for the ground story of the building.

1. **Taller Spaces.** Spaces exceeding the allowable floor-to-floor heights of the Building Type are not permitted on Primary Street facades. These spaces are unlimited on interior lot and Secondary Street facades, and limited to no more than 50 percent of Major or Minor Parkway facades.

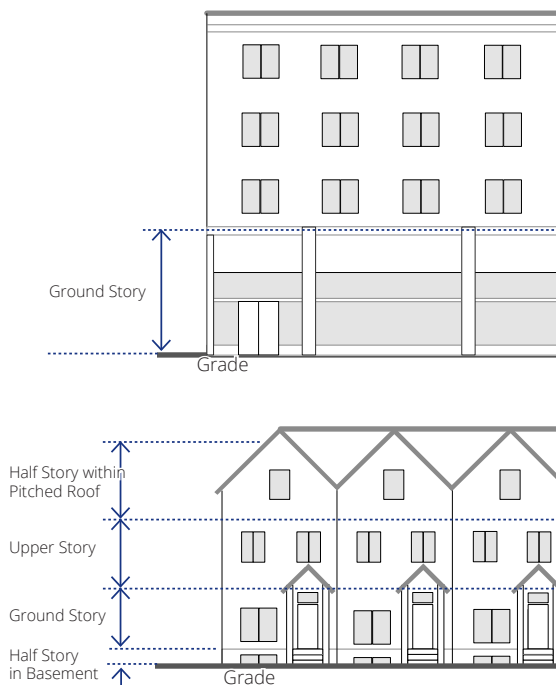


Figure 27-105-21 Measuring Story Height

- b. **Ground Story Elevation.** The allowable ground story elevation for each Building Type is defined, establishing whether or not a visible basement is permitted.

1. **Visible Basements.** Visible basements, permitted by entrance type, are optional. The visible basement shall be a maximum of one-half the height of the tallest story.

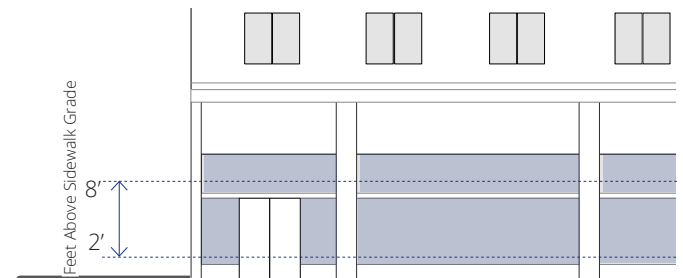


Figure 27-105-22 Measuring Ground Story Windows



Figure 27-105-23 Measuring Fenestration per Facade

(3) Uses within Building

The following explains the line item requirements for each Building Type Table within the section entitled "Uses within Building." Refer to 27-104(f) Uses for uses permitted within each Zoning District. The requirements in this section of the Building Type Tables may limit those uses within a specific Building Type.

- a. Ground and Upper Story.** The uses or category of uses which may occupy the ground and/or upper story of a building.
- b. Parking Within Building.** The area(s) of a building in which parking is permitted within the structure.
- c. Required Occupied Space.** The required depth of Occupied Space from the noted facade(s). Refer to 27-621 Terms Defined for the definition of Occupied Space.

(4) Street Facade Design Requirements

The following explains the line item requirements for each Building Type within the section entitled "Street Facade Requirements".

These requirements apply only to facades facing public or private streets and facades facing main parking lots where visitors or customers park.

- a. Minimum Fenestration.** (Refer to Figure 27-105-23 Measuring Fenestration per Facade). The following articulates the minimum amount of ground story transparent glass required on facades facing street and parking facades.
 - 1. **Measurement.** Ground Story windows, when defined separately from the overall minimum fenestration, shall be measured between 2 feet and 8 feet from the average grade at the base of the front facade. Refer to Figure 27-105-22 Measuring Ground Story Windows.
 - 2. **Transparent.** Transparent means any glass in windows and/or doors, including any mullions, that is highly transparent with low reflectance.
 - 3. **Other Stories.** A general minimum fenestration requirement applies to all other stories visible from any street. Refer to 27-98(d)(4)a Quantity of Windows.
 - 4. **Arcades, Galleries, Courtyards.** Ground story windows shall be located on the

interior facade of any arcade or gallery. Ground story windows shall continue around a minimum of 60 percent of courtyard facades.

- b. Entry Area.** Size requirements and recommended types of entrance areas per Building Type.
- c. Required Number of Street Entrances.** The minimum number of and maximum spacing between entrances on the ground floor building facade with Primary Street frontage.
- d. Vertical Facade Divisions.** The use of a vertically oriented expression line or form to divide the facade into vertical divisions at increments no greater than the dimension shown, as measured along the base of the facade. Elements may include a column, pilaster, or other continuous vertical ornamentation a minimum of 1.5 inch depth. Refer to 27-621 Terms Defined for the definition of expression line.
- e. Horizontal Facade Divisions.** The use of a horizontally oriented expression line or form to divide portions of the facade into horizontal divisions at locations designated. Elements may include a cornice, belt course, molding, string courses, or other continuous horizontal ornamentation a minimum of 1.5 inch depth.

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27-106 Open Space Types

SECTIONS

- (a) Intent
- (b) Applicability
- (c) General Requirements
- (d) Stormwater in Open Space Types
- (e) Plaza
- (f) Green
- (g) Commons
- (h) Park

(a) Intent

To provide open space as an amenity that promotes physical and environmental health within the community and to provide access to a variety of active and passive open space types.

(b) Applicability

The standards outlined apply to open space required per Perimeter Center district (refer to 27-104(b) through 27-104(e)).

(1) Existing Open Space. At the discretion of the city, existing open space on the site may be used to meet the minimum requirement; however, the existing open space shall conform to one of the types defined.

(2) Rooftop Open Space. Rooftop terraces or open space shall not count towards required open space.

(3) Fee in-lieu. Open space requirements of this section must be met by open space provided on the development site, unless off-site open space or a fee in lieu of open space provision is approved in accordance with the special administrative permit process.

(c) General Requirements

All open space shall meet the following requirements.

(1) Open Space Types. All open space provided within any Perimeter Center District shall comply with one of the Open Space Types defined by 27-106(e) through 27-106(h).

(2) Access. All Open Space Types shall provide public access from a pedestrian route associated with a vehicular right-of-way and/or adjacent building entrances/exits.

(3) Dedicated. Open Space Types shall be designated by easement or, with permission of the City, may be either located within the right-of-way or dedicated by plat as a separate lot.

(4) Fencing. Open Space Types may incorporate fencing provided that the following requirements are met.

- a. Height.** Fencing shall be a maximum height of 48 inches, unless approved by the community development director for such circumstances as proximity to railroad right-of-way and use around athletic facilities.
- b. Level of Opacity.** Fence opacity shall be no greater than 60 percent.
- c. Type.** Chain-link fencing is not permitted along any street frontage, with the exception of dedicated sports field or court fencing approved by the community development director.
- d. Spacing of Openings.** Openings or gates shall be provided on every street face at a minimum of one per every 200 feet.

(5) Open Water Body. All open water bodies, such as lakes, ponds, pools, creeks, and streams, within an Open Space Type shall be located at least 20 feet from a property line to allow for pedestrian and bicycle access as well as a landscape area surrounding the water body.

(6) Parking Requirements. Parking shall not be

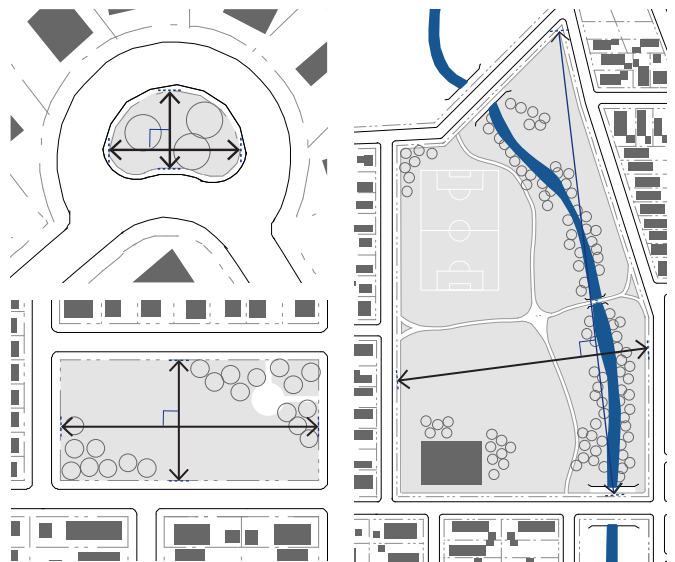


Figure 27-106-1 Measuring Minimum Dimensions

required for any Open Space Type, unless otherwise determined by the community development director.

(7) Continuity. New open space shall connect to abutting, or proximate existing or planned trail right-of-way.

(8) Measuring Size.

- a. Size.** The size of the Open Space Type is measured to include all landscape and paving areas associated directly with the Open Space Type.
- b. Minimum Dimension.** The minimum length or width of the Open Space Type, as measured along the longest two straight lines intersecting at a right angle defining the maximum length and width of the lot. Refer to Figure 27-106-1 Measuring Minimum Dimensions.

(9) Minimum Percentage of Street Frontage

Required. A minimum percentage of the open space perimeter, as measured along the outer edge of the space, shall be located directly adjacent to a Parkway, Primary Street, or Secondary Street. This requirement provides access and visibility to the open space.

(10) Improvements. As noted in the specific requirements for each Open Space Type (27-106(e) through 27-106(h)), the following types of site improvements and structures may be permitted on an Open Space Type.

- a. Fully Enclosed Structures Permitted.** Fully enclosed structures may include such uses as small cafes, kiosks, community centers, and restrooms.
 - (1) **Maximum Area.** For some civic open space types, fully enclosed structures are permitted, but limited to a maximum building coverage as a percentage of the open space area.
 - (2) **Semi-Enclosed Structures.** Open-air structures, such as gazebos, are permitted in all open space types.
- b. Maximum Impervious and Semi-Pervious Surface Permitted.** The amounts of impervious and semi-pervious coverage are provided separately for each Open Space Type (27-106(e) through 27-106(h)) to allow an additional amount of semi-pervious surface, such as permeable paving, above the impervious surfaces permitted, including, but not limited to, sidewalks, paths, and structures as permitted.

c. Maximum Percentage of Open Water Body.

The maximum amount of area within an Open Space Type that may be covered by an open water body, including, but not limited to, ponds, lakes, and pools.

(d) Stormwater in Open Space Types

Stormwater management practices, such as normally dry storage and retention facilities or ponds maintaining water at all times, may be integrated into Open Space Types and utilized to meet stormwater requirements for surrounding parcels.

(1) Stormwater Features. Stormwater features in open space may be designed as formal or natural amenities with additional uses other than stormwater management, such as an amphitheater, sports field, or a pond or pool as part of the landscape design. Stormwater features shall not be fenced and shall not impede public use of the land they occupy.

(2) Walls. Retaining walls over 30 inches in height are not permitted in any open space accommodating stormwater.

(3) Qualified Professional. A qualified design professional shall be utilized to incorporate the stormwater features into the design of the open spaces.

#E.1.

27-106 Open Space Types

Plaza

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(e) Plaza

The intent of the plaza is to provide a formal Open Space Type of medium scale to serve as a gathering place for civic, social, and commercial purposes. The Plaza may contain a greater amount of impervious coverage than any other Open Space Type. Special features, such as fountains and public art installations, are encouraged.

PLAZA CRITERIA	
Dimensions	
Minimum Size	0.10 acres
Minimum Dimension	60 feet
Minimum Percentage of Street Frontage Required	25%
Improvements	
Fully Enclosed Structures	Permitted; may cover maximum 5% of plaza area
Maximum Impervious Surface + Semi-Pervious Surface	40% + 20%
Maximum Percentage of Open Water & Stormwater Features	30%



(f) Green

The intent of the green is to provide informal, medium scale active or passive recreation for building occupants and visitors within walking distance, mainly fronted by streets.

GREEN CRITERIA	
Dimensions	
Minimum Size	0.50 acres
Minimum Dimension	45 feet
Minimum Percentage of Street Frontage Required	25%
Improvements	
Fully Enclosed Structures	Not permitted
Maximum Impervious Surface + Semi-Pervious Surface	20% + 15%
Maximum Percentage of Open Water & Stormwater Features	30%

27-106 Open Space Types

Commons

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(g) Commons

The intent of the commons is to provide an informal, small to medium scale space for active or passive recreation for a limited area. Commons are typically internal to a block and tend to serve adjacent building occupants.



(h) Park

The intent of the park is to provide informal active and passive large-scale recreational amenities to local residents and the greater region. Parks have primarily natural plantings and are frequently created around an existing natural feature such as a water body or stands of trees.

COMMONS CRITERIA

Dimensions

Minimum Size	0.25 acres
Minimum Dimension	45 feet
Minimum Percentage of Street Frontage Required	0%; requires a minimum of two access points with a total depth of 100 feet maximum (minimum 30 feet wide)

Improvements

Fully Enclosed Structures	Not permitted
Maximum Impervious Surface + Semi-Pervious Surface	30% + 10%
Maximum Percentage of Open Water & Stormwater Features	30%

PARK CRITERIA

Dimensions

Minimum Size	1 acre
Minimum Dimension	30 feet; minimum average width of 80 feet
Minimum Percentage of Street Frontage Required	30% for parks less than 5 acres; 20% for parks 5 or more acres in size

Improvements

Fully Enclosed Structures	Permitted in parks 2 acres or larger in size
Maximum Impervious Surface + Semi-Pervious Surface	20% + 10%
Maximum Percentage of Open Water & Stormwater Features	30%

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ARTICLE II. - ZONING DISTRICTS

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DIVISION 3. - SPECIAL PURPOSE ZONING DISTRICTS

Sec. 27-86. - ~~PC, Perimeter Center district. Reserved~~

~~RESERVED~~

~~(Ord. No. 2013-10-15, § 1(Exh. A § 27-6.10), 10-14-2013)~~

Sec. 27-87. - PD, Planned Development district.

...

(b) *Establishing a planned development.*

- (1) No PD district may be established without the concurrent approval of an overall development plan (ODP) by the mayor and city council, in accordance with subsection (c).
- (2) PD districts must have a minimum contiguous land area of 200,000 square feet. ~~ten acres if located within the boundaries of an adopted master plan. PD districts outside the boundaries of an adopted master plan must have a minimum contiguous area of 15 acres.~~

DIVISION 4. – OVERLAY ZONING DISTRICTS

...

Sec. 27-98. – Perimeter Center Overlay.¹

Secs. 27-98—27-~~110~~102. - Reserved.

DIVISION 5. – PERIMETER CENTER DISTRICTS²

Secs. 27-103—27-106 – Reserved for PC District Provisions

Secs. 27-107—27-110. - Reserved.

...

¹ Proposed new overlay district provisions presented in separate attached document (located here upon codification).

² Proposed districts presented in separate attached document. New district regulations will be located here upon codification.

ARTICLE IV. - GENERALLY APPLICABLE REGULATIONS

DIVISION 1. - PARKING AND CIRCULATION³

...

Sec. 27-202. - Minimum motor vehicle and bicycle parking ratios.

Except as otherwise expressly stated, off-street motor vehicle and bicycle parking spaces must be provided in accordance with the following table, provided that:

- (a) no use is required to provide more than 8 bicycle parking spaces, and
- (b) the community development director is authorized to reduce minimum bicycle parking ratios for multi-tenant centers and other sites occupied by multiple uses when the director determines that common bike rack locations and overall bike parking availability will be adequate to meet bicycle parking demands on the subject lot;

Minimum Off-Street Parking Ratios		
USES	Motor Vehicle Parking	Bicycle Parking
RESIDENTIAL		
Household Living		
Detached house	2 spaces per dwelling unit	<u>None</u>
Attached house	2 spaces per dwelling unit	<u>None</u>
Multi-unit building	1 space per dwelling unit + 1 additional space per bedroom for 2+ bedroom units + 1 visitor space per 8 units	<u>0.1 spaces per dwelling unit; min. 2 spaces</u>
Multi-unit building (age-restricted 62 years+)	1 space per dwelling unit + 1 visitor space per 8 units	<u>0.05 spaces per dwelling unit; min. 2 spaces</u>
Group Living		
Convent and monastery	5 spaces per 1,000 sq. ft.	<u>None</u>
Fraternity house or sorority house	1 space per bed	<u>None</u>
Nursing home	1 space per 2 beds	<u>None</u>
Personal care home, registered (1—3 persons)	4 spaces	<u>None</u>
Personal care home, family (4—6 persons)	4 spaces	<u>None</u>
Personal care home, group (7—15 persons)	4 spaces	<u>None</u>
Personal care home, congregate (16 or more)	1 space per 4 clients	<u>None</u>
Residence hall	0.25 spaces per sleeping room	<u>0.1 spaces per sleeping room; min. 8 spaces</u>
Shelter, homeless	1 space per 10-person capacity	<u>None</u>
Supportive living	1 space per 2 living units	<u>None</u>
Transitional housing facility	1 space per 4 beds	<u>None</u>

³ Includes only those sections with proposed revisions.

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Minimum Off-Street Parking Ratios		
USES	Motor Vehicle Parking	Bicycle Parking
QUASI-PUBLIC AND INSTITUTIONAL		
Ambulance Service	1 space per service vehicle plus 1 space per 2 employees	<u>None</u>
Club or Lodge, Private	10 spaces per 1,000 sq. ft.	<u>None</u>
Cultural Exhibit	1 space per 3 fixed seats <u>0.33 spaces per seat</u> ; 40 spaces per 1,000 sq. ft. in largest assembly room if no fixed seats	<u>0.05 spaces per seat; min. 4 spaces</u>
Day Care		
Day care facility, adult (6 or fewer persons)	4 spaces	<u>None</u>
Day care center, adult (7 or more)	5 spaces per 1,000 sq. ft.	<u>None</u>
Day care facility, child (6 or fewer persons)	4 spaces	<u>None</u>
Day care center, child (7 or more)	5 spaces per 1,000 sq. ft.	<u>None</u>
Educational Services		
College or university	10 spaces per classroom	<u>None</u>
Kindergarten	5 spaces per 1,000 sq. ft.	<u>None</u>
Research and training facility, college or university affiliated	10 spaces per classroom	<u>None</u>
School, private elementary or middle	2 spaces per classroom	<u>Min. 4 spaces</u>
School, private senior high	5 spaces per classroom	<u>Min. 4 spaces</u>
School, specialized non-degree	10 spaces per classroom	<u>Min. 4 spaces</u>
School, vocational or trade	10 spaces per classroom	<u>Min. 4 spaces</u>
Hospital	1 space per 2 beds	<u>None</u>
Place of Worship	1 space per 3 fixed seats or 40 spaces per 1,000 sq. ft. in largest assembly room if there are no fixed seats	<u>0.05 spaces per seat; min. 4 spaces</u>
Utility Facility, Essential	As determined per subsection 27-203(6)	
COMMERCIAL		
Adult Use	13.33 spaces per 1,000 sq. ft.	<u>None</u>
Animal Services	3.3 spaces per 1,000 sq. ft.	<u>None</u>
Communication Services (except as noted below)	3.3 spaces per 1,000 sq. ft.	<u>None</u>
Telecommunication tower or antenna	None	<u>None</u>
Construction and Building Sales and Services	4 spaces per 1,000 [sq. ft.] of customer-accessible sales area + 3.3 spaces per 1,000 sq. ft. of office floor area + 0.5 spaces per 1,000 [sq. ft.] of additional indoor floor area	<u>None</u>
Eating and Drinking Establishments (<u>see also Sec. 27-204(f)</u>)		
Restaurant, accessory to allowed office or lodging use	6.67 spaces per 1,000 sq. ft.; <u>3.3 spaces per 1,000 sq. ft. for PC-zoned property⁴</u>	<u>None</u>

⁴ This could be tied to the PC overlay, but by tying it to PC base zoning, we provide added incentive to opt for PC zoning.

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USES	Minimum Off-Street Parking Ratios	
	Motor Vehicle Parking	Bicycle Parking
Restaurant, drive-in or drive-through	10 spaces per 1,000 sq. ft.; minimum 5 spaces	<u>Min. 4 spaces</u>
Restaurant, other than drive-through or drive-in	6.67 spaces per 1,000 sq. ft.; <u>3.3 spaces per 1,000 sq. ft. for PC-zoned property</u>	<u>Min. 4 spaces</u>
<u>Carry-out only restaurant</u>	<u>2 spaces per 1,000 sq. ft.</u>	<u>Min. 2 spaces</u>
Food truck	None <u>for temporary locations</u>	<u>None</u>
Other eating or drinking establishment	6.67 spaces per 1,000 sq. ft.; <u>3.3 spaces per 1,000 sq. ft. for PC-zoned property</u>	<u>Min. 2 spaces</u>
Entertainment and Spectator Sports (except as stated below)	1 space per 3 fixed seats or 40 spaces per 1,000 sq. ft. in largest assembly room if there are no fixed seats	<u>Min. 4 spaces</u>
Special events facility	10 spaces per 1,000 sq. ft.	<u>None</u>
Financial Services		
Banks, credit unions, brokerage and investment services	3.3 spaces per 1,000 sq. ft.; <u>2.5 spaces per 1,000 sq. ft. for PC-zoned property</u>	<u>0.33 spaces per 1,000 sq. ft.; min. 2 spaces</u>
Convenient cash business	4 spaces per 1,000 sq. ft.	<u>0.33 spaces per 1,000 sq. ft.; min. 2 spaces</u>
Pawn shop	4 spaces per 1,000 sq. ft.	<u>0.33 spaces per 1,000 sq. ft.; min. 2 spaces</u>
Food and Beverage Retail Sales	4 spaces per 1,000 sq. ft.; <u>2.5 spaces per 1,000 sq. ft. for PC-zoned property</u>	<u>0.1 spaces per 1,000 sq. ft.; min. 4 spaces</u>
Funeral and Interment Services	0.5 spaces per 1,000 sq. ft.	<u>None</u>
Cemetery, columbarium, or mausoleum	None (parking allowed on internal roads/drives)	<u>None</u>
Crematory	0.5 spaces per 1,000 sq. ft.	<u>None</u>
Funeral home or mortuary	1 space per 3 fixed seats or 40 spaces per 1,000 sq. ft. in largest assembly room if there are no fixed seats	<u>None</u>
Lodging	1.25 spaces per guest room; <u>1 space per guest room for PC-zoned property</u>	<u>None</u>
Medical Service		
Home health care service	3.3 spaces per 1,000 sq. ft.	<u>None</u>
Hospice	1 space per 2 beds	<u>None</u>
Kidney dialysis center	4 spaces per 1,000 sq. ft.	<u>None</u>
Medical and dental laboratory	3.3 spaces per 1,000 sq. ft.	<u>Min. 2 spaces</u>
Medical office/clinic	4 spaces per 1,000 sq. ft.	<u>Min. 2 spaces</u>
Office or Consumer Service	3.3 spaces per 1,000 sq. ft.; <u>2.5 spaces per 1,000 sq. ft. for PC-zoned property</u>	<u>Min. 2 spaces</u>
Parking, Non-accessory	N/A	<u>Min. 2 spaces</u>
Personal Improvement Service	4 spaces per 1,000 sq. ft.; <u>2.5 spaces per 1,000 sq. ft. for PC-zoned property</u>	<u>0.25 spaces per 1,000 sq. ft.; min. 2 spaces</u>
Repair or Laundry Service, Consumer	4 spaces per 1,000 sq. ft.; <u>2.5 spaces per 1,000 sq. ft. for PC-zoned property</u>	<u>0.25 spaces per 1,000 sq. ft.; min. 2 spaces</u>
Research and Testing Services	3.3 per 1,000 sq. ft.	
Retail Sales	4 spaces per 1,000 sq. ft. + 1 space per 1,000 sq. ft. of outdoor display/sales areas; <u>2.5 spaces per 1,000 sq. ft.</u>	<u>0.1 spaces per 1,000 sq. ft.; min. 4 spaces</u>

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Minimum Off-Street Parking Ratios		
USES	Motor Vehicle Parking	Bicycle Parking
	<u>ft. for PC-zoned property (see also “shopping center” requirements)</u>	
<u>Shopping Center [1]</u>	<u>0 to 400,000 sq. ft. = 4.5 spaces per 1,000 sq. ft.</u> <u>400,001 to 600,000 sq. ft. = 5.0 spaces per 1,000 sq. ft.</u> <u>600,001 or more sq. ft. = 5.5 spaces per 1,000 sq. ft.</u>	<u>0.1 spaces per 1,000 sq. ft.; min. 4 spaces (see also 27-202(b))</u>
Sports and Recreation, Participant		
Golf course and clubhouse, private	2 spaces per hole	<u>None</u>
Health club	<u>4 spaces per 1,000 sq. ft.; 2.5 spaces per 1,000 sq. ft. for PC-zoned property</u>	<u>0.25 spaces per 1,000 square feet; min. 6 spaces</u>
Private park	As determined per subsection 27-203(6)	
Recreation center or swimming pool, neighborhood	1 space per 5 members; minimum 10 spaces in R districts; minimum 20 spaces in nonresidential districts	<u>1 space per 20 adult members; min. 4 spaces</u>
Recreation grounds and facilities	As determined per subsection 27-203(6)	
Tennis center, club and facilities	1 space per 5 members; minimum 10 spaces in R districts; minimum 20 spaces in nonresidential districts	<u>1 space per 20 adult members; min. 4 spaces</u>
Other participant sports and recreation (Indoor)	5 spaces per 1,000 sq. ft.	<u>0.4 spaces per 1,000 square feet; min. 4 spaces</u>
Other participant sports and recreation (Outdoor)	As determined per subsection 27-203(6)	
Vehicle and Equipment, Sales and Service		
Car wash	Vehicle stacking spaces per section 27-211	<u>None</u>
Gasoline sales	3 spaces per service bay/stall	<u>None</u>
Vehicle repair, minor	3 spaces per service bay/stall	<u>None</u>
Vehicle repair, major	3 spaces per service bay/stall	<u>None</u>
Vehicle sales and rental	1 space per employee + 2 spaces per service bay/stall	<u>None</u>
Vehicle storage and towing	4 spaces + 1 per employee	<u>None</u>
INDUSTRIAL		
Manufacturing and Production, Light	0.5 spaces per 1,000 sq. ft.	<u>None</u>
Wholesaling, Warehousing and Freight Movement	0.5 spaces per 1,000 sq. ft.	<u>None</u>
AGRICULTURE AND TRANSPORTATION		
Agriculture		
Agricultural produce stand	None	<u>None</u>
Community garden	None	<u>None</u>
Crops, production of	None	<u>None</u>
TRANSPORTATION		
Heliport	None	<u>None</u>

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USES	Minimum Off-Street Parking Ratios	
	Motor Vehicle Parking	Bicycle Parking
Stations and terminals for bus and rail passenger service	As determined per subsection 27-203(6)	<u>None</u>
Taxi stand and taxi dispatching office	As determined per subsection 27-203(6)	<u>None</u>

[1] Parking requirements for shopping centers are calculated based on the area of the entire shopping center, rather than the individual uses within the center. This parking calculation provision applies only to shopping centers in which restaurants make up less than 50 percent of the center's gross floor area. Parking requirements for any drive-through uses within a shopping center are always calculated separately.

(Ord. No. 2013-10-15, § 1(Exh. A § 27-12.20), 10-14-2013)

Sec. 27-204. - ~~Allowed m~~**Motor vehicle parking reductions.**

(a) *Transit-served locations.*

(1) The community development director may authorize up to a reduction in the number of minimum off-street motor vehicle parking requirement spaces required for nonresidential uses located on lots within 1,500 feet of the pedestrian entrance of a commuter-light rail station or bus rapid transit stop in accordance is reduced by with the administrative permit provisions of article V, division 7. The extent of reduction may not exceed three 5 spaces or 25-33 percent from the minimums stated in Sec. 27-202, whichever is results in a greater reduction. Any authorized reduction in motor vehicle parking spaces must be offset by provision of bicycle parking spaces at a rate of one bicycle space for each 2 reduced motor vehicle parking spaces. Required bicycle parking spaces may not be counted toward satisfying this requirement.

(2) The minimum off-street motor vehicle parking requirement for dwelling units in multi-unit and mixed-use buildings located on lots within 1,500 feet of the pedestrian entrance of a light rail station or bus rapid transit stop is reduced by 15 percent from the minimums stated in Sec. 27-202. Any authorized reduction in motor vehicle parking spaces must be offset by provision of bicycle parking spaces at a rate of one bicycle space for each 2 reduced motor vehicle parking spaces. Required bicycle parking spaces may not be counted toward satisfying this requirement. No bicycle parking may be used to reduce any guest parking requirements.

(b) **Motorcycle parking.** In parking lots containing over 20 motor vehicle parking spaces, motorcycle or scooter parking may be substituted for up to five automobile parking spaces or five percent of required motor vehicle parking, whichever is less. For every four motorcycle or scooter parking spaces provided, the automobile parking requirement is reduced by one space. Each motorcycle and scooter space must have a concrete surface and minimum dimensions of four feet by eight feet. This provision applies to existing and proposed parking lots.

(c) **Bicycle parking.** Uses that provide bicycle parking and storage spaces in excess of the minimum requirements of Sec. 27-202 are eligible for a reduction of required motor vehicle parking, in accordance with section 27-205

(d) *Car-share service.*

- (1) For any development, one parking space or up to five percent of the total number of required spaces, whichever is greater, may be reserved for use by car-share vehicles. The number of required motor vehicle parking spaces is reduced by one space for every parking space that is leased by a car-share program for use by a car-share vehicle. Parking for car-share vehicles may be provided in any non-required parking space.
- (2) For any residential or mixed-use development that (a) is required to provide 50 or more parking spaces to serve residential dwelling unit and (b) provides one or more spaces for car-share vehicles, the number of required parking spaces may be reduced by four spaces for each reserved car-share vehicle parking space. No reduction of required visitor parking spaces is allowed.

(e) On-street Parking.

Nonresidential uses may count on-street parking spaces on public street rights-of-way abutting the subject property may be counted towards satisfying off-street motor vehicle parking requirements for nonresidential uses if such spaces meet city design specifications. One on-street parking space credit may be taken for each 20 linear feet of abutting right-of-way where parallel on-street parking is allowed. Credit for angled parking, where allowed, will be determined by the community development director. Only space on the same side of the street as the subject use may be counted, except that the community development director is authorized to allow spaces on the opposite side of the street to be counted if the property on that side of the street does not have the potential for future development. In calculating credit for on-street parking, all fractional spaces are rounded down.

(f) Outdoor Dining Areas

- (1) Except as otherwise expressly stated in this ordinance, minimum off-street parking requirements for eating and drinking establishments must be based on the amount of indoor and outdoor floor area.
- (2) Outdoor dining areas containing up to 24 seats or 10% of the indoor public floor area devoted to seating (whichever amount is less) are exempt from off-street parking requirements. Off-street parking spaces must be provided in accordance with the minimum off-street parking ratios of Sec. 27-202 for all portions of outdoor seating areas exceeding 10% the indoor public floor area devoted to seating and for all portions of outdoor dining areas with a capacity of more than 24 seats.

(ge) Shared parking.

- ~~—(1)~~ Sharing parking among different users can result in overall reductions in the amount of motor vehicle parking required. Shared parking is encouraged as a means of conserving scarce land resources, reducing stormwater runoff, reducing the heat island effect caused by large paved areas and improving community appearance.
- (2) The community development director is authorized to approve shared parking facilities ~~are allowed~~ for mixed-use projects and for arrangements in which multiple uses propose to share the same parking facilities with different times of peak parking demand, subject to approval by the community development director. Applicants proposing to use shared parking as a means of reducing overall motor vehicle parking requirements must submit:

- a. The names and addresses of the uses and of the owners or tenants that are sharing the parking;
 - b. The uses for which shared parking is proposed and the gross floor area occupied by such uses;
 - ~~b.c.~~ The location and number of parking spaces that are being shared;
 - ~~ed.~~ A shared parking analysis supporting the shared parking request;
 - ~~ed.~~ A legal instrument such as an easement or deed restriction guaranteeing access to the parking for the shared parking users.
- (3) The community development director is authorized to specify the shared parking methodology to be used or to require that the analysis be ~~The required shared parking analysis must be based on the latest edition of the Urban Land Institute's (ULI) shared parking model or be~~ prepared by registered engineer in the State of Georgia with expertise in parking and transportation. The shared parking analysis must demonstrate that the peak parking demands of the subject uses occur at different times and that the parking area will be large enough for the anticipated demands of ~~both all~~ uses.
- (4) Shared parking may be located off site, provided that at least 75 percent of the required number of parking spaces for the subject use must be located on-site. Off-site parking is subject to the regulations of subsection 27-206(c). The requirement for 75 percent of required parking spaces to be located on-site does not apply when all uses making use of the shared parking are located within the boundaries of the PC Overlay.
- (5) Required residential parking and accessible parking spaces (for persons with disabilities) may not be shared and must be located on site.
- (6) In all cases, the property owner or management company is responsible for ensuring that adequate parking is available and does not impact adjacent property or rights-of-way.

(gh) Alternative compliance.

The motor vehicle parking ratios of this division are not intended to prevent development and redevelopment or to make development and redevelopment economically impractical. In order to allow for flexibility in addressing the actual expected parking demand of specific uses, alternative compliance parking ratios may be approved through the special exception process (see article V, division 6) only if the zoning board of appeals determines that the proposed parking reductions are not likely to cause adverse impacts on traffic circulation and safety or on the surrounding area and the applicant provides a parking study or survey or a parking demand management plan in accordance with the following:

- (1) The applicant submits a parking study or survey, prepared and sealed by a registered professional engineer in the State of Georgia with expertise in parking and transportation demonstrating that the motor vehicle parking ratios of section 27-202 do not accurately reflect the actual parking demand that can reasonably be anticipated for the proposed use; ~~and or~~
- (2) The applicant submits a parking demand management plan demonstrating a comprehensive and long-term approach to reducing on-site parking demand incorporating at least 3 of the following techniques:

- a. Provision of or cost-participation in shuttle services from rail or bus transit or off-site parking facilities owned or leased by the applicant or employers who are tenants of the applicant's project.
- b. Subsidy of employee use of high-occupancy motor vehicles such as carpools and vanpools;
- c. Instituting a parking charge and not permitting such charge to be employer-subsidized;
- d. Provision of subsidized transit passes;
- e. Provision for bicycle commuters including but not limited to indoor bike storage and shower and changing facilities;
- f. Other techniques acceptable to the zoning board of appeals and capable of reducing off-street parking demand at the work site.

~~(2) The zoning board of appeals determines that the proposed reduced parking ratios are not likely to cause adverse impacts on traffic circulation and safety or on the surrounding area.~~

(Ord. No. 2013-10-15, § 1(Exh. A § 27-12.40), 10-14-2013)

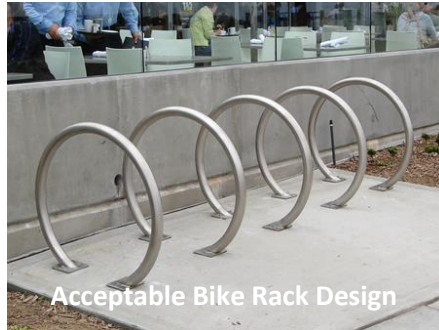
Sec. 27-205. - Bicycle parking.

- (a) **General.** This section allows reduction of motor vehicle parking requirements in exchange for providing bicycle parking facilities in excess of minimum bicycle parking requirements. It also establishes location and design requirements for required and non-required bicycle parking facilities.
- (b) **Replacement of motor vehicle parking spaces with additional bicycle parking spaces.** Any nonresidential use may convert or substitute up to 10 percent of to 25-required motor vehicle parking spaces in exchange for providing additional bicycle parking spaces at the following ratios, provided that the total motor vehicle parking reduction does not exceed 25 spaces:
 - (1) A reduction of one motor vehicle parking space is permitted for each six short-term bicycle parking spaces provided. Required bicycle parking spaces required under Sec. 27-202 may not be counted in determining allowed vehicle parking reductions.
 - (2) A reduction of one motor vehicle parking space is permitted for each two long-term bicycle parking spaces provided. Required bicycle parking spaces required under Sec. 27-202 may not be counted in determining allowed vehicle parking reductions.
- (c) **Location and design.**
 - (1) **Short-term bicycle parking spaces.**
 - a. **Location.** Off-street bicycle parking spaces required by Sec. 27-202 and sshort-term bicycle parking spaces provided to receive parking credit in accordance with subsection (b) of this Sec. 27-205 must be located in highly visible, illuminated areas that do not interfere with pedestrian movements Such Short-term bicycle parking spaces must be located within 100 feet of a customer entrance.
 - b. **Design.** Off-street bicycle parking spaces required by Sec. 27-202 and sshort-term bicycle parking spaces provided to receive parking credit in accordance with subsection (b) of this Sec. 27-205 must:

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1. Consist of bike racks or lockers that are anchored so that they cannot be easily removed;
2. Be of solid construction, resistant to rust, corrosion, hammers, and saws;
3. Allow both the bicycle frame and the wheels to be locked with the bicycle in an upright position using a standard U-lock;



4. Be designed so as not to cause damage to the bicycle;
5. Facilitate easy locking without interference from or to adjacent bicycles; and
6. Have minimum dimensions of two feet in width by six feet in length, with a minimum overhead vertical clearance of seven feet.

(2) *Long-term bicycle parking and storage spaces.*

- a. **Location.** Long-term bicycle parking spaces provided to receive parking credit in accordance with subsection (b) [of this Sec. 27-205](#) must be provided in at least one of the following locations:
 1. In weather-protected stationary racks or lockers that comply with the short-term bicycle parking location requirements of subsection (c)(1)a.
 2. In a locked room;
 3. In a weather-protected locked area that is enclosed by a fence or wall with a minimum height of seven feet;
 4. In a private garage or private storage space serving an individual dwelling unit within a multi-dwelling (residential) building;

5. In a weather-protected area within clear view of an attendant or security personnel;
 6. In a weather-protected area continuously monitored by security cameras; or
 7. In a weather-protected area that is visible from employee work areas.
- b. **Design.** Long-term bicycle parking spaces provided to receive parking credit in accordance with subsection (b) of this Sec. 27-205 must:
1. Be covered to provide protection from weather and secured to prevent access by unauthorized persons;
 2. Consist of bike racks or lockers anchored so that they cannot be easily removed;
 3. Be of solid construction, resistant to rust, corrosion, hammers, and saws;
 4. Allow both the bicycle frame and the wheels to be locked with the bicycle in an upright position using a standard U-lock;
 5. Be designed so as not to cause damage to the bicycle;
 6. Facilitate easy locking without interference from or to adjacent bicycles; and
 7. Have minimum dimensions of two feet in width by six feet in length, with a minimum overhead vertical clearance of seven feet. Bicycle lockers are exempt from overhead clearance requirements.

Sec. 27-208. - Parking area design.

- (c) **Geometric design.** Parking lots must comply with the geometric design standards of the Institute of Traffic Engineers (ITE).
Parking lots must be designed in accordance with the dimensional standards in the following table, which shows minimum dimensions for various parking layouts (angles).
Requirements for layouts or angles not shown here may be interpolated from the layouts shown, as approved by the community development director.

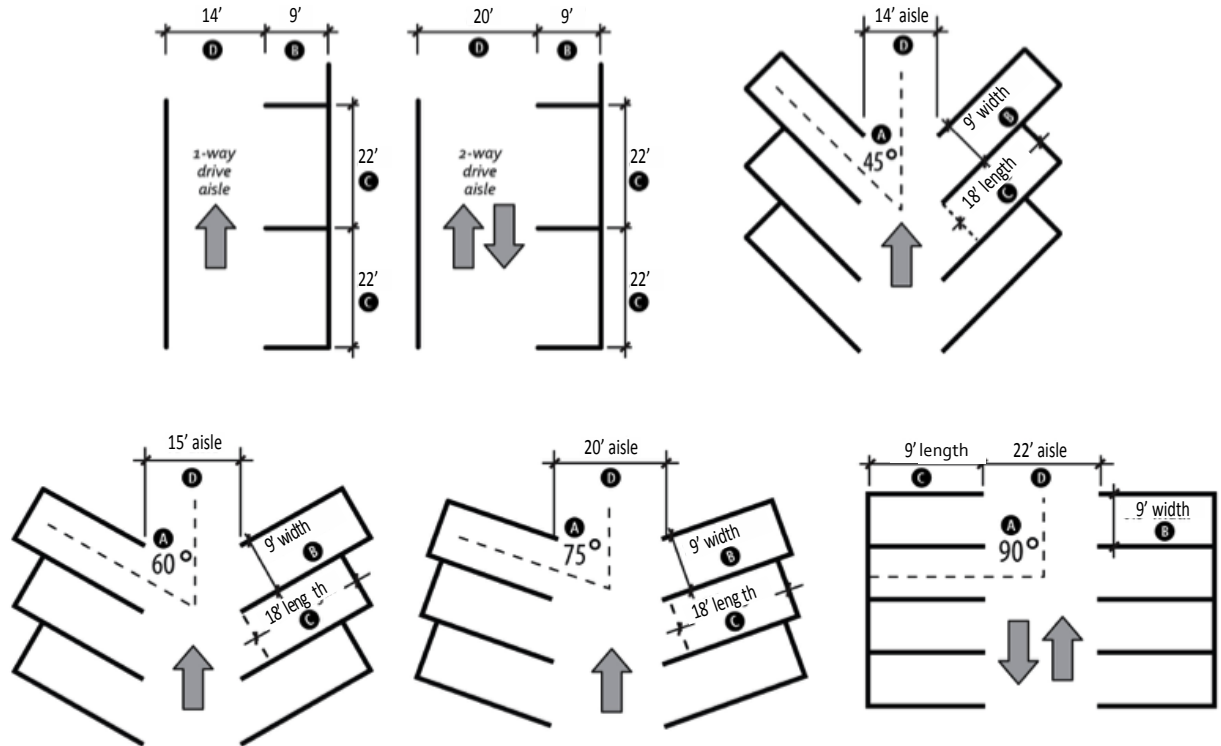
A	B	C	D
0°	9.0	22.0	14.0/20.0
45°	9.0	18.0	14.0/19.0
60°	9.0	18.0	15.0/20.0
75°	9.0	18.0	20.0/21.0
90°	9.0	18.0	-/22.0

A = Stall Angle, **B** = Stall Width, **C** = Stall Length, **D** = Aisle Width (1-way/2-way)

Figure 12-1A: Parking Area Geometrics

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DIVISION 2. – LANDSCAPING AND SCREENING

Sec. 27-230 Transition Yards

(a) Applicability

The transition yard landscaping regulations of this section apply along interior property lines in those instances expressly identified in this zoning ordinance and only to the following activities:

- (1) The construction or installation of any new principal building or principal use; and
- (2) The expansion of any existing principal building or principal use that ~~results in an increase in gross floor area or site area improvements impervious surface area~~ by more than ~~525%~~ or 1,000 square feet, whichever is greater. In the case of expansions triggering transition yard requirements, the transition yard must be placed to provide reasonable screening and buffering of areas visually impacted by the proposed expansion, as determined by the community development director. In the case of expansions that trigger compliance with transition yard requirements, transition yard landscaping is required only in proportion to the degree of expansion. The city arborist is authorized to allow the transition yard to be established adjacent to the area of expansion or to disperse transition yard landscaping along the entire site transition area.

(b) Transition Yard Types

- (1) Four transition yard types are established in recognition of the different contexts that may exist. They are as follows:

Transition Yard Types				
Specifications	TY1	TY2	TY3	TY4
Min. Yard Width (feet) [1]	7.5	10	15	30
Min. Fence/Wall Height (feet)	6 (wall required)	6 [2]	6NA	6NA
Min. Trees (per 100 feet)				
Canopy	Not required	3	4	4
Understory	4	3	4	5
Min. Shrubs (per 100 feet)	Not required	Not required 30 [2]	30 15	30

- [1] Yard widths calculated on the basis of average per 100 feet, provided that the yard width at any point may not be less than 50% of the minimums stated in the table. Required zoning district setbacks may be counted toward satisfying transition yard widths.

[2] TY2 transition yard must include either (a) a fence/wall or (b) shrubs meeting the minimum requirements of this table.

- (2) Transition yard requirements are minimum regulations. Property owners who are subject to transition yard requirements may elect to provide a higher level transition yard.
- (3) The city arborist is authorized to count existing vegetation and structural elements as satisfying all or part of transition yard requirements upon

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determining that such existing vegetation and structural elements meet the overall intent of these landscaping and screening regulations.

~~(2)~~(4) No buildings or parking areas may be located in required transition yards. Breaks for motorized and non-motorized transportation access are allowed.

(c) Zoning District Transition Yards

Transition yards are required between zoning districts in accordance with the following minimum requirements.

Zoning of Subject Site	Zoning of Abutting Site									
	Zoning District ↓→	R, RA	RM-150 to 75	OCR, CR-1, RM-HD	O-I-T, NS	NS, C-1, C-2	O-I, PC-1, PC-2	PC-3, PC-4	O-D	M
	R or RA	None	TY1	TY2	TY1	TY2	TY2	TY1	TY1	TY3
	RM-150 to 75	TY3	None	None	TY1	TY1	TY1	TY1	TY1	TY3
	OCR, CR-1, RM-HD	TY4	None	None	None	None	None	None	None	None
	O-I-T, NS	TY2	TY1	TY1	None	None	None	None	None	None
	C-1, C-2	TY2	TY1	TY1	None	None	None	None	None	None
	O-I, PC-1, PC-2	TY3	TY2	TY1	None	None	None	None	None	None
	PC-3, PC-4	TY4	TY3	None	None	None	None	None	None	None
	O-D	TY3	TY3	TY2	TY1	None	TY1	TY1	None	None
	M	TY4	TY3	TY2	TY1	TY1	TY2	TY2	None	None
	PD	As approved at time of PD approval								

~~(c)~~(d) Landscape Materials and Design

Transition yard landscaping is subject to the regulations of Sec. 27-233.

Sec. 27-271. - Measurement of wing wall and retaining wall height.

1. The height of wing walls ~~and retaining walls~~ is measured from the bottom of the footing or foundation at the base of the wall to the topmost point on the wall.
2. For zoning purposes the height of retaining walls is measured from the natural adjacent grade on the lowest side of the wall to the top of the wall.

(Ord. No. 2013-10-15, § 1(Exh. A § 27-15.60), 10-14-2013)

ARTICLE V. - REVIEW AND APPROVAL PROCEDURES

...

DIVISION 3. - SPECIAL LAND USE PERMITS

...

Sec. 27-359. - Review and approval criteria.

(a) General.

Except as otherwise expressly stated in this ordinance, The-the following review and approval criteria must be used in reviewing and taking action on all special land use permit applications:

- (1) Whether the proposed use is consistent with the policies of the comprehensive plan;
- (2) Whether the proposed use complies with the requirements of this zoning ordinance;
- (3) Whether the proposed site provides adequate land area for the proposed use, including provision of all required open space, off-street parking and all other applicable requirements of the subject zoning district;
- (4) Whether the proposed use is compatible with adjacent properties and land uses, including consideration of:
 - a. Whether the proposed use will create adverse impacts upon any adjoining land use by reason of noise, smoke, odor, dust or vibration generated by the proposed use;
 - b. Whether the proposed use will create adverse impacts upon any adjoining land use by reason of the hours of operation of the proposed use;
 - c. Whether the proposed use will create adverse impacts upon any adjoining land use by reason of the manner of operation of the proposed use;
 - d. Whether the proposed use will create adverse impacts upon any adjoining land use by reason of the character of vehicles or the volume of traffic generated by the proposed use;
 - e. Whether the size, scale and massing of proposed buildings are appropriate in relation to the size of the subject property and in relation to the size, scale and massing of adjacent and nearby lots and buildings; and
 - f. Whether the proposed plan will adversely affect historic buildings, sites, districts, or archaeological resources.
- (5) Whether public services, public facilities and utilities—including motorized and nonmotorized transportation facilities—are adequate to serve the proposed use;
- (6) Whether adequate means of ingress and egress are proposed, with particular reference to nonmotorized and motorized traffic safety and convenience, traffic flow and control and emergency vehicle access;

- (7) Whether adequate provision has been made for refuse and service areas; and
- (8) Whether the proposed building as a result of its proposed height will create a negative shadow impact on any adjoining lot or building.

(b) Major Exceptions to Perimeter Center Regulations.

The general criteria of Sec. 27-359(a) do not apply to special land use permit requests for major exceptions to Perimeter Center Overlay and Perimeter Center zoning district regulations. In reviewing and taking action on special land use permit requests for major exceptions to Perimeter Center Overlay and Perimeter Center zoning district regulations, review and decision-making bodies must consider the following three criteria:

- (1) Whether the major exception request, if granted, will result in development that is inconsistent with the stated intent of the regulations;
- (2) Whether the major exception request, if granted, will result in development that is in keeping with the overall character of the surrounding area; and
- (3) Whether any negative impacts resulting from the granting of the major exception will be mitigated to the maximum practical extent.

DIVISION 7. – SPECIAL ADMINISTRATIVE PERMITS

Sec. 27-436. - Applicability.

The community development director is authorized to approve the following as special administrative permits in accordance with the procedures of this division:

- (1) Any use or activity expressly authorized to be approved by special administrative permit pursuant to the provisions of this zoning ordinance;
- (2) Reductions ~~of~~ in minimum off-street motor vehicle parking and loading ratios by up to one space or ten percent, whichever is greater and reductions in minimum bicycle parking ratios by up to two spaces or ten percent, whichever is greater;
- (3) Minor exceptions to those Perimeter Center Overlay and Perimeter Center zoning district regulations expressly identified in section 27-98(a)(7)a. and section 27-104(a)(5)a. Reduction of minimum off-street parking ratios for any lot located within 1,500 feet of the entrance of a commuter rail station or bus rapid transit stop by up to three spaces or 25 percent, whichever is greater⁵;
- (4) Reduction of zoning district setback requirements by up to ten percent;
- (5) Encroachment of building additions not exceeding one story or 18 feet in height into required rear setbacks by up to ten feet;
- (6) Type B home occupations that are solely teaching-related and conducted entirely within the principal dwelling;

⁵ Transit-based reduction is proposed to become as-of-right.

- (7) Relatives residences;
- (8) Antennas that project more than ten feet above the height of the structure to which they are attached;
- (9) Reduction of minimum building spacing requirements for multiple buildings on a single lot by up to 10 percent;
- (10) Increase in the maximum front door threshold height allowed by section 27-147
- (11) Increase in the maximum retaining wall height, as allowed by section 27-269
- (12) Reduction of the minimum retaining wall setback requirement, as allowed by section 27-269; and
- (13) Reduction of the minimum wing wall side setback requirement, as allowed by section 27-270

(Ord. No. 2013-10-15, § 1(Exh. A § 27-23.10), 10-14-2013)

Sec. 27-437. - Authority to file.

Applications for approval of special administrative permits may be filed by the owner of the subject property or the property owner's authorized agent.

(Ord. No. 2013-10-15, § 1(Exh. A § 27-23.20), 10-14-2013)

Sec. 27-438. - Application filing.

Special administrative permit applications must be filed with the community development director.

(Ord. No. 2013-10-15, § 1(Exh. A § 27-23.30), 10-14-2013)

Sec. 27-439. - Posted notice.

A sign must be placed in a conspicuous location on the subject property at least 30 days before the date of the community development director's decision on the special administrative permit request. This required notice must indicate the earliest date that a decision on the special administrative permit will be made and indicate the nature of the request and a contact where additional information can be obtained.

(Ord. No. 2013-10-15, § 1(Exh. A § 27-23.40), 10-14-2013)

Sec. 27-440. - Community development director's decision.

- (a) The community development director must review each application for a special administrative permit and act to approve the application, approve the application with conditions, deny the application or refer the application to the zoning board of appeals for consideration as a special exception (if related to parking) or a variance.
- (b) The community development director may not take final action to approve or deny a special administrative permit application until at least 15 days after the date that posted notice was provided. All decisions must be made in writing within 30 days of the date that the application was filed.
- (c) The community development director's decision to approve or deny must be based on the approval criteria of section 27-441

(Ord. No. 2013-10-15, § 1(Exh. A § 27-23.50), 10-14-2013)

Sec. 27-441. - Review and approval criteria.

Special administrative permits may be approved by the community development director only when the community development director determines that any specific approval criteria associated with the authorized special administrative permit and the following general approval criteria have been met:

- (1) The grant of the special administrative permit will not be detrimental to the public health, safety or welfare of the public or injurious to the property or improvements;
- (2) The requested special administrative permit does not go beyond the minimum necessary to afford relief, and does not constitute a grant of special privilege inconsistent with the limitations upon other similarly situated properties; and
- (3) The requested special administrative permit is consistent with all relevant purpose and intent statements of this zoning ordinance.

(Ord. No. 2013-10-15, § 1(Exh. A § 27-23.60), 10-14-2013)

Sec. 27-442. - Appeals.

Final decisions of the community development director may be appealed by any person aggrieved by the decision in accordance with the appeal procedures of article V, division 8.

(Ord. No. 2013-10-15, § 1(Exh. A § 27-23.70), 10-14-2013)

Sec. 27-443. - Amending conditions of approval.

A request for changes in conditions of approval attached to an approved special administrative permit must be processed as a new special administrative permit application in accordance with the procedures of this division.

(Ord. No. 2013-10-15, § 1(Exh. A § 27-23.90), 10-14-2013)

Sec. 27-444. - Reporting.

The community development director must maintain records of all special administrative permits that have been approved or denied and provide a summary of such actions to the city council and planning commission at least four times per calendar year.

(Ord. No. 2013-10-15, § 1(Exh. A § 27-23.100), 10-14-2013)

DIVISION 10. – SITE PLAN REVIEW**Sec. 27-485. - Applicability.**

(a) General. Site plan review and approval, in accordance with the procedures of this division, is required before a land development permit may be issued.

(b) Exemptions.

- (1) Site plan review requirements do not apply to detached houses.
- (2) The community development director is authorized to waive or modify the site plan review requirements for proposed construction activities and changes of use that will result in no significant exterior changes or site modifications.

Sec. 27-486. - Authority to file.

Applications and site plans may be filed by the owner of the subject property or the property owner's authorized agent.

Sec. 27-487. - Application filing.

Site plans must be filed with the community development director. The site plan must include at least the following information, unless waived by the community development director:

- (1) Site location map. Location of site within a half mile context of streets, rail lines, stations, and notable sites.
- (2) Site data table. Tabular summary comparing zoning requirements with proposed plans, including lot, building and site regulations and parking requirements.
- (3) Survey Plat. Location and dimensions of property lines, easements, rights-of-way and a legal description of the site.
- (4) Boundaries. Development boundaries and proposed phasing (as applicable).
- (5) Existing Conditions Plan. Existing on-site and adjacent off-site structures, driveways, sidewalks, streets, utilities (underground and above ground), easements, and pavement noted either on an aerial photograph or site survey. Identify all structures proposed for demolition.
- (6) Construction Staging Plan. Plans for deliveries and storage of construction materials, location of employee parking and work areas, plans for site and adjacent right-of-way clean-up.
- (7) Existing Natural Resources Plan. Existing topography, trees, vegetation, drainageways, floodplain/way, or other unique features including plans for removal or modifications of existing natural resource areas.
- (8) Grading Plan and Site Plan. A preliminary grading plan and site plan delineating all proposed structures and surfaces, including parking, pavement, sidewalks, patios, landscape areas, retaining walls and any freestanding signs.
- (9) Building Plans. Floor plans illustrating compliance with all applicable requirements of the Perimeter Center overlay and Perimeter Center zoning district.
- (10) Use Requirements. A table of uses is required on the building plan delineating locations and floor area of proposed uses, illustrating compliance with applicable use regulations.
- (11) Building Elevations. Building elevations of all facades, rendered to illustrate compliance with the general building design criteria of the Perimeter Center overlay (See Sec. 27-98(d) and all applicable building type and design regulations of the underlying Perimeter Center zoning district (See Sec. 27-105). Include information on colors and building materials.

(12) Landscape Plan. Plans illustrating compliance with all applicable landscaping, screening and transition regulations, including a tabular summary identifying all plant materials, sizes and graphic symbols used on the landscape plan.

(13) Parking Plan. Parking layout plan, including driveway and drive aisle location and design.

(14) Other Information. Any other information deemed necessary by the community development director to allow for competent review of the site plan or to demonstrate compliance with applicable regulations, expressly including Perimeter Center Overlay and Perimeter Center zoning district regulations.

Sec. 27-488. - Community development director's decision.

(a) The community development director must review each site plan and act to approve the site plan, approve the site plan with conditions, or deny site plan approval.

(b) The community development director's decision must be based on the approval criteria of section 27-489.

(c) Decisions to deny site plan approval must be accompanied by a written explanation of the reasons for denial.

Sec. 27-489. - Review criteria.

Special administrative permits may be approved by the community development director, after consulting with other city departments and affected agencies, only when the community development director determines that the proposed plan complies with all applicable regulations.

Sec. 27-490. - Appeals.

Final decisions of the community development director may be appealed by any person aggrieved by the decision in accordance with the appeal procedures of article V, division 8.

Sec. 27-491. - Transfer of site plan approval.

Approved site plans, and any attached conditions, run with the land and are not affected by changes in tenancy or ownership.

Sec. 27-492. - Amending conditions of approval.

A request for changes in conditions of approval attached to an approved site plan must be processed as a new site plan application in accordance with the procedures of this division.

Sec. 27-493. - Lapse of approval.

An approved site plan lapses and has no further validity or effect 12 months after the date of its approval unless a land development permit is issued for construction pursuant to the approved site plan. The community development director is authorized to grant up to 2 successive 12-month extensions if the community development director determines that there have been no changes to the zoning ordinance or land development regulations that would significantly affect the approved site plan. Site plan extension requests must be filed before the site plan lapses.

ARTICLE VII. - MEASUREMENTS, LANGUAGE AND DEFINITIONS

DIVISION 3 - DEFINITIONS

Add the following to the alphabetical listing of definitions:

Bicycle Parking, Long-Term means bicycle parking spaces provided to meet the needs of residents, employees or occupants of a site who leave bicycles unattended overnight, during the workday or other long periods of time.

Bicycle Parking, Short-Term, means bicycle parking spaces provided to meet the needs of customers and other short-term visitors to a site.

Building type, civic has the meaning ascribed in 27-105(b)(2)e.

Building type, detached house has the meaning ascribed in See 27-105(b)(2)d.

Building type, general has the meaning ascribed in 27-105(b)(2)b.

Building type, shopfront has the meaning ascribed in 27-105(b)(2)a.

Building type, townhouse has the meaning ascribed in 27-105(b)(2)c.

Courtyard means an outdoor area enclosed by a building facade on at least 3 sides and open to the sky.

Expression line means an architectural feature consisting of a decorative, three-dimensional, linear element, horizontal or vertical, protruding or indented at least 1.25 inches from the exterior facade of a building. It is typically utilized to delineate the top or bottom of floors or stories of a building.

Fenestration means, for purposes of administering and interpreting the Perimeter Center districts and overlay only, a measurement of the percentage of a facade that has highly transparent, low-reflectance windows, not including mirrored glass.

Frontage means, for purposes of administering and interpreting the Perimeter Center districts and overlay only, required pedestrian and bicyclist accommodations within the space between the building and the existing curb for all street types within the Perimeter Center overlay. Street types are identified in Figure 27-98-2.

Interior lot façade means any facade that does not face a street, faces the interior of the lot, or is located such that a parking lot is between the building and the street.

Main parking lot means a parking lot that contains more than 50 percent of the parking designated for a particular building.

Occupied space means interior building space regularly occupied by the building users. It does not include storage areas, utility space, or parking, but includes hallways, stairwells, and elevators.

Open water means a pond, lake, reservoir, or other water feature holding water at all times and the with the water surface fully exposed.

Pedestrianway means a pathway designed for use by pedestrians; it can be located mid-block allowing pedestrian movement from one street to another without traveling along the block's perimeter.

Pervious ~~surface paving or pavement~~ means a ground covering of hard-surfaced material that is designed and installed to allow percolation or infiltration of stormwater into the soil below. ~~placed in an improved area, for such uses as walkways and pedestrian plazas/terraces, on-grade vehicle parking lots, and driveways.~~ Pervious ~~surface paving or pavement systems~~ include but are not limited to porous concrete and modular porous paver systems that are designed to allow infiltration of stormwater, ~~consistent with stormwater bmps.~~ Pervious paving or pavement areas are not considered "impervious surface areas" for the purposes of calculating impervious surface coverage.

Semi-pervious surface means, for purposes of administering and interpreting the Perimeter Center districts and overlay only, either a material covering a surface that allows for at least 40 percent absorption of water into the ground or plant material, such as pervious pavers, permeable asphalt and concrete, or gravel, or a vegetated roof.

Shopping center means a group of retail and other commercial establishments that is planned, developed, owned and/or managed as a single property, typically with collective parking, shared driveways, common signage and other shared features.

Stoop means a platform entranceway at the door to a building, providing a transition between the interior of the building and the sidewalk outside the building. The stoop may be elevated or at grade, and may be covered by a canopy or awning.

Story, ground (or ground floor) means the first floor of a building on any street facade that is level to or elevated above the finished grade on any street facade.

Story, half means a story either in the basement of the building, partially below grade and partially above grade, or a story fully within the roof structure with windows facing the street.

Story, Upper (or upper floor) means all floors located above the ground story of a building. Upper stories may be half stories.

Street façade means the facade of a building that faces a street right-of-way.

Vegetated roof means that portion of a building roof covered with vegetation and a growing medium, planted over a waterproofing membrane. It may also include additional layers such as a root barrier and drainage and irrigation systems.

Visible basement means A half story or basement partially below grade and exposed above grade a minimum of one half the minimum floor-to-floor height.

Sec. 27-166.

(d) Location. Accessory uses and structures must be located on the same lot as the principal use to which they are accessory, unless otherwise expressly stated. Accessory buildings are expressly prohibited in required front street setbacks