

#### **MEMORANDUM**

To: Mayor and City Council

From: Steve Foote, AICP

Date: September 26, 2016

**Subject: SLUP 16-091:** David Blumenthal, on behalf of Dirk Laukien, owner of 5490

Chamblee Dunwoody Road, Dunwoody, GA 30338, seeks a Special Land Use Permit from Chapter 27-97(g)(1) to exceed the maximum allowed parking in the Dunwoody Village Overlay District. The tax parcel number is 18 366 01

013.



#### **BACKGROUND**

The site, 5490 Chamblee Dunwoody Road, is located in Dunwoody Village on the west side of Chamblee Dunwoody Road just north of the intersection with Nandina Lane. The property is zoned Local Commercial (C-1) District and abuts properties zoned C-1 on all sides. The property is approximately 0.74 acres (32,356 square feet) in area. The site contains a single story, 4,200 square foot building, built in 1980, which was once the Old Hickory House restaurant. The site has been vacant since the restaurant closed in August of 2014. The current owner purchased the property in 2016. The applicant, on behalf of SunTrust Bank,



has applied for three variances and one Special Land Use Permit (SLUP), to allow for redevelopment of the site. Site variances were recently approved by the Zoning Board of Appeals. The SLUP request is to exceed the maximum number of parking spaces allowed in the Overlay.

SunTrust currently has a bank branch located in Dunwoody Village at the corner of Dunwoody Village Parkway and Mount Vernon Road. That address is 1710 Mount Vernon Road. Suntrust leases that space. Suntrust plans to move out of that space and relocate to the subject site. The applicant has stated that Suntrust would purchase the subject site and own the land and bank building.

The project calls for a complete redevelopment of the subject site. The applicant has proposed to demolish the existing building and parking lot; construct a 3,543 square foot bank building with a new parking lot behind the building; install bicycle parking, landscaping, landscape islands in the parking lot, and the required public improvements along Chamblee Dunwoody Road. The site, as it exists today, has very little green space, with 0.7 acres (94% of the site) consisting of impervious surfaces. The proposed development will provide additional green space and plantings and locate the proposed bank building near the public sidewalk. The redevelopment will convert approximately 0.27 acres of impervious surfaces to pervious open green space, and the impervious lot coverage will be reduced to 58.1% of the site. Along the frontage, there is a 6-foot wide sidewalk in the right-of-way and landscaped areas with three healthy magnolia trees. The magnolia trees will remain. The public improvements that the applicant will provide along the frontage include installing the additional 6 foot wide sidewalk addition, bike lane, streetscape furniture, and lighting improvements along the frontage.

At issue is the parking. The subject site currently contains a total of 57 parking spaces located along all lot lines and around the existing building itself. The Dunwoody Village Overlay allows a maximum of 3 parking spaces per 1,000 square feet of floor area for all non-residential uses on all properties in the Overlay area. That equates to a maximum of 11 parking spaces for the new building. The applicant states that 11 spaces will be insufficient parking for its staff and customers and has requested the SLUP to install 22 parking spaces. By comparison, the bank's current location on Mt. Vernon Road, located on an approximately 0.6 acre lot, which was developed prior to the Overlay, has 24 parking spaces and a 3,040 square foot building.

The three companion variance requests, (ZBA 16-081), were as follows:

- a.) a variance from Sec. 27-211 to allow the drive-through lane to encroach the rear setback of 25 feet;
- b.) a variance from Sec. 27-73 to allow the drive thru building to encroach the rear setback of 25 feet;
- c.) a variance from Sec. 27-250 to increase the maximum allowed footcandles for lighting on the site.

All three variance requests were approved by the Zoning Board of Appeals on August 4, 2016.

With the approval of the variance requests, the site plan meets all base zoning regulations. The applicant has proposed to follow all Overlay regulations other than what is requested in the SLUP for parking. The building and drive-through structure will comply with all architectural specifications of the Overlay, and the project will be reviewed by DRAC prior to the issuance of any permits.



#### **ANALYSIS**

Procedural excerpts from the code, as discussed herein, are provided below. The full text of the DVO is attached for reference. Portions of both the Comprehensive Plan, related to the Dunwoody Village Character Area, and its addendum, the Dunwoody Village Master Plan, are attached for consideration. An analysis of surrounding land uses is also provided.

#### **Surrounding Land Analysis**

Direction	Zoning	Use	Current Land Use
N	C-1	Bank	Commercial
S	C-1	Restaurant with drive-through	Commercial
E	C-1	Shopping Center	Commercial
W	C-1	Shopping Center	Commercial

#### **Zoning Ordinance Analysis**

#### **Purpose:**

DVO, Section 27-97(I)

Modifications and adjustments. The regulations of subsections (e) through (k), may be modified only if reviewed and approved in accordance with the special land use permit procedures of article V, division 3.

#### **Relevant Code Sections:**

Chapter 27, Section 27-97

- (g) Parking and circulation. The parking and circulation regulations of article IV, division 4, apply within the Dunwoody Village Overlay district except as modified by the following regulations:
  - (1) New nonresidential buildings and nonresidential uses and additions to existing nonresidential buildings and nonresidential uses may not provide more than three parking spaces per 1,000 square feet of floor area. This provision does not require that existing "excess" parking spaces be removed.

#### **Modifications requested:**

1. DVO, Section 27-97(g)(1) Parking and Circulation: To exceed the 3 parking spaces per 1,000 square feet of floor area maximum to increase the number of spaces allowed on the subject lot from 11 spaces to 22 spaces.

#### **Dunwoody 2015-2035 Comprehensive Plan Analysis**

The Comprehensive Plan serves as the City's guiding policy document for land use decisions. It divides the city into context-specific "Character Areas." The subject parcel is located in the "Dunwoody Village Character Area", which has historically been the center of



Dunwoody. The context of the area is further broken down into sub-areas by the Dunwoody Village Master Plan. The Master Plan identifies this parcel in the "Neighborhood and Convenience Retail" sub-area.

#### **Dunwoody Village Master Plan**

The Dunwoody Village Master Plan more specifically addresses the desired character of the Neighborhood and Convenience Retail district as primarily existing retail and commercial uses in one to two story buildings, with limited opportunity for new development that will see tenant upgrades with façade, internal landscape, pedestrian, and site improvements over time. The required architectural design and bulk standards in the DVO were developed specifically to implement the desired vision and development character sought in the Dunwoody Village Character Area. As stated in Section 27-97(a), the primary purpose and intent of the DVO is to implement the policies and objectives of the comprehensive plan and Dunwoody Village Master Plan.

#### **REVIEW AND APPROVAL CRITERIA**

Chapter 27, Section 27-359 identifies the following criteria to be applied by the department of planning, the planning commission, and the city council in evaluating and deciding any application for a special land use permit. No application for a special land use permit shall be granted by the city council unless satisfactory provisions and arrangements have been made concerning each of the following factors, all of which are applicable to each application:

- 1. Whether the proposed use is consistent with the policies of the comprehensive plan; The proposed use is consistent with the policies of the comprehensive plan. The DVMP states the shopping areas that include the subject site will likely remain retail uses with some limited, storefront offices, and that the neighborhood/convenience retail areas should maintain consistency with the architectural character of the area. The Plan calls for installation of streetscape improvements, screening parking from the street by putting it behind the building, and enhancing parking lot landscaping, all of which the applicant has proposed to do.
- 2. Whether the proposed use complies with the requirements of this zoning ordinance; The "financial institution" use is an allowed use in the C-1 zoning district. There are three features of the development of the site which are noncompliant with the base zoning ordinance for the C-1 District. Variances have been approved to bring them into compliance. The site is noncompliant with the DVO only in regards to the excessive number of parking spaces, for which this SLUP is being requested.

The applicant has proposed to construct the buildings to be in full compliance with the architectural requirements of the DVO.

3. Whether the proposed site provides adequate land area for the proposed use, including provision of all required open space, off-street parking and all other applicable requirements of the subject zoning district;

The proposed site provides adequate land area for the proposed use in terms of required open space, required off-street parking spaces, and all other requirements of the base zoning code and DVO. Adequate room for pedestrian buffer zones and pedestrian circulation is also provided.



- 4. Whether the proposed use is compatible with adjacent properties and land uses, including consideration of:
  - a. Whether the proposed use will create adverse impacts upon any adjoining land use by reason of noise, smoke, odor, dust or vibration generated by the proposed use;
    - No, the proposed use will not create adverse impacts upon any adjoining land use by reason of noise, smoke, odor, dust or vibration generated.
  - b. Whether the proposed use will create adverse impacts upon any adjoining land use by reason of the hours of operation of the proposed use;

    No, the proposed use will not create adverse impacts upon any adjoining land use by reason of the hours of operation. The hours of operation are in line with the hours of operation of many of the surrounding uses such as commercial, retail, and restaurant uses.
  - c. Whether the proposed use will create adverse impacts upon any adjoining land use by reason of the manner of operation of the proposed use;

    No, considering that the proposed commercial bank use is similar in manner of operation to surrounding uses (which include other banks), it would not appear to create adverse impacts upon any adjoining land use by reason of the manner of operation.
  - d. Whether the proposed use will create adverse impacts upon any adjoining land use by reason of the character of vehicles or the volume of traffic generated by the proposed use;
    The proposed use will not create adverse impacts upon any adjoining land use by reason of the character of vehicles or the volume of traffic generated. The proposed commercial bank use will create lower levels of traffic during peak hours than the previous restaurant use or most other uses allowed in the C-1 district.
  - e. Whether the size, scale and massing of proposed buildings are appropriate in relation to the size of the subject property and in relation to the size, scale and massing of adjacent and nearby lots and buildings; and The proposed building is of reasonable size, scale, and massing. Yet, the maximum number of parking spaces allowed on a site in the DVO is dependent upon the square footage of the building. The applicant, then, could increase the insufficient amount of parking spaces, at least partially, by constructing a larger building.
  - f. Whether the proposed plan will adversely affect historic buildings, sites, districts, or archaeological resources.

    The proposed plan will not have an impact on any historic buildings, sites, districts, or archaeological resources. The area on the site which the applicant proposes to develop is already built out.
- Whether public services, public facilities and utilities—including motorized and non-motorized transportation facilities—are adequate to serve the proposed use;
   Public services, facilities, and utilities are sufficient to serve the proposed use.



- Whether adequate means of ingress and egress are proposed, with particular reference to non-motorized and motorized traffic safety and convenience, traffic flow and control and emergency vehicle access;
  - There applicant proposes closing the remaining curb cut on the back corner of the site. Proposed emergency vehicle access is sufficient. The applicant has proposed a pedestrian circulation plan that meets both the base zoning requirements and the DVO requirements for pedestrian circulation.
- 7. Whether adequate provision has been made for refuse and service areas; and **Proposed provisions for refuse and services are adequate.**
- 8. Whether the proposed building as a result of its proposed height will create a negative shadow impact on any adjoining lot or building.

  No negative shadow impact will occur on any adjoining lot or building.

#### **RECOMMENDATION**

#### **Planning Commission Recommendation**

The Planning Commission heard the applicant's SLUP request at their regularly scheduled August meeting on August 9, 2016. After discussion with the applicant and staff, the Planning Commission **recommended approval** of the SLUP application with the following conditions:

- 1. The subject property shall be developed in accordance with the site plan and elevations, completed by Travis Pruitt and Associates, dated July 22, 2016.
- 2. The applicant shall move the off-street sidewalk so that it is a minimum of 10-feet from the curb.

#### **Staff Recommendation**

Based upon the findings and conclusions herein, Staff recommends the application for a Special Land Use Permit from Chapter 27-97(g)(1) to exceed the maximum allowed parking in the Dunwoody Village Overlay District, be **approved**, with the following exhibits and conditions, because it meets all of the criteria for approval of a SLUP:

EXHIBIT A: Site plan and elevations, completed by Travis Pruitt and Associates, dated July 22, 2016

- 1. The subject property shall be developed in general conformity with Exhibit A above, with changes to meet land development and zoning regulations.
- 2. The applicant shall move the off-street sidewalk so that it is a minimum of 10-feet from the curb.

#### **Attachments**

- Ordinance
- Planning Commission August 9, 2016 draft meeting minutes
- Site Plan, dated July 22, 2016
- Color Site Plan and Landscape Plan
- Elevation Drawings



- Pictures taken by staff
- Maps
- Sec. 27-97 DVO excerpt
- Dunwoody Comprehensive Plan excerpt
- Dunwoody Village Master Plan Land Use and Framework Plan excerpt
- SLUP Application Packet

STATE OF GEORGIA CITY OF DUNWOODY

#### **ORDINANCE 2016-XX-XX**

# AN ORDINANCE TO AMEND THE CITY OF DUNWOODY ZONING MAP FOR ZONING CONDITIONS OF LOT PARCEL NUMBER 18 366 01 013 IN CONSIDERATION OF SPECIAL LAND USE PERMIT SLUP 16-091 (5490 Chamblee Dunwoody Road)

- **WHEREAS:** Notice to the public regarding of said modification to conditions of zoning has been duly published in the Official News Organ of the City of Dunwoody, Georgia; and
- **WHEREAS,** the Special Land Use Permit 16-091 is requested to waive the requirement of the Dunwoody Village Overlay District Regulations of Section 27-97(g)(1) to allow to allow an increase in the maximum allowed parking in the zoning district; and
- **WHEREAS:** the current property contains a single-story, 4200 square foot restaurant building, the former Hickory House which the owner wishes to redevelop into a branch of Sun Trust Bank; and
- **WHEREAS:** the applicant has proposed to demolish the existing building, construct a new 3,543 square foot bank building with a new parking lot behind the building, install bicycle parking, add new green space, landscaping and landscape islands in the parking lot, and install the required 6-foot wide sidewalk addition with bike lane and streetscape furniture and lighting improvements; and
- **WHEREAS:** the Mayor and City Council find that this request is in keeping with the requirements of the Dunwoody Village Master Plan which addresses the desired character of the area.

**NOW THEREFORE,** The Mayor and City Council of the City of Dunwoody hereby ordains as follows:

Applicant's request for a SLUP to waive the requirements of Sections 27-97(g)(1), to increase the maximum allowed parking spaces, to a maximum of 22 spaces, is hereby **Approved**, subject to the following exhibits and conditions:

EXHIBIT A: Site plan and elevations, completed by Travis Pruitt and Associates, dated September 6, 2016

- 1. The subject property shall be developed in general conformity with Exhibit A above, with changes to meet land development and zoning regulations.
- 2. The applicant shall move the off-street sidewalk so that it is a minimum of 10 feet from the curb.

SO ORDAINED AND EFFECT	11VE, this day of, 2010
	Approved by:
Attest:	Denis L. Shortal, Mayor  Approved as to Form and Content
Sharon Lowery, City Clerk SEAL	City Attorney

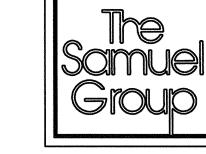
SITE ZONING: C-1 (LOCAL COMMERCIAL DISTRICT

(DUNWOODY VILLAGE OVERLAY DISTRICT)

BUILDING SETBACK LINES:

FRONT YARD: 0-10 FEET BUILD TO ZONE





ARCHITECTURE PLANNING INTERIORS

5887 GLENRIDGE DRIVE SUITE 375 ATLANTA, GEORGIA 30328 TEL: 404.233.8110 FAX: 404.233.7229

SIDE YARD : BUILD TO ZO OF BLDG. PERCENT IN BUI REAR YARD : 30 FEET				
BUILDING SUMM	IARY:			
1—STORY BUILDING 3,543 SQUARE F BUILDING HEIGHT: 25 FEE		-		
PARKING SUMM	ARY:			
	REQUIR	ED*	PROVIDED	
REGULAR PARKING: HANDICAP PARKING:			21 1	
TOTAL PARKING SPACES:	11		22	
*THREE SPACES PER 1000	SQUARE	FEET OF	GROSS FLOOR	AREA.
LOT COVERAGE CALCULATI TOTAL SITE AREA = 3235		74 AC)		

PERVIOUS AREA = 1,927 SF (0.04 AC) IMPERVIOUS AREA = 30,429 SF (0.70 AC) PERVIOUS AREA = 13,363 SF (0.31 AC)

NEW LOT COVERAGE = 58.1%

IMPERVIOUS AREA = 18,993 SF (0.43 AC)

SLUP REQUESTS

± 1.06 ACRES

1. A SPECIAL LAND USE PERMIT IS REQUIRED FOR AN INCREASE IN MAXIMUM PARKING SPACES FROM 11 TO 22. 2. A SPECIAL LAND USE PERMIT FOR THE INCREASE IN THE MAXIMUM AREA BETWEEN FACE OF BUILDING AND SIDEWALK.

Date Description 06/21/2016 UPDATED SLUP PLAN 06/30/2016 REDUCED PROPOSED PARKING 07/22/2016 ADDED SETBACK VARIANCE LABLES

Seal / Signature



	I T	ravis Pruitt & Associates, Inc.
ARE NO STATE WATERS WITHIN 200 FEET OF THE SITE.		
RE ARE NO WETLANDS ON THE SITE.	Project Name	
roperty does not lie within a 100 year flood hazard zone fined by the F.E.M.A. Flood Insurance Rate Map of b County Community Panel Number 13089C0005J MAY 16, 2013.	SUNTRUST BANK	C - DUNWOODY
ition regarding the reputed presence, size, character and location ting underground utilities and structures is shown hereon. There	Project Number	
certainty of the accuracy of this information and it shall be lered in that light by those using this drawing. The n and arrangement of underground utilities and structures shown may be inaccurate and utilities and structures not shown may be intered. The owner, his employees, his consultants and his otors shall hereby distinctly understand that the surveyor is not sible for the correctness or sufficiency of this information.	1629068	
	Scale	
CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UTILITIES. ACTOR SHALL HAVE ALL UTILITIES FLAGGED WITH INVERTIONS <u>PRIOR</u> TO CONSTRUCTION. NOTIFY ENGINEER OF ALL PANCIES OR ADDITIONAL UTILITIES ENCOUNTERED.	1"=20'	
ALL CURB RADII ARE _5' , UNLESS OTHERWISE NOTED.		
	1	•

NOTE: ALL DIMENSIONS ARE TO FACE OF CURB, FACE OF BUILDING, Description OR CENTER OF STRUCTURE, UNLESS OTHERWISE NOTED. NOTE: <u>ALL</u> PIPE LENGTHS ARE SCALED LENGTHS FROM CENTER OF STRUCTURE. CONTRACTOR SHALL VERIFY PRIOR TO ORDERING PIPE. © Copyright 2016 Travis Pruitt & Associates, Inc. THESE DRAWINGS AND THEIR REPRODUCTIONS ARE THE PROPERTY OF THE ENGINEER AND MAY NOT BE REPRODUCED, PUBLISHED, OR USED IN ANY WAY WITHOUT THE WRITTEN PERMISSION OF THIS ENGINEER.

DRWN BY: ABC CHKD BY: DJB JN: 160111 CN: 160111PN FN: 211-E-167

1-800-282-7411 Know what's below.
Call before you dig.

TAX PARCEL 18 366 01 002

N/F

MANHATTAN CHAMBLEE DUNWOODY PROPERTIES LLC

DEED BOOK 21100, PAGE 164

ZONED: C-1 DUMPSTER ENCLOSURE 185.69' (RDH) 10' PROPOSED DRIVE—THROUGH SETBACK LINE \_\_\_\_\_\_ NEW 14STORY VARIANCE REQUEST TO REDUCE THE DRIVE—THROUGH SETBACK LINE FROM 25—FEET TO 10—FEET (SEE ARCH. PLANS FOR EXACT BLDG. DIM.) 3543 SQ. FT. FFE = ±1023.35 VARIANCE REQUEST TO REDUCE THE REAR BUILDING
SETBACK LINE FROM
30-FEET TO 12-FEET \_\_ NÈW STREET LIGHT `POLE (TYP.) DRIVE-THROUGH SETBACK LINE NEW MONUMENT \_ SIGN TAX PARCEL 18 366 01 001 PEACHTREE SHOPS OF DUNWOODY LLC DEED BOOK 25135, PAGE 140 ZONED: C-1 TAX PARCEL 18 366 01 001 PEACHTREE SHOPS OF DUNWOODY LLC DEED BOOK 25135, PAGE 140 PRIVATE DRIVE (INGRESS AND EGRESS EASEMENT DEED BOOK 4151, PAGE 330) TAX PARCEL 18 366 01 014 CICERO GARNER, JR. & DOUGLAS W. MORGAN DEED BOOK 4804, PAGE 243 (BURGER KING RESTAURANT - LEASE AGREEMENT AS RECORDED IN DEED BOOK 14185, PAGE 692)

GRAPHIC SCALE - IN FEET

CITY OF DUNWOODY AUGUST 9, 2016 PLANNING COMMISSION MINUTES

The Planning Commission of the City of Dunwoody held a Meeting on August 9, 2016 at 6:00 PM. The meeting was held in the City of Dunwoody City Hall, 41 Perimeter Center East, Dunwoody, Georgia 30346. Present for the meeting were the following:

Voting Members: Paul Player, Chair

Bob Dallas, Vice-Chair

Kirk Anders, Commission Member Bill Grossman, Commission Member Richard Grove, Commission Member Renate Herod, Commission Member Thomas O'Brien, Commission Member

Also Present: Steve Foote, Community Development Director

John Olson, City Planner

Andrew Russell, Planning Coordinator Ronnie Kurtz, Planning Technician

#### A. CALL TO ORDER

Bob Dallas called the meeting to order.

Kirk Anders motioned to approve Bob as chair for meeting. Richard Grove seconded.

Passed For: 4 (Anders, Dallas, Grove, O'Brien); Against: 0; Abstain: 0; Absent: 3 (Grossman, Herod, Player)

#### B. ROLL CALL

Bill Grossman, Renate Herod, and Paul Player arrived after approval of the minutes, before discussion of new business.

#### C. MINUTES

1. Approval of Meeting Minutes from July 12, 2016 Planning Commission Meeting

Richard Grove motioned to approve both sets of minutes. Kirk Anders seconded.

Passed For: 3 (Anders, Dallas, Grove); Against: 0; Abstain: 1 (O'Brien);

Absent: 3 (Grossman, Herod, Player)

2. <u>Approval of Meeting Minutes from July 12, 2016 Planning Commission Special Called Meeting</u>

Approved under Item C.1.

D. ORGANIZATIONAL AND PROCEDURAL ITEMS (Item D moved to after Item F to allow time for more Commission members to arrive)

#### E. <u>UNFINISHED BUSINESS</u>

No unfinished business.

#### F. NEW BUSINESS

1. <u>SLUP 16-091: David Blumenthal, on behalf of Dirk Laukien, owner of 5490 Chamblee Dunwoody Road, Dunwoody, GA 30338, seeks a Special Land Use Permit from Chapter 27-97(g)(1) to exceed the maximum allowed parking in the Dunwoody Village Overlay District. The tax parcel number is 18 366 01 013.</u>

Andrew Russell presented on behalf of staff and recommended approval of the application with conditions. Andrew responded to questions from the Commission.

Den Webb, attorney for the applicant, presented on behalf of application. Den stated the owner of the Shoppes of Dunwoody, located behind the subject site, is supportive of the request and the project.

No one spoke in opposition.

The Board asked questions of the applicant and staff. Richard Grove asked staff questions regarding the parking space regulations in the Dunwoody Village Overlay and whether the Commission might expect to hear similar requests in the future. Bob Dallas asked the applicant questions regarding bicycle accommodations. Kirk Anders asked for the required stacking capacity for a bank.

Staff responded that the site meets vehicular stacking regulations.

Bob Dallas motioned to approve the Special Land Use Permit with the following conditions:

- 1. The subject property shall be developed in accordance with the site plan and elevations, completed by Travis Pruitt and Associates, dated July 22, 2016.
- 2. The applicant shall move the off-street sidewalk so that it is a

minimum of 10-feet from the curb.

Kirk Anders seconded.

Passed For: 7; Against: 0; Abstain: 0; Absent: 0

2. <u>SLUP 16-092</u>: Transwestern, applicant of a portion of 1134 Hammond Road and 4400 Ashford Dunwoody Drive, Dunwoody, GA 30346, by Jessica Hill, attorney for the applicant, seeks a.) Special Land Use Permit from Chapter 27, Section 27-73(b)(1) to increase the building height in a C-1 (Commercial) District from 2 stories/35 feet to 20 stories/300 feet. The tax parcel numbers are 18 348 01 020 and 18 348 01 006.

John Olson presented on behalf of staff and recommended approval of the application with conditions.

Jessica Hill, attorney for the applicant, presented on behalf of the application. Jessica stated the design and proposal have been created in conjunction with city staff. Jessica stated the project is consistent with the comprehensive plan, and the applicant is amicable to staff's recommendations.

No one spoke in opposition.

Renate Herod asked about parking availability and if a traffic study had been executed.

Kirk Anders asked for clarification of who will own the parking spaces and why so many will be needed for the project

Bill asked if any major tenant had been identified, and if the development would be profitable without tax-free municipal bonds.

Trent Germano, representative of applicant, Transwestern, replied that it is a speculative building that would not begin being built until there was enough pre-leasing. Trent stated that without the bonds for title program, the lease rate would be too high compared to market to attract investors.

Thomas O'Brien asked if any study had been done to determine how much the office tenants use public transit. Trent stated that MARTA and GRTA say 25-30% of commuters use public transit.

Bob Dallas noted that he distributed a study about a central park in the Perimeter area. Bob stated the study for the location of a park includes a portion of the parcel in question.

Bob Dallas motioned to approve the Special Land Use Permit with the

#### following conditions:

- 1. The subject property shall be developed in accordance with the site plan and elevation packet, dated July 21, 2016 with changes to meet land development and zoning regulations.
- 2. The City of Dunwoody is allowed to use the triangular piece of land referenced by Bob Dallas above the eastern MARTA parking deck as a park at such time that the City should so choose.
- 3. The City and General Growth Properties will continue working together to develop what is referred to as 'Perimeter Park.'

Bill Grossman seconded.

Passed For: 6 (Dallas, Grossman, Grove, Herod, Player, O'Brien); Against: 1 (Anders); Abstain: 0; Absent: 0

# D. <u>ORGANIZATIONAL AND PROCEDURAL ITEMS</u> ( Item D moved from before Item E)

#### Elect Vice-Chair

Kirk Anders motioned to approve Bob Dallas as Vice Chair. Bill Grossman seconded.

Passed For: 7; Against: 0; Abstain: 0; Absent: 0

#### G. OTHER BUSINESS

#### H. PUBLIC COMMENT

Bill Baker, GM for Perimeter Mall, gave an update on discussions about the park with City stakeholders in relation to 'Perimeter Park.'

#### I. COMMISSION COMMENT

Kirk Anders asked for an update on the status of the Perimeter Overlay plan.

Steve Foote, Community Development Director, stated the City hopes to adopt it by the end of the year.

Richard Grove stated he would like info about precedents from similar cases in the packet moving forward.

Paul Player stated he believes the mayor does not have the authority to

remove someone from a board for being on a DHA member.

### J. <u>ADJOURN</u>

Attest:

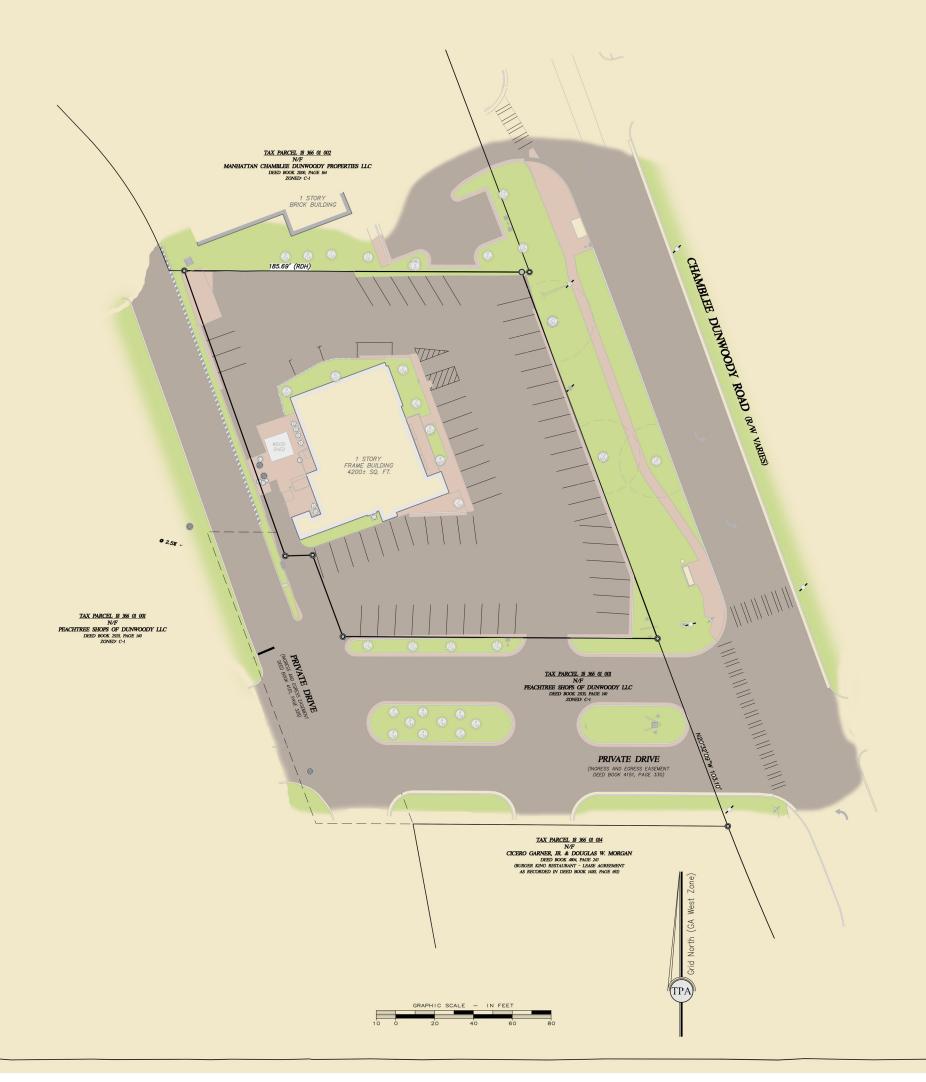
Secretary

	Dallas nded.	motioned	to	Adjourn	Motion	to	Adjourn.	Renate	Herod
Pass	ed For:	7; Against:	0; A	Abstain: 0	; Absent	: 0			
				,	Approve	d by	:		
					Chair				

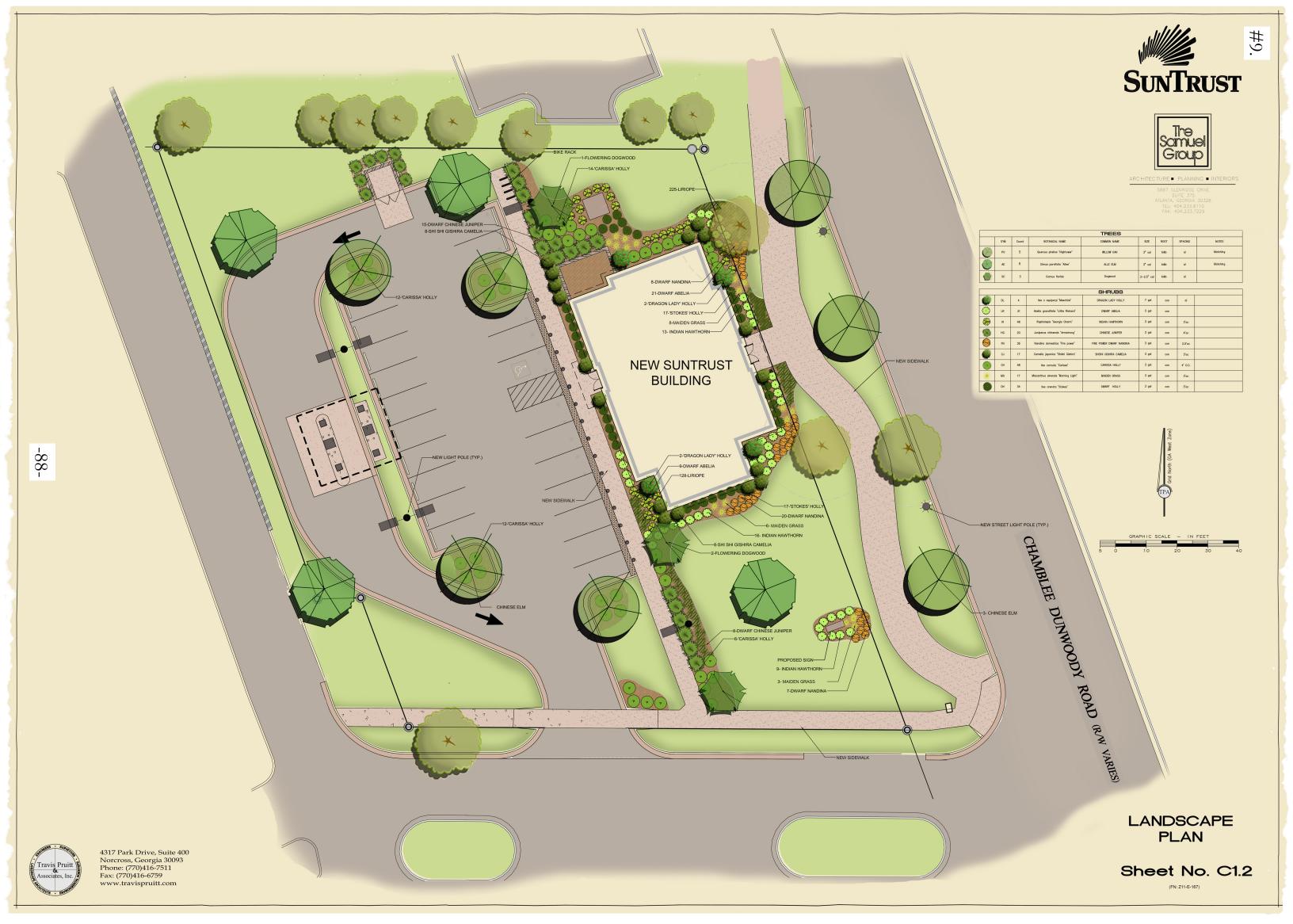




5887 GLENRIDGE DRIVE SUITE 375 ATLANTA, GEORGIA 30328 TEL: 404.233.8110 FAX: 404.233.7229



**EXISTING SITE** 



woody Rd., Dunwoody, GA 30338

SunTrust New Banking Center

LEGEND

Project Name

Seal / Signature

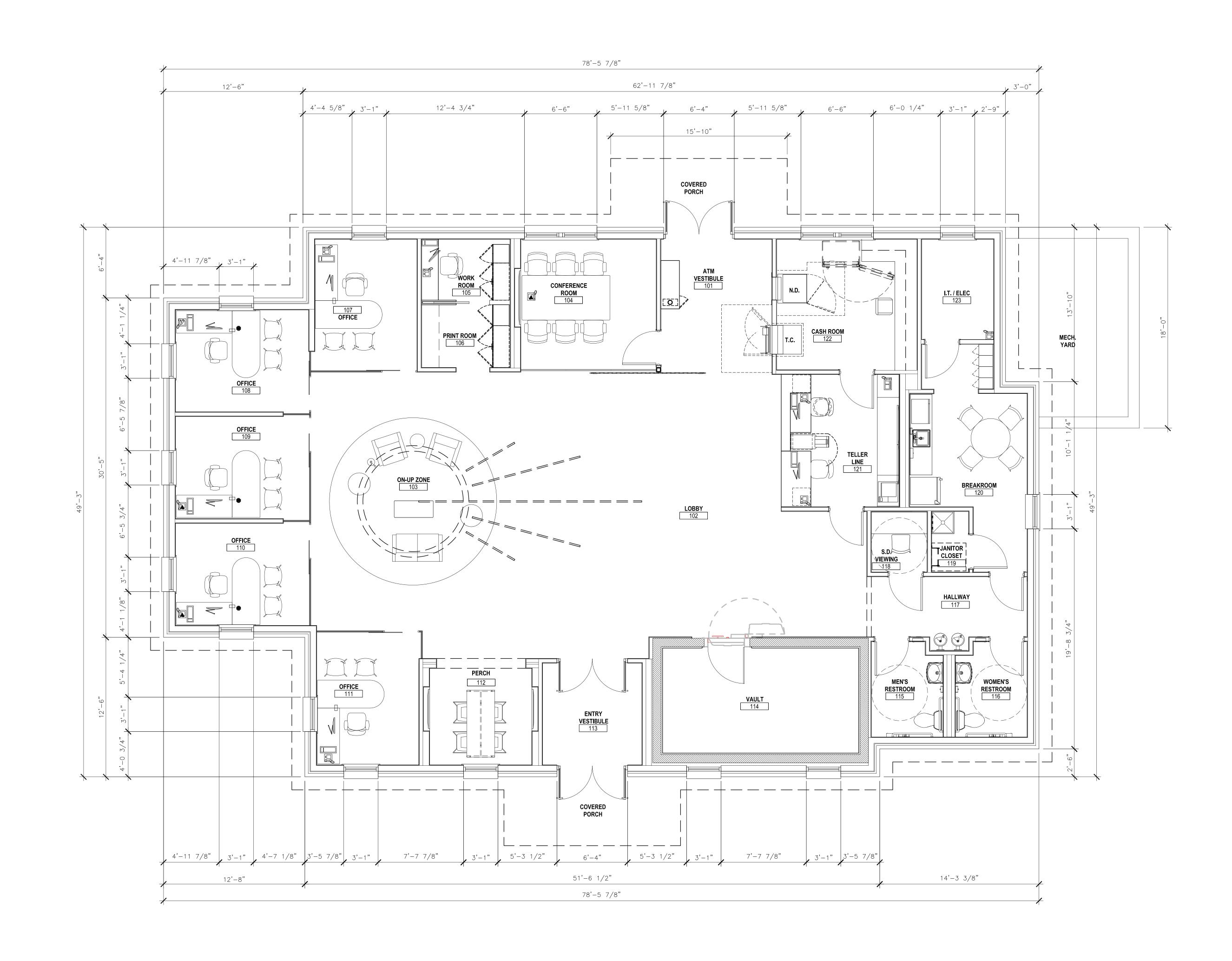
SUNTRUST BANK - DUNWOODY Project Number

1629068

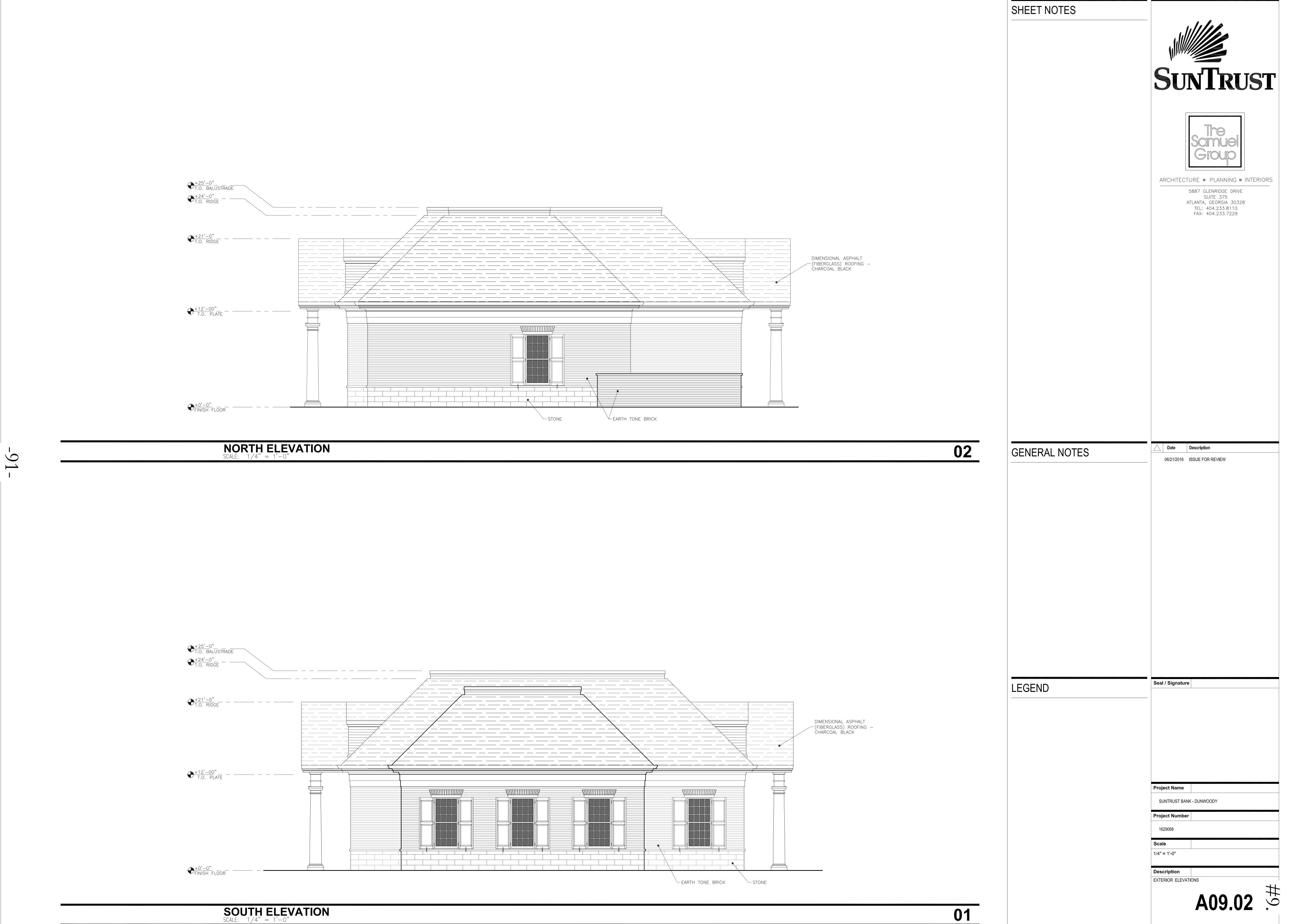
1/4" = 1'-0"

Description

CONSTRUCTION FLOOR PLAN







SunTrust New Banking Center - Dunwoody Branch - 5490 Chamblee Dunwoody Rd., Dunwoody, GA 30338



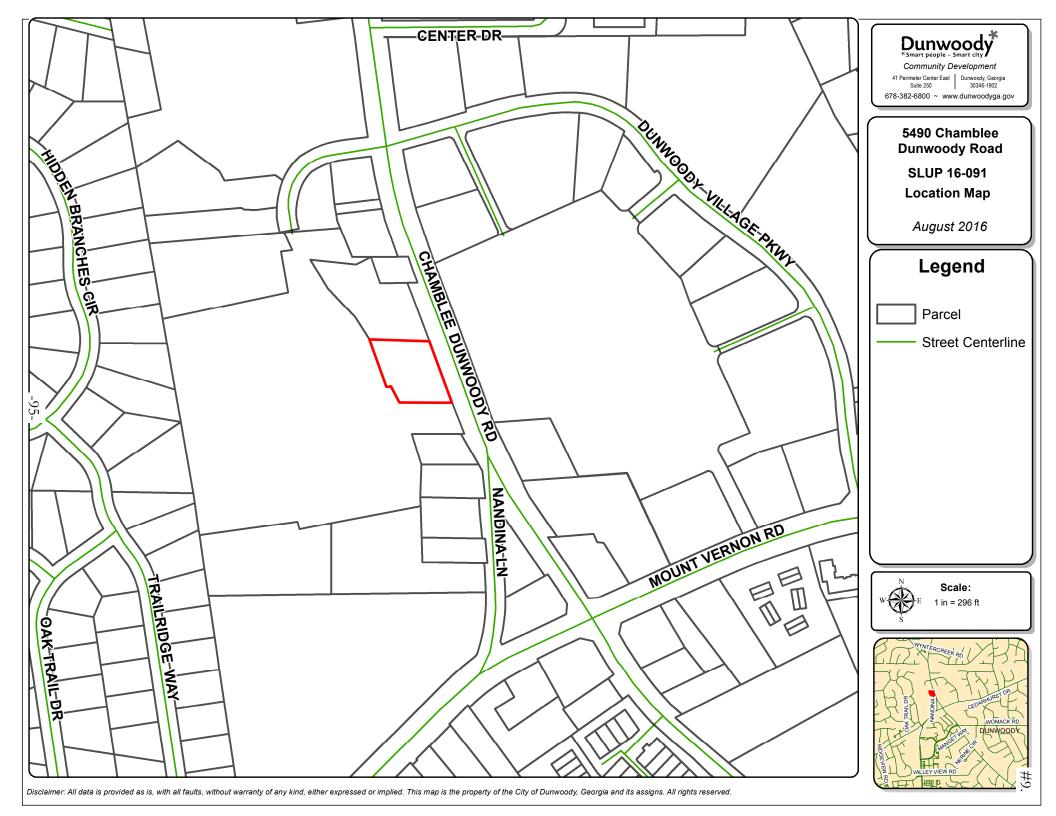
















Community Development

41 Perimeter Center East Dunwoody, Georgia Suite 250 Dunwoody, Georgia 30346-1902 678-382-6800 ~ www.dunwoodyga.gov

5490 Chamblee Dunwoody Road SLUP 16-091 Aerial Map

August 2016

## Legend

Parcel

Street Centerline

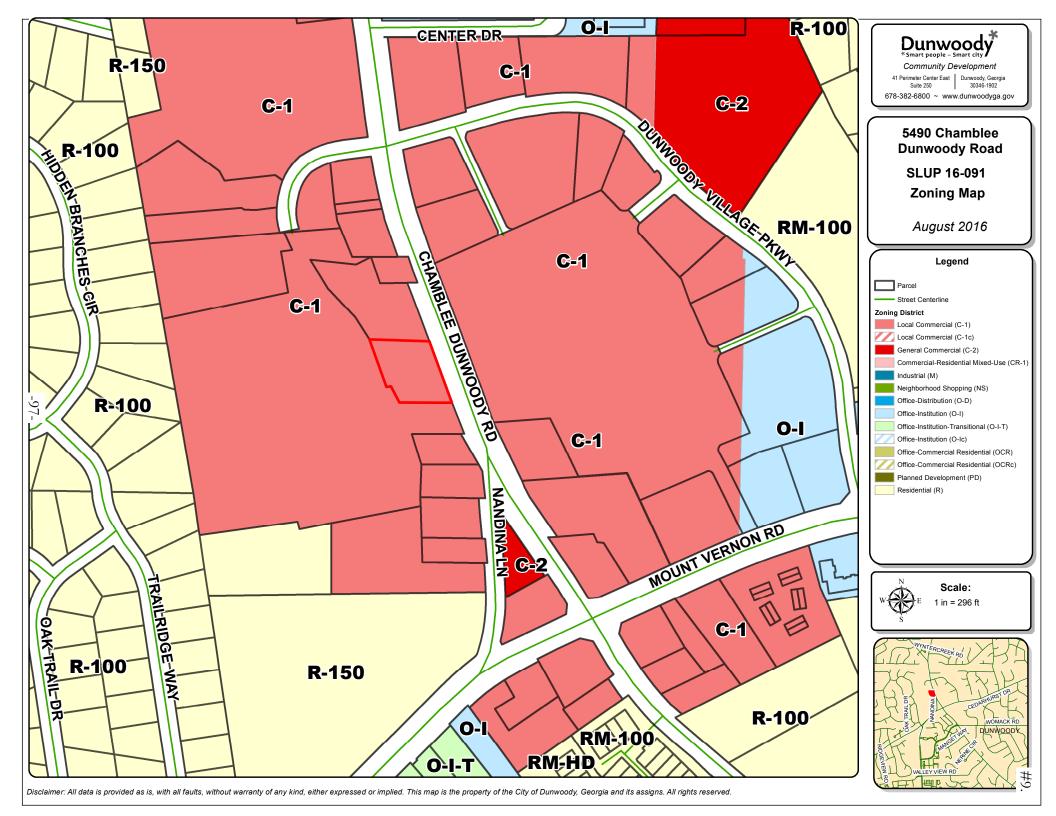


Scale:

1 in = 167 ft



Disclaimer: All data is provided as is, with all faults, without warranty of any kind, either expressed or implied. This map is the property of the City of Dunwoody, Georgia and its assigns. All rights reserved.



#### CHAPTER 27 - ZONING ORDINANCE[1]

#### Footnotes:

**Editor's note**—Ord. No. 2013-10-15, § 1, adopted Oct. 14, 2013, repealed former Ch. 27, §§ 27-1—27-1654, and enacted a new Ch. 27 as set out herein. Former Ch. 27 pertained to similar subject matter. See the Code Comparative Table for a complete derivation. For stylistic purposes, a uniform system of headings, catchlines, capitalization, citation to state statutes, and expression of numbers in text have been used to conform to the Code of Ordinances. Additions made for clarity are indicated by brackets and obvious misspellings and punctuation errors have been corrected without notation.

#### **ARTICLE II. - ZONING DISTRICTS**

#### DIVISION 2. - NONRESIDENTIAL AND MIXED-USE ZONING DISTRICTS

#### Sec. 27-71. - General.

(a) The districts. The city's nonresidential and mixed-use zoning districts are listed below.

\_\_\_\_

	Zoning District	Map Symbol
	Office-Institution	0-1
Office	Office-Institution-Transitional	O-I-T
	Office-Distribution	O-D
	Office-Commercial-Residential	OCR
	Neighborhood Shopping	NS
Commercial	Local Commercial	C-1
	Commercial-Residential Mixed-Use	CR-1
	General Commercial	C-2
Industrial	Industrial	М

#### (b) Purposes.

- (1) General. The nonresidential and mixed-use districts are generally intended to promote consistency with the comprehensive plan and provide opportunities for shopping, employment, entertainment and living.
- (2) Office-institution and office-institution-transitional. The primary purposes of the O-I and O-I-T districts are as follows:
  - a. To provide convenient locations for office and institutional uses;
  - b. To provide locations for the development of cultural, recreational, educational and health service facilities; and
  - To limit building heights to two stories in O-I-T zoned areas adjacent to single-dwelling residential districts.
- (3) Office-distribution. The primary purpose of the O-D district is to provide convenient locations for office and distribution establishments.
- (4) Office-commercial-residential. The primary purposes of the OCR district are as follows:
  - a. To provide for economic development within the city through redevelopment of parcels of land that have been used in the past for commercial and light industrial uses but that have become obsolete and now offer an opportunity for establishing new moderate-intensity mixed-use developments consisting of a combination of office, commercial and residential uses;
  - b. To promote redevelopment and new development in an environment that is pedestrianoriented and that provides employment, shopping, entertainment and living opportunities in close proximity thereby reduces auto dependency; and
  - To encourage the conversion of vacant commercial and industrial buildings into mixed-use projects.
- (5) Neighborhood shopping. The primary purposes of the NS district are as follows:
  - a. To provide convenient neighborhood retail shopping and service areas within the city;
  - b. To provide for the development of new neighborhood shopping districts;
  - To help ensure that the size and scale of neighborhood shopping centers and individual uses within shopping centers are compatible with the scale and character of surrounding neighborhoods; and
  - To accommodate uses designed to serve the convenience shopping and service needs of the immediate neighborhood.
- (6) Local commercial. The primary purposes of the C-1 district are as follows:
  - a. To provide convenient local retail shopping and service areas within the city;
  - b. To provide for the development of new local commercial districts; and
  - To accommodate uses designed to serve the convenience shopping and service needs of groups of neighborhoods.
- (7) Commercial-residential mixed-use. The primary purposes of the CR-1 district are as follows:
  - To provide convenient local retail shopping and service areas within a mixed-use (commercial-residential) setting;

- b. To provide for the development of new commercial-residential mixed-use districts; and
- c. To promote development patterns that accommodate residential, employment and entertainment within a walkable, mixed-use environment.
- (8) General commercial. The primary purposes of the C-2 district are as follows:
  - a. To provide convenient general business and commercial service areas within the city;
  - b. To provide for the development of new general commercial districts; and
  - To accommodate uses designed to serve the general business and commercial service needs of the city.
- (9) Industrial. The primary purposes of the M district are as follows:
  - To provide areas for the establishment of businesses engaged in the manufacturing, processing, creating, repairing, renovating, painting, cleaning, or assembling of goods, merchandise, or equipment;
  - b. To help ensure that establishments operate so as to not create adverse noise and other impacts on nearby residential, office, commercial and mixed-use districts; and
  - To help ensure that M districts are located in areas with access to major arterials and freeways.

(Ord. No. 2013-10-15, § 1(Exh. A § 27-5.10), 10-14-2013)

Sec. 27-72. - Uses allowed.

The following table identifies uses allowed in nonresidential and mixed-use zoning districts. See [subsection] 27-111(4) for information about how to interpret the use table.

DISTRICTS										Supplemental
USES	0- I	O- I-T	O- D	OCR	NS	C- 1	CR-	C- 2	М	Regulations
P = use permitted as of right / A = administrative	•				spe	cial	exce	ptic	n re	eq'd / S = special
land use	e pe	rmit	req'	d						
RES	SIDE	NTIA	۱L							
Hous	ehol	d Liv	ing							
Detached house	-	Р	-	-	-	-	-	-	-	27-147
Multi-unit building	-	-	-	S	-	-	S	-	-	
Mixed-use building, vertical	-	-	-	Р	-	-	Р	-	-	

Gro	oup	Livin	g							
Convent and monastery	Р	Р	-	Р	-	-	-	-	-	27-146
Fraternity house, sorority house or residence hall	Р	-	-	-	-	_	-	-	-	
Nursing home	Р	Р	-	-	-	-	-	-	Р	
Personal care home, family (1—4 persons)	-	-	Р	-	Р	Р	Р	Р	-	
Personal care home, group (5—7 persons)	-	-	Р	-	Р	Р	Р	Р	-	
Personal care home, community (8+ persons)	Р	Р	Р	_	Р	Р	Р	Р	-	27-145
Child caring institution (1—6 persons)	Р	Р	Р	-	Р	Р	Р	Р	-	
Child caring institution (7—15 persons)	Р	Р	Р	-	Р	Р	Р	Р	-	
Child caring institution (16 or more)	Р	S	Р	_	Р	Р	Р	Р	-	
Community living arrangement (1—4 persons)				Р		Р	Р			
Shelter, homeless	S	S	-	-	-	Р	Р	Р	-	27-140
Transitional housing facility	S	S	-	-	-	Р	Р	Р	-	27-140
QUASI-PUBLIC	ANI	O INS	STITU	JTION	IAL	1	ı			
Ambulance Service	-	-	-	-	-	Р	Р	Р	Р	
Club or Lodge, Private	Р	Р	Р	_	-	Р	Р	Р	Р	
Cultural Exhibit	Р	Р	Р	-	-	Р	Р	Р	-	
Day care facility, adult (6 or fewer persons)	-	_	Р	_	-	-	-	-	-	27-137
Day care center, adult (7 or more)	Р	Р	Р	Р	Р	Р	Р	Р	-	
Day care facility, child (6 or fewer persons)	-	-	Р	-	-	-	-	-	-	

Day care center, child (7 or more)	Р	Р	Р	Р	P	Р	Р	Р			
Educat	Educational Services										
College or university	Р	Р	Р	-	-	-	-	-	-		
Kindergarten	-	-	Р	Р	P	Р	Р	P	-	27-141	
Research and training facility, college or university affiliated	Р	Р	P	-	-	-	-	-	P		
School, private elementary, middle or senior high	Р	Р	P	Р	-	P	P	P	Р	27-148	
School, specialized non-degree	Р	Р	Р	Р	-	Р	Р	Р	Р		
School, vocational or trade	Р	Р	Р	-	-	Р	Р	Р	Р		
Hospital	Р	-	-	-	-	-	-	-	-		
Place of Worship	Р	Р	Р	Р	P	Р	Р	Р	Р	27-146	
Utility Facility, Essential	Е	E	Р	E	E	Р	Р	Р	Р	27-151	
COI	MME	RCI	٩L	ı	1		ı				
А	dult	Use									
Body art service								Р	Р		
Sexually oriented business	Р	-	-	Р	-	-	-	Р	Р	27-149	
Anin	nal S	ervi	ces		1		1				
Animal care/boarding	-	-	-	S	S	Р	Р	Р	Р	27-131	
Animal grooming	-	-	-	Р	P	Р	Р	Р	Р	27-131	
Animal hospital/veterinary clinic	-	-	-	Р	P	Р	Р	Р	Р	27-131	

Commun	icati	ion S	ervi	ces							
Radio and television broadcasting stations	Р	Р	Р	-	-	Р	Р	Р	Р		
Recording studios	Р	Р	Р	-	-	Р	Р	Р	Р		
Telecommunication tower	А	-	Α	-	S	Α	Α	А	Α	27-150	
Telecommunication antenna, co-located	Р	Р	Р	Р	Р	Р	Р	Р	Р	27-150	
Construction and Building Sales and Services											
Building or construction contractor	-	-	-	-	-	-	-	Р	Р		
Commercial greenhouse or plant nursery	-	-	-	-	-	-	-	P	Р		
Electrical, plumbing and heating supplies and services	-	-	-	-	-	Р	Р	-	Р		
Lumber, hardware or other building materials establishment	-	-	-	-	-	P	Р	Р	P		
Eating and Dri	nkin	g Est	tablis	shme	nts	1		<u> </u>			
Restaurant, accessory to allowed office or lodging use	P	-	-	Р	-	P	Р	Р	Р		
Restaurant, drive-in or drive-through	-	-	-	-	-	Р	S	Р	Р		
Food truck	Р	Р	Р	Р	Р	Р	Р	Р	Р	27-138	
Other eating or drinking establishment	-	_	-	Р	Р	Р	Р	Р	-		
Entertainment	and	Spe	ctato	or Spo	orts		1	1			
Auditorium or stadium	-	-	-	-	-	-	-	Р	Р		
Drive-in theater	-	-	-	-	-	-	-	Р			
Movie theater	-	-	-	Р	-	-	-	Р	-		

Special events facility	-	Р	-	-	-	Р	Р	Р	-			
		_										
Financial Services												
Banks, credit unions, brokerage and investment services	Р	Р	P	Р	Р	Р	Р	Р	Р			
Convenient cash business	-	-	-	-	-	-	-	Р	-	27-136		
Pawn shop	-	-	-	-	-	-	-	Р	-	27-144		
Food and Be	Food and Beverage Retail Sales											
Liquor store (as principal use)	-	-	-	-	-	Р	Р	Р	Р			
Liquor store (accessory to lodging or 3+ story office)	-	-	Р	Р	-	-	-	-	-			
Other food and beverage retail sales	-	-	Р	Р	Р	Р	Р	Р	Р			
Funeral and	Inte	rme	nt Se	ervice	S	1						
Cemetery, columbarium, or mausoleum	Р	Р	Р	-	-	-	-	-	-			
Crematory	-	-	-	-	-	-	-	-	S			
Funeral home or mortuary	Р	-	-	-	-	Р	Р	Р	Р			
Lodging	Р	-	Р	Р	-	Р	Р	Р	Р			
Medical Service												
Home health care service	Р	Р	-	-	-	-	-	-	-			
Hospice	Р	Р	-	-	-	-	-	-	-			
Kidney dialysis center	Р	Р	-	-	-	-	-	-	-			
Medical and dental laboratory	Р	Р	-	Р	-	Р	Р	-	Р			

Medical office/clínic         P		l _		l _		Ι_						
Parking, Non-accessory  S - P P P P P P P P 27-143  Personal Improvement Service  Barber shop, beauty shop, nail salon, massage and/or spa establishments, estheticians, and other "typical" uses per [subsection] 27-114(14)  Other personal improvement service P P P P P P P P P P P P P P P P	Medical office/clinic	P	P	P	P	P	P	P	P	P		
Personal Improvement Service  Barber shop, beauty shop, nail salon, massage and/or spa establishments, estheticians, and other "typical" uses per [subsection] 27-114(14)   P   -   -   P   P   P   P   P   P   P	Office or Consumer Service	Р	Р	Р	Р	Р	Р	Р	Р	Р		
Barber shop, beauty shop, nail salon, massage and/or spa establishments, estheticians, and other "typical" uses per [subsection] 27-114(14)  Other personal improvement service	Parking, Non-accessory	S	-	Р	-	-	Р	Р	Р	Р	27-143	
and/or spa establishments, estheticians, and other "typical" uses per [subsection] 27-114(14)	Personal Improvement Service											
Repair or Laundry Service, Consumer	and/or spa establishments, estheticians, and	Р	-	-	Р	Р	Р	Р	Р	Р	27-114(14)	
Laundromat, self-service	Other personal improvement service	-	-	-	-	-	Р	Р	Р	Р		
Laundry or dry cleaning drop-off/pick-up P P P P P P P P P P  Other consumer repair or laundry service P P P P P P P P  Research and Testing Services P - P P P P P P P P  Retail Sales  Retail sales of goods produced on the premises P P P P P P P P P  Shopping Center P P P P P P P P P P P P P P P P P	Repair or Launc	dry S	ervi	ce, C	onsui	mer	1	1				
Other consumer repair or laundry service	Laundromat, self-service	-	-	-	Р	Р	Р	Р	Р	-		
Retail Sales  Retail sales of goods produced on the premises	Laundry or dry cleaning drop-off/pick-up	Р	-	-	Р	Р	Р	Р	Р	Р		
Retail Sales  Retail sales of goods produced on the premises   -   -   -   -   -   -   -   -   P    Shopping Center   -   -   P   P   P   P   P   P   P   P	Other consumer repair or laundry service	-	-	-	Р	Р	Р	Р	Р	Р		
Retail sales of goods produced on the premises	Research and Testing Services	Р	-	Р	Р	-	-	-	Р	Р		
Shopping Center	Re	tail	Sales	5								
Other retail sales P P P P P P -  Sports and Recreation, Participant  Golf course and clubhouse, private P P P P P P P P P P P P P P P P P P P	Retail sales of goods produced on the premises	-	-	-	-	-	-	-	-	Р		
Sports and Recreation, Participant  Golf course and clubhouse, private PPPPPPPPPPPPPPPPPPPPPPPPPPPPPPPPPPPP	Shopping Center	-	-	-	Р	Р	Р	Р	Р	-		
Golf course and clubhouse, private PPPPPPPP  Health club - PPPPPPPPPPP	Other retail sales	-	-	Р	Р	Р	Р	Р	Р	-		
Health club - PPPPPP	Sports and Recreation, Participant											
	Golf course and clubhouse, private	Р	Р	Р	-	-	-	-	Р	Р		
Private park PPP	Health club	-	-	Р	Р	Р	Р	Р	Р	Р		
	Private park	Р	Р	Р	-	-	-	-	-	-		

						1		1		l i	
Recreation center or swimming pool, neighborhood	P	Р	Р	-	-	-	-	-	P		
Recreation grounds and facilities	-	-	Р	_	-	-	-	Р	-		
Tennis center, club and facilities	Р	Р	Р	Р	-	Р	Р	Р	-		
Other participant sports and recreation (Indoor)	Р	-	-	Р	-	Р	Р	Р	-		
Other participant sports and recreation (Outdoor)	-	-	-	-	-	-	-	Р			
Vehicle and Equip	Vehicle and Equipment, Sales and Service										
Car wash	-	-	-	-	-	Р	-	Р	Р	27-134	
Gasoline sales	-	-	-	-	-	Р	-	Р	Р	27-139	
Vehicle repair, minor	-	-	-	_	-	Р	-	Р	Р	27-153	
Vehicle repair, major	-	-	-	-	-	-	-	Р	Р	27-152	
Vehicle sales and rental	-	-	-	-	-	S	S	Р	Р	27-154	
Vehicle storage and towing	-	-	-	-	-	-	-	Р	Р	27-155	
INI	DUS	TRIA	L								
Manufacturing and Production, Light	-	-	-	-	-	-	-	Р	Р		
Wholesaling, Wareho	Wholesaling, Warehousing and Freight Movement										
Warehousing and storage	-	-	Р	-	-	-	-	-	-		
Self-storage warehouse	-	-	Р	-	-	-	-	-	Р		
Storage yard and truck terminal S											
AGRICULTURE A	AGRICULTURE AND TRANSPORTATION										

Agriculture										
Agricultural produce stand	-	-	-	-	-	-	-	-	Р	
Community garden	Р	Р	Р	Р	Р	Р	Р	Р	Р	27-135
Crops, production of	-	-	-	-	-	-	-	-	Р	
Transportation										
Heliport	S	-	S	-	-	S	S	-	Р	
Stations and terminals for bus and rail passenger service	S	-	-	-	-	-	-	-	-	
Taxi stand and taxi dispatching office	-	-	_	-	-	Р	Р	-	Р	

(Ord. No. 2013-10-15, § 1(Exh. A § 27-5.20), 10-14-2013; Ord. No. 2015-01-05, § 1, 1-26-2015; Ord. No. 2015-06-13, § 1, 6-22-2015)

Sec. 27-73. - Lot and building regulations.

- (a) This section establishes basic lot and building regulations that apply in nonresidential and mixed-use zoning districts. These regulations offer certainty for property owners, developers and neighbors about the limits of what is allowed; they are not to be construed as a guarantee that stated minimums and maximums can be achieved on every lot. Other factors, such as topography, the presence of protected resources, off-street parking and other factors may work to further limit actual building and development potential.
- (b) The lot and building standards of the following table apply to all principal and accessory uses allowed in nonresidential and mixed-use districts, unless otherwise expressly stated in this zoning ordinance. Article VII, division 1, identifies exceptions to these regulations and rules for measuring compliance (see also Figure 5-1).

	Regulation	O-I	O-I-T	O-D	OCR	NS	C-1	CR-1	C-2	М
L1	Minimum Lot Area (sq. ft.)	20,000	20,000[1	43,560	87,120	20,000	20,000	20,000	30,000	30,000
L2	Minimum Lot Frontage (ft.)	100	100	150	100	100	100	100	100	100

	Maximum Density (dwelling units per acre)	NA	NA	NA	30	NA	NA	80	NA	NA
	Minimum Building/Structur e Setbacks (ft.)									
S 1	Street, front and side	50	40	75	0	50	50	0	50	75
S 2	Side, interior	20	20	20	20	20	20	20[2]	20	20
S 3	Rear	30	30	30	40	30	30	30	30	30
С	Maximum Lot Coverage (%)	80	80	80	80	80	80	80	80	80
	Maximum Building Height (stories/ft.)	5/70[3	2/35	2/35[4	2/35[4	2/25	2/35[4	3/45[4	2/35[4	5/70[3
	Maximum Building Floor Area (sq. ft.)	NA	NA	NA	NA	50,000[5	NA	NA	NA	NA

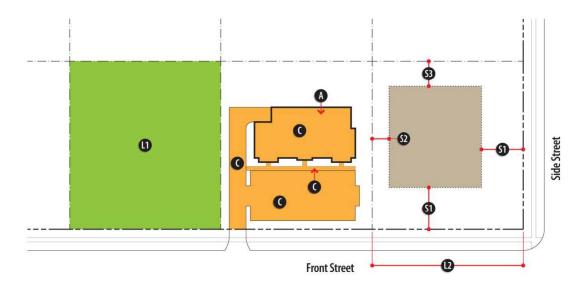
<sup>[1]</sup> Attached house developments are subject to a minimum lot area requirement of 4,000 square feet per dwelling unit.

<sup>[2]</sup> No interior side setback required abutting C-1, CR-1 or C-2-zoned lots.

<sup>[3]</sup> Buildings may exceed three stories in height only if approved by fire and rescue services. Buildings in excess of five stories or 70 feet in height may be approved only through the special land use permit procedures of article V, division 3. Multi-unit residential and vertical mixed-use buildings that abut any attached single-dwelling residential district may not exceed 40 feet in height. Multi-unit residential buildings and vertical mixed-use buildings that abut any detached single-dwelling residential district may not exceed 35 feet in height.

- [4] Buildings in excess stated height limits may be approved through the special land use permit procedures of article V, division 3. Buildings may exceed three stories in height only if approved by fire and rescue services.
- [5] No individual building may exceed 50,000 sq. ft. (GSF). No multi-tenant center may exceed 100,000 sq. ft.

Figure 5-1: Lot and Building Regulations Diagram, Nonresidential and Mixed-use Districts



(Ord. No. 2013-10-15, § 1(Exh. A § 27-5.30), 10-14-2013; Ord. No. 2015-01-05, § 1, 1-26-2015)

Sec. 27-74. - Other regulations.

Uses and development in nonresidential and mixed-use zoning districts may be subject to other regulations and standards, including the following.

- (1) Nonconformities. See article VI, division 4.
- (2) Accessory uses and structures. See article III, division 3.
- (3) Parking. See article IV, division 1.
- (4) Landscaping and screening. See article IV, division 2.
- (5) Signs. See chapter 20 of the Municipal Code.
- (6) Outdoor storage. See section 27-286.
- (7) Temporary uses. See article III, division 4.
- (8) Outdoor lighting. See article IV, division 3.

(Ord. No. 2013-10-15, § 1(Exh. A § 27-5.40), 10-14-2013)

Secs. 27-75—27-85. - Reserved.

**DIVISION 4. - OVERLAY ZONING DISTRICTS** 

Sec. 27-96. - General.

- (a) Establishment. Overlay zoning district regulations and overlay zoning district boundaries may be established or amended only in accordance with the amendment procedures of article V, division 2.
- (b) Interpretation. Overlay zoning district regulations apply in combination with underlying (base) zoning district regulations and all other applicable regulations of this zoning ordinance. When overlay district standards conflict with standards that would otherwise apply under this zoning ordinance, the regulations of the overlay zoning district govern. Otherwise, all applicable regulations of this zoning ordinance apply in overlay districts.

(Ord. No. 2013-10-15, § 1(Exh. A § 27-7.10), 10-14-2013)

Sec. 27-97. - DVO, Dunwoody Village Overlay.

- (a) Purpose and intent. The Dunwoody Village Overlay district is primarily intended to implement the policies and objectives of the comprehensive plan and the Dunwoody Village master plan. It is further intended to help:
  - (1) Maintain and enhance the identity and image of the Dunwoody Village area;
  - (2) Accommodate and promote walkable, development patterns containing a complementary mix of land uses;
  - (3) Create new opportunities for public open spaces and gathering spaces in the commercial core of Dunwoody;
  - (4) Ensure that new development and substantial additions to existing buildings are compatible with the pre-1900 Mid Atlantic American Colonial Architecture that is characteristic of the district;
  - (5) Support efforts to create a vibrant shopping and entertainment area in which merchants and businesses thrive and grow, thereby helping to maintain property values and keeping vacancy rates low; and
  - (6) Maintain and enhance the area's role as a place for civic activities and public gatherings within well-designed open spaces.
- (b) Redevelopment. The city council anticipates that in the future a developer may desire to redevelop all or portions of the Dunwoody Village area, and that the type of redevelopment proposed may be difficult or impossible to carry out under the existing zoning. To accommodate and encourage large-scale redevelopment in accordance with the approved Dunwoody Village Master Plan, the city council may consider rezoning or planned development (PD) development proposals.
- (c) Thresholds for compliance.
  - (1) Full compliance. Except as otherwise expressly stated by the specific provisions of this section (section 27-97), permits for the following building and construction activities may be issued only if the entire building is determined to comply with the applicable regulations:
    - a. Construction of a new building;
    - b. Construction of building additions that result in a ten-percent or greater increase in the floor area or building coverage of the existing building;
    - c. Exterior construction or remodeling with a total value of 15 percent or more of the county tax assessor's 100 percent assessed value of the existing improvements only; and
    - d. Interior construction or remodeling with a total value of 25 percent or more of the county tax assessor's 100 percent assessed value of the existing improvements only.

- (2) Partial compliance. Permits for exterior remodeling or building activities that do not trigger full compliance may be issued only if the portion of the building affected by the work is determined to be in compliance with all applicable overlay district regulations.
- (d) Design review. No land-disturbance permit, building permit or sign permit may be issued for buildings or construction activities that are subject to one or more of the overlay district regulations of this section until the design review process of article V, division 4, has been completed.
- (e) Architecture and design.
  - (1) Exterior materials.
    - a. Exterior cladding material must consist of stone, earth tone brick (the preferred material) or white/cream painted horizontal lap siding. Lap siding must be wood, fiber cement lap siding or other substitute approved by the design review advisory committee because of its wood-like appearance and durability. If lap siding is used, the base of the structure must have brick or stone cladding from the grade to the first floor elevation. Vertical siding, stucco, external insulating finishing system (EIFS), metal siding, metal trim, vinyl siding, vinyl trim, marble siding, marble trim, exposed concrete and block are prohibited.
    - b. Exterior siding material must be consistent and uniform on all exterior elevations. Siding material must be predominantly brick, stone or lap siding. Buildings and building additions with masonry on only street-facing facades are prohibited. Buildings and building additions with first floor masonry and second floor lap siding are prohibited.
    - c. All exposed bricks must be approximately eight inches wide by three inches deep by 2.67 inches high and must be laid in a running bond. Engineer-size bricks and Flemish bond are also allowed. All joints must be tooled with grapevine joint, and mortar must be buff or ochre in color. Stone veneers must have ochre tooled mortar joints.







- d. When lap siding is used, the maximum allowable exposure on lap siding is eight inches.
- e. Applied trim materials, cornice and window casings must consist of painted wood or painted fiber cement or other substitute material approved by the design review advisory committee because of its wood-like appearance, durability and ability to hold paint. Metal, vinyl, stucco, block stone and concrete are prohibited, except that wrought-iron handrails are permitted. Nonmasonry trim colors are limited to colors available in the Martin Senour Williamsburg Paint Collection or similar approved alternatives.





- f. Exposed portions of the foundation must be covered in masonry veneer. Exposed block, stucco and concrete are prohibited.
- (2) Roofs. The following requirements apply to roofs visible from public rights-of-way, outdoor activity areas (e.g., seating areas) or other areas of the site intended for public access:
  - a. Gabled roofs, hipped roofs, or combinations of such roof forms are permitted. Flat roofs and shed roofs are prohibited.



- b. Exterior roofing material is limited to asphalt (fiberglass) shingles, slate or cedar shake. Roofs must be black, a dark shade of gray or weathered wood color. All asphalt (fiberglass) shingles must be dimensional. Standing seam copper or bronze color metal roofs are permitted only as accents on porches or dormers.
- c. Roof overhangs must be at least eight inches but not greater than 12 inches. Gabled ends may have either an overhang or a flush rake.





- d. Eave lines must be consistent, largely unbroken and horizontal. All eaves must be architecturally detailed with one or more of the following elements: dental molding, crown molding, built-up fascia, or frieze board. The total width of fascias/cornices and rake trim must be at least 9.25 inches.
- e. Roofs must contain at least one roof projection for every 75 lineal feet of building frontage. Roof projections may include cupolas, dormers, balustrade walks, chimneys or gables.

#### (3) Building massing.

- Buildings that are longer than 75 feet must be designed so as to appear as multiple structures through the use of varied roof forms, building projections or architectural details.
- b. Buildings that are longer than 100 feet must provide a pedestrian arcade or covered porch with minimum dimensions of eight feet in any direction. Exterior metal columns are prohibited. Exterior columns must include a base and a capital, and must generally align with story heights. Two-story exterior columns are prohibited. All exterior columns must be traditional in style.
- c. The apparent exterior floor-to-floor height of each story of a building may not exceed 12 feet. Individual floors must be delineated on the building facade through the use of window placement and horizontal details.



- d. Buildings must have at least one building projection on the front facade below the eave line. Building projections may consist of stoops, bay windows, covered porches, extruded entrances, pedestrian arcades or other approved features.
- e. Primary building walls must be rectilinear and simplified in form. Curved walls or non-90-degree corners are prohibited, except that bay projections may be allowed.
- (4) Fenestration. The following requirements apply only to areas visible from public rights-of-way, outdoor activity areas (e.g., seating areas) or other areas of the site intended for public access:
  - a. Doors must be compatible with pre-1900 Mid Atlantic American Colonial Architecture style. All-glass doors and flush doors are prohibited. Solid doors must be six-panel and may have sidelights or a transom sashes. Windowed doors must contain a solid border, a minimum of six-inch-wide panels, and must include mullions or divided lights not exceeding 12 inches in any direction. Mullions must be 0.75 inches in size. French, three-quarter glass or nine-light doors are allowed if they comply with the requirements of this paragraph.
  - b. Individual doors must be of a single color and are limited to colors available in the Martin Senour Williamsburg Paint Collection or similar approved alternatives. Dark stained doors are allowed if the shade is equal to or darker than Minwax special walnut.
  - Primary doors may not exceed 42 inches in width and 96 inches in height. Larger doors are prohibited.
  - d. Windows must be vertically proportioned standard sizes, with a minimum width of 28 inches and minimum height of 66 inches. Transom windows are not subject to vertical proportion requirements and do not count in the overall window proportion. Vertical windows may be grouped to create storefront windows but are limited to 15-foot sections with a minimum of four-inch mulls. Group windows must be separated by at least five feet of unbroken wall space. Slit windows, strip windows, and ribbon windows are prohibited.

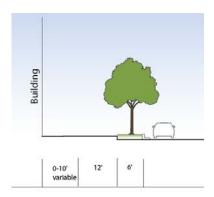






- e. Blank facades are prohibited. Windowless sections of the front facade may not exceed 20 feet in width. Windows and doors must be provided on at least ten percent of the front facade but may not constitute no more than 50 percent of the total area of the front facade. For this purpose, windows must be measured at the sash and only the swinging part of the doors may be counted. Casing is not included in the measurement. Windows must generally be spaced in an even rhythm.
- f. All windows must be rectilinear double hung, provided that arch top and fixed sash windows are allowed. Triangular or otherwise angular windows are prohibited. Round windows are permitted as accent windows in locations such as gables. Louvered gable vents are allowed, but they must be rectilinear and surrounded by one-by-four and backband.
- g. All windows must have the appearance of mullions or divided lights. Mullions must be at least 0.75 inches in width. Panes must be vertical rectilinear, generally square in proportion. Diagonal panes are not permitted except in arch windows.
- h. Shutters must be constructed of wood or a substitute material approved by the design review advisory committee because of its wood-like appearance and durability. Shutters must be sized to fit the window and include horizontal slats or raised panels. Metal and vinyl shutters are prohibited. Shutter colors are limited to colors available in the Martin Senour Williamsburg Paint Collection or similar approved alternatives. Shutters must appear operable, with hinges and tie backs.
- i. All windows must have sill and header trim details. Cut brick jack arches must be installed on all windows visible from the street or parking lot.
- j. The bottom of windows must be at least 20 inches above grade.
- (f) Signs. All signs within the Dunwoody Village Overlay district are subject to the requirements of the city's sign ordinance and the following additional regulations:
  - (1) Signs must be designed and constructed to be compatible with the architectural style that is characteristic of the Dunwoody Village Overlay district area.
  - (2) Ground signs must be monument-style signs with a brick base.
  - (3) For lots containing nine or fewer storefronts, ground signs may not exceed eight feet in height and eight feet in width.
  - (4) For lots containing ten or more storefronts, ground signs may not exceed 12 feet in height and eight feet in width.

- (5) Window signs may not exceed two square feet in area.
- (6) Banners are prohibited except for pole banners as permitted in chapter 26 of the Municipal Code.
- (7) Signs must have a matte finish and be constructed of wood or other substitute material approved by the design review advisory committee because of its wood-like appearance and durability.
- (8) Sign colors, except for logos, are limited to colors available in the Martin Senour Williamsburg Paint Collection or similar approved alternatives.
- (9) All illuminated signs must be indirectly illuminated or halo lighted.
- (10) Neon, gas, colored, flashing, animated, marquee, sound emitting, fluorescent, rotating or otherwise moving signs are prohibited. This prohibition notwithstanding, a single LED or neon sign up to two square feet in area is allowed, provided that the sign does not employ any flashing, animation, movement or sound and provided that the sign may be illuminated only during hours in which the establishment is open for business.
- (11) Sign shape and lettering is limited as follows:
  - Sign facing must be flat in profile and may not exceed eight inches in thickness.
  - b. Signs with more than two faces are prohibited.
  - c. Sign lettering and logo content may not exceed 18 inches in height.
- (12) Standard informational signs:
  - May not be more than three feet above grade;
  - May not exceed six square feet in area;
  - c. May not have more than two sign faces;
  - d. May not be made from flexible materials, such as vinyl, cloth or paper;
  - e. Must be free-standing and not attached to permanent or temporary structures;
  - f. Must comply with the color standards of the district; and
  - g. Must be maintained in good repair.
- (g) Parking and circulation. The parking and circulation regulations of article IV, division 4, apply within the Dunwoody Village Overlay district except as modified by the following regulations:
  - (1) New nonresidential buildings and nonresidential uses and additions to existing nonresidential buildings and nonresidential uses may not provide more than three parking spaces per 1,000 square feet of floor area. This provision does not require that existing "excess" parking spaces be removed.
  - (2) Parking areas must be separated from the main road by a minimum distance of 30 feet and include at least the landscaping required by section 27-229. Wherever possible, parking must be confined to the rear of structures or be placed underground.
- (h) Landscaping. The landscaping and screening regulations of article IV, division 2, apply within the Dunwoody Village Overlay district. See also subsection (i).
- Streetscape and pedestrian amenities.
  - (1) A minimum six-foot wide landscape area must be established abutting the back of the curb along abutting streets. This landscape area must adjoin a minimum 12-foot wide sidewalk. Buildings must abut the sidewalk or be located within ten feet of the sidewalk. If buildings are set back from the edge of the required 12-foot sidewalk, the setback area must include features such as outdoor dining and seating areas, plazas and landscaped open spaces that provide a safe, comfortable and active environment for pedestrians.



- (2) Street trees, spaced no more than 50 feet on center, must be provided in the required landscape area between the travel lanes and the sidewalk.
  - Maintenance of trees is the full responsibility of the owner of the adjacent site or the owner
    of the property on which the tree is located if it is located on private property.
  - b. Trees species must be selected from the city's approved street tree list, which is available in the community development department; alternative cultivars may be approved on a case-by-case basis with the approval of the city arborist.
  - c. Trees must be at least 2.5-inch caliper and at least eight feet in height at the time of planting. As street trees mature, they must be trimmed as necessary to provide a minimum vertical clearance of seven feet above the sidewalk.
- (3) Pedestrian buffer zones with a minimum width of ten feet must be provided abutting the sides and rear of all buildings. These areas provide a buffer between buildings and parking and service areas. The pedestrian buffer zones must contain walkways, planting areas, plazas, and similar pedestrian-oriented landscaped spaces. All pedestrian walkways within buffer zones must be at least six feet in width and finished with brick pavers or other approved accents or designs.
- (4) The front entrance of all buildings must be easily and safely accessible to pedestrians from the public sidewalk through a combination of pedestrian walkways and crosswalks. All entrances to crosswalks and sidewalks must include wheelchair ramps, per code.
- (5) Covered sidewalks that are a part of the building and that are located within the buildable area of the lot are encouraged. Such covered sidewalks may be used for outdoor seating and dining, and as terraces and arcades.
- (6) In multi-tenant retail buildings, a continuous, unimpeded walkway must be provided to connect all business entrances.
- (7) Sidewalks must conform to the following:
  - Sidewalks must be paved using concrete or alternative pervious material as approved by city staff.
  - b. Where a sidewalk exists conforming to the standards of this ordinance except for the width, the developer must supplement the existing sidewalk width, adding width to create a 12foot sidewalk.
  - c. Streetscaping performed as a function of city-initiated redevelopment must provide a landscape area at least six feet in depth.
- (8) Lighting must conform to the following:
  - a. Pedestrian and street lighting must be placed in the landscape zone at intervals of 90 to 100 feet on center and must be equal distance from required street trees, in accordance with the Georgia Power Area-Wide Pedestrian Lighting Plan.

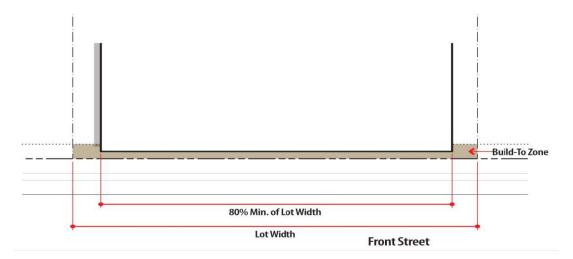
- b. Pole height may not exceed 15 feet.
- c. Light poles and lamps must be selected from the city's approved streetscape list, which is available in the community development department; alternative designs may be approved on a case-by-case basis with the approval of the community development director.
- (9) Furniture must be provided as follows:
  - a. Benches and trash and recycling receptacles must be installed every 250 feet along the public street and at each building entrance adjacent to a pedestrian walkway.
  - b. Furniture must be selected from the city's approved streetscape list, which is available in the community development department; alternative designs may be approved on a caseby-case basis with the approval of the community development director.
- (1) Maintenance of trash and recycling receptacles, including servicing, repair, and replacement, is the full responsibility of the nearest adjacent property owner.
- (2) Recycling receptacles must be clearly identified with symbols and/or text indicating its intended use for recyclables.
- (j) Public areas, service areas and retaining walls.
  - (1) Public plazas and outdoor dining areas must be easily accessible to pedestrians and provide protection from vehicular traffic by means of their location and design.
  - (2) All dumpsters and other building service areas must be concealed from view of public rights-of-way, publicly accessible areas of the site and residential zoning districts. All dumpsters must be concealed with secured gates screening in accordance with section 27-231.
  - (3) Retaining walls must comply with the city building code. Visible areas of retaining wall must be covered with the face brick or stone of the downhill neighbor with a roll lock at the top. Horizontal wall expanses exceeding 20 feet must include offset pilasters with the same brick or stone rising three courses above the top of the adjoining wall and finished with a double course capital of the same brick or stone.
  - (4) All utilities must be placed underground.
- (k) Village core area. The village core area form and design regulations of this subsection apply within designated village core area of the DV-O district to all new buildings. The regulations also apply to renovations of and additions to existing buildings within the village core area that result in an increase of 50 percent or more in the building' gross floor area. These requirements govern in the event of conflict with other DV-O district regulations.
  - (1) Build-to-zone.
    - a. The build-to zone is the area on the lot where a certain percentage of the front building facade must be located, measured as a minimum and maximum yard (setback) range from the edge of the right-of-way. The requirements are as follows:

Build-to-Zone	
Minimum/maximum (feet)	0/10
Minimum percent of building in build-to-zone (percent)	80
Parking Setback	

Minimum (feet)	30

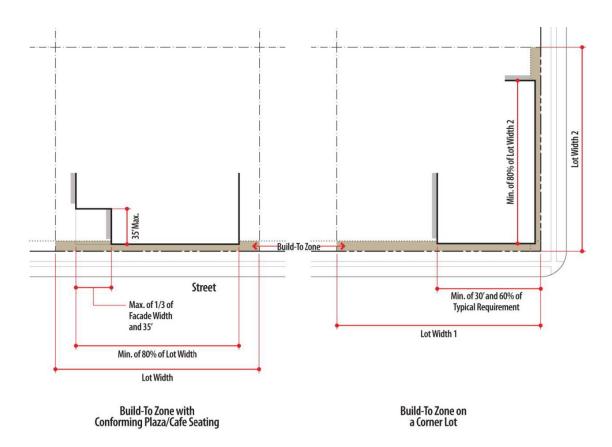
b. The required percentage specifies the amount of the front building facade that must be located in the build-to zone, based on the width of the front building facade divided by the width of the lot.

Figure 7-1: Build-to-Zone (BTZ)



- c. Outdoor open space, plazas and outdoor dining areas are counted as part of the building for the purpose of measuring compliance with build-to zone requirements, provided that:
  - 1. The area does not exceed one-third the length of the building face or 35 feet, whichever is less; and
  - 2. The area is no more than 35 feet in depth (see Figure 7-2).

Figure 7-2: BTZ with allowed plaza (left) and BTZ on corner lot (right)



d. On corner lots, minimum requirements governing the percent of building facade that must be located in the build-to-zone may be reduced by 60 percent along one of the frontages, at the property owner's option, provided that a building facade must be placed in the buildto zone for the first 30 feet along each street extending from the corner (the intersection of the build-to-zones) (see Figure 7-2).

#### (2) Transparency.

a. Transparency regulations govern the percentage of a street-facing building facade that must be covered by glazing (e.g., transparent windows and doors). The minimum requirements are as follows:

Transparency	
Minimum ground story (%)	65
Minimum upper story (%)	20

- b. The transparency of a ground story facade is measured between 2.5 and 12 feet above the adjacent sidewalk.
- c. The transparency of an upper-story facade is measured from top of the finished floor to the top of the finished floor above. When there is no floor above, upper-story transparency is measured from the top of the finished floor to the top of the wall plate (see Figure 7-3).

Top of **Wall Plate** Measurable Area (3rd Floor) Top of **Finished Floor** Measurable Area (2nd Floor) Top of **Finished Floor** Measurable Area (3rd Floor) 15, Front Sidewalk Elevation Eligible Transparent Area (Numerator) // Overall Facade Area (Denominator)

Figure 7-3: Transparency Measurements

d. Glazed element must be clear and non-reflective and not be painted or tinted, provided that low-emission (Low-E) glass coatings are permitted.

#### (3) Blank wall area.

 Blank walls are areas on the exterior facade of a building that do not include a substantial material change; windows or doors; columns, pilasters or other articulation greater than 12 inches in depth. Blank wall limits are established as follows:

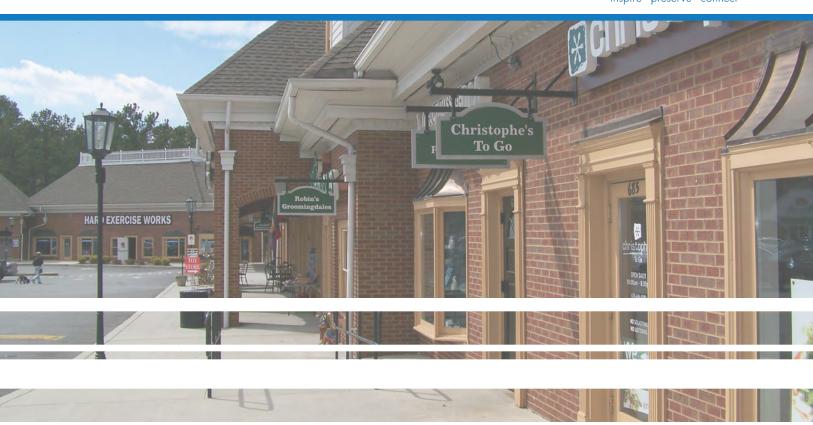
Maximum blank wall length (feet)	20
	1

- b. Blank wall area regulations apply in both a vertical and horizontal direction.
- (4) Street-facing building entrances. At least one street-facing building entrance must be provided on all principal buildings. The building entrance must provide ingress and egress for residents and customers. Additional entrances off another street, pedestrian area or internal parking area are also permitted.

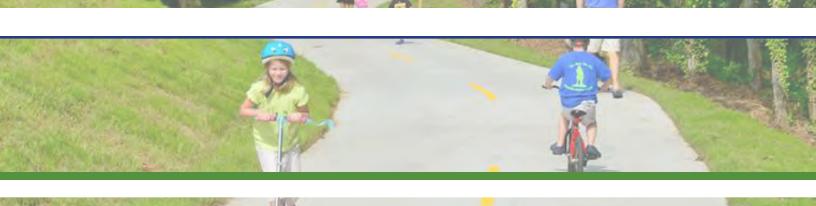
(I) Modifications and adjustments. The regulations of subsections (e) through (k) may be modified only if reviewed and approved in accordance with the special land use permit procedures of article V, division 3.

(Ord. No. 2013-10-15, § 1(Exh. A § 27-7.20), 10-14-2013; Ord. No. 2015-01-05, § 1, 1-26-2015) Secs. 27-98—27-110. - Reserved.





# CITY OF DUNWOODY 2015-2035 COMPREHENSIVE PLAN



### **DUNWOODY VILLAGE**

#### Vision/Intent

Dunwoody Village has historically been the "heart" of Dunwoody. A master planning process established a detailed vision for this center of the community, focused on pedestrian and bicycle amenities, functional public open space, a multimodal transportation environment, architectural controls, connectivity and place making. A sense of history will add to the charm and sense of place. This area envisions a "village green" with civic activities and amenities, and redevelopment will draw community members to shopping, dining and entertainment. Furthermore, redevelopment should have a residential component for day and evening activity to foster community. The design should embody the unique character of Dunwoody.

## **Future Development**

Form: Master planned design, high quality building materials, civic amenities, integrated open space and appropriate transitions from greater to less intense uses. The periphery of the character area will include a large transitional area to adequately protect single-family residential and other residential homes in the area.

Use (see Future Land Use Map): Mixed-Use (containing a mix of office, retail and residential, including both either vertical or horizontal mixed-use through the district), townhomes, other owner-occupied housing, live-work units, civic institutional, community retail (not greater than 50,000 square feet per tenant space), local and unique business, boutique retail, public assembly and entertainment.



FIGURE 6: Dunwoody Village Character Area Map



▲ Food Market in Dunwoody Village



▲ Local Shops



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#### **Action Items**

- Create an active community center with public places to gather, following a master planning process that potentially supports a redevelopment investment program.
- Establish way-finding or landmark features that unify the Village and can be used across the City, if a way-finding or gateway plan does not already exist for the greater City as a whole.
- Regularly review the Dunwoody Village Overlay to ensure enforcement meets intent of Overlay and Character Area vision.
- Creatively address the parking and congestion that new local activity may generate.
- Identify solutions for structural parking.
- Establish bicycle network for new connectivity throughout the City so that "all roads lead to the Village."
- Establish infrastructure thresholds that new development must meet.
- Complete site location evaluation and financing plan locating City Hall, Police and other civic functions, including options for joint publicprivate partnership. Note that the Georgetown area is also being considered as a possible location of City Hall.
- Create venues for cultural events like music and create programs for public uses of City Hall and library.
- For detailed circulation and open space recommendations concerning the Dunwoody Village character area, see the Dunwoody Village Master Plan.
- Pursue the creation of a central green space and altenatives for acquiring that space.

#### REDEVELOPMENT WITH OPEN SPACE

Several regional examples of redevelopment with open space were referenced during Community Meeting discussions about Dunwoody Village. The City of Smyrna allows higher buildings adjacent to the plaza at the Market Village. The City of Dunwoody envisions similar development and rules whereby densities may increase, contingent upon the provision of amenities (open space, plazas, etc.) or development features.



FIGURE 7: Dunwoody Village Master Plan

# 2.5 FUTURE LAND USE

The Future Land Use Map like the Character Areas map is a **visual representation of the City's future development policy.** Interpretation of the map is provided in the supporting text to be considered along with the City's zoning, the Character Areas Map, and other local policies when decision-makers consider land development questions or requests.

FIGURE 16: Future Land Uses Table

	LAND USE	DESCRIPTION	ZONING CATEGORIES
	Single Dwelling Residential	The predominant use of land is for single-family dwelling units, including detached, semi-attached or duplexes.	R- districts (R-150, R-100, R-85, R-75, R-60, R-50, RA, RA-5, RA-8, R-CD, R-CH)
	Multi-dwelling Residential - Apartments	The predominant use of land is for multifamily dwelling units, typically 12 units per acre or more.	RM- districts (RM-150, RM-100, RM-85, RM-75, RM-HD)
	Multi-dwelling Residential - Other	The predominant use of land is for multi-dwelling units, 3 or more units attached, including townhouses and condominums.	RM- districts (RM-150, RM-100, RM-85, RM-75, and RM-HD)
	Commercial	Land dedicated to non-industrial business uses, including retail sales, office, service, and entertainment facilities. Accessory commercial uses may be located as a single use in one building or grouped together in a shopping center or office building.	O-I, O-I-T, C-1, C-2, NS, O-D
HIGH SCHOOL	Public/ Institutional	Government uses at all levels, and institutional land uses. Government uses include City Hall, police and fire stations, libraries, post offices, schools, etc. Examples of institutional land uses include colleges, churches, cemeteries, hospitals, etc. Does not include facilities that are publicly owned, but would be classified more accurately in another land use category. For example, publicly owned parks and/or recreational facilities are classified under park/recreation/conservation category; and general office buildings containing government offices (such as the current Dunwoody City Hall) are included in the commercial category.	Any zoning district.

LAND USE	DESCRIPTION	ZONING CATEGORIES
Transportation/ Communication/ Utilities	Major transportation routes, public transit stations, power generation plants, railroad facilities, radio towers, telephone switching stations, airports, port facilities or other similar uses.	Any zoning district, subject to applicable zoning restrictions.
Parks/ Recreation/ Conservation - Public	Land dedicated to active or passive recreational uses held in public ownership or land trust. These areas may include playgrounds, public parks, nature preserves, community centers or similar uses.	Any zoning district.
Parks/ Recreation/ Conservation - Private	Land dedicated to active or passive recreational uses in private ownership. These areas may include subdivision recreation areas, golf courses, swim and tennis centers, or similar uses.	Any zoning district.
Mixed Use	A mixture of uses on the same parcel, vertical (same building) or horizontal (different buildings). Typically this is made up of a combination of commercial and residential uses, but could include a mix of other uses as well.	OCR, PD, CR-1
Perimeter Center	Livable regional center with office, retail, mixed-use, and multi-use residential buildings.	Draft PC-1, PC-2, PC-3 and PC-4 Districts



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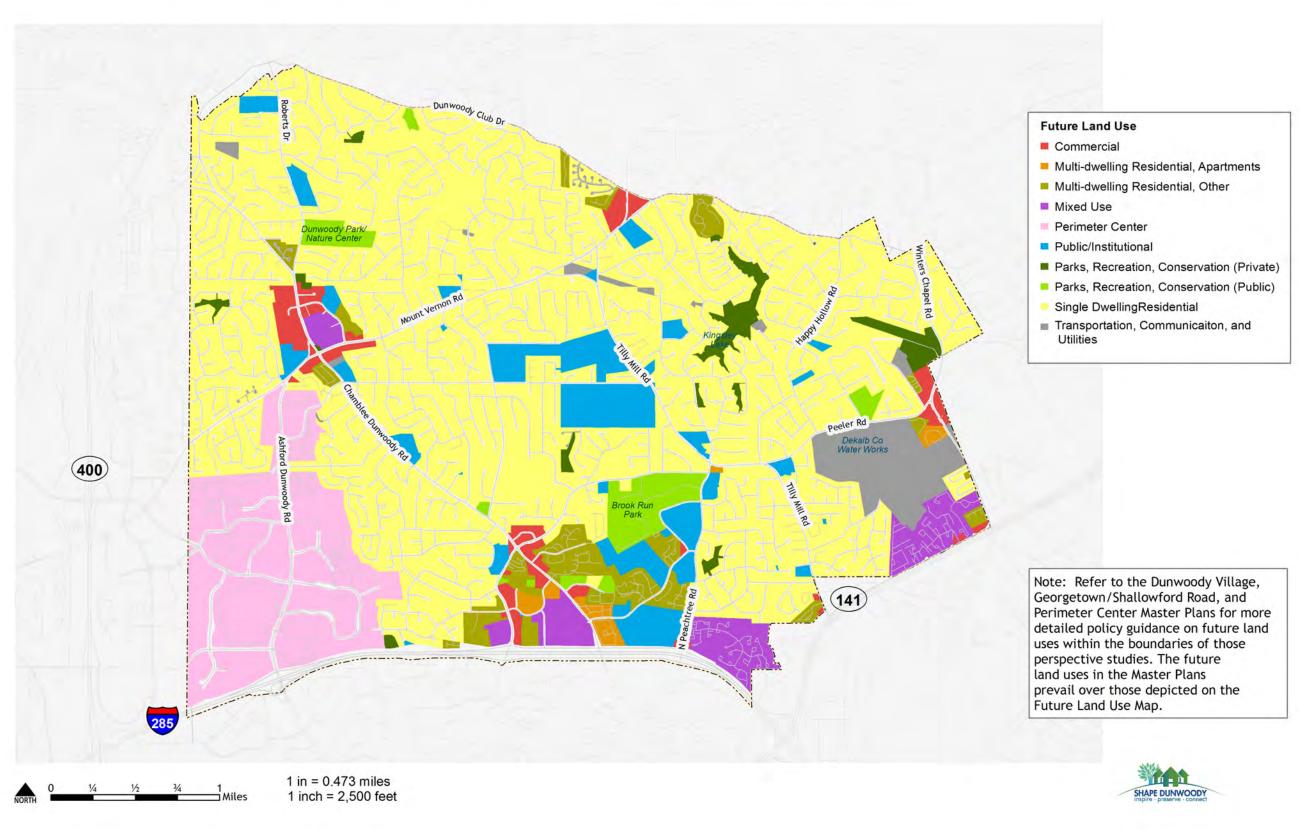


FIGURE 17: Future Land Use Map



## LAND USE FRAMEWORK PLAN

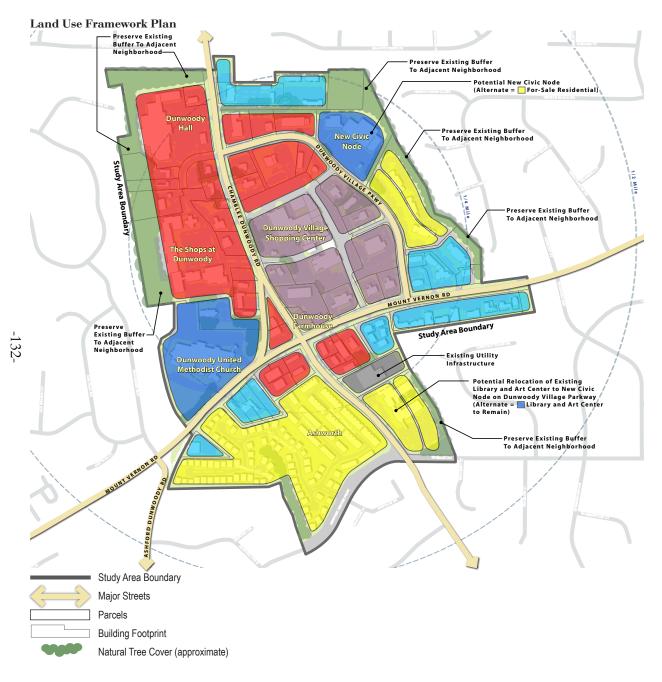
The Land Use Framework Plan is intended to guide future redevelopment and community improvements within the Master Plan area over the next 10-20 years. The Dunwoody Village encompasses over 150 acres of property. Based upon current real estate market conditions and the significant amount of existing retail and office space within the Dunwoody Village, it is unlikely that large portions of the Village could experience redevelopment in the short-term (next 3-5 years).

Additionally, a number of properties have multiple owners (office condominiums), are stable institutions or are higher performing retail properties and are unlikely to change in the short- to midterm. These properties include the Ashworth neighborhood, Dunwoody United Methodist Church, The Shops at Dunwoody, Dunwoody Hall, and a series of office condos on the north side of the study area and along Mount Vernon Road. While some of these properties may need some upkeep and improvement, other areas within the Village exhibit lower levels of activity, rents, and occupancies. These areas are outlined in previous sections of this report related to Activity and Redevelopment Potential.

The land use framework plan seeks to create a 20-year vision for community improvement and redevelopment in areas that are most likely or most susceptible to change. Consistent with the community consensus points the Land Use Framework Plan seeks to:

- Create a more vibrant Village Center including a focal point community green space
- Encourage more compact development forms that promote walkability, but are appropriately sized and scaled to maintain and enhance the area's unique character and identity
- Maintain and enhance convenience retail centers that provide daily goods and services for nearby residents
- "Prune" lower performing retail and office sites over time to improve the area's market fundamentals and visual appeal of the Village
- Allow for modest residential development to enhance walkability and enliven the Village, but in doing so, encourage high-quality, owner-occupied units targeted at empty nesters and active adults

The following sections outline key strategies and recommendations for various districts within the Dunwoody Village Land Use Framework Plan.



#### DUNWOODY VILLAGE DISTRICTS

#### NEIGHBORHOOD & CONVENIENCE RETAIL:

- · Primarily existing retail/commercial
- · Limited professional office
- · Limited opportunity for new development
- Facade / tenant upgrades over time
- Internal landscape / pedestrian / site improvements over time (private)
- · Generally 1-2 story development



#### SMALL-SCALE OFFICE:

- · Existing office space
- · Limited opportunity for new development
- Facade / tenant upgrades over time
- Internal landscape / pedestrian / site improvements over time (private)
- Primarily 1-2 story development (third level of occupied space in some existing buildings)



#### CIVIC / INSTITUTIONAL:

- Existing civic / institutional uses (Dunwoody United Methodist Church)
- Opportunity for new Civic Node
- New "town green / plaza" (0.5-1.0 acres)
- Potential new Municipal Complex
- Potential relocation of existing Library & Arts Center (from Chamblee Dunwoody Road)
- Potential new civic uses
- 1-3 story development
- Potential supporting retail / office (small-scale 1-2 stories)
- NOTE: See separate detailed site concept



#### FOR-SALE RESIDENTIAL:

- Existing townhomes and small-lot single-family (Ashworth)
- · Opportunity for new development (Chamblee Dunwoody Road site & Dunwoody Village Parkway site)
- 10 to12 units per acre
- 2 to 3 stories with private garages
- Age-targeted (empty nesters)
- Large units (greater than 2,000 sq. ft.)
- For-sale product
- Preserve existing buffers
- High-quality design / materials



#### VILLAGE CENTER MIXED-USE:

- · Existing Village commercial
- Preservation of Cheek-Spruill Farmhouse
- Preservation of Fresh Market and Walgreens
- Significant opportunity for redevelopment (multiple sites)
- New "town green" (1.5-2.0 acres)
- New internal street grid
- "Walkable" storefront character
- Potential new storefront retail / office
- Potential for-sale loft housing over ground-floor commercial (40-80 units)
- Age-targeted housing (empty nesters)
- Large units (greater than 2,000 sq. ft.)
- Generally for-sale product
- Primarily 2-3 story development
- High-quality design / materials (compatible with Village aesthetic)
- NOTE: See separate detailed site concepts



# NEIGHBORHOOD/CONVENIENCE RETAIL

Neighborhood retail centers that meet the daily needs of nearby residents and workers are one of the mainstays of the Dunwoody Village. Under this plan there should be incentives for façade, landscape, and streetscape enhancements to improve the appearance and accessibility of commercial properties primarily along the west side of Chamblee Dunwoody Road and the north end of Dunwoody Village Parkway. However, these shopping areas including Dunwoody Hall, the Shops at Dunwoody, and smaller parcels at the intersection of Chamblee Dunwoody Road and Mount Vernon Road will likely remain retail uses with some limited, storefront offices. Neighborhood/convenience retail areas should maintain consistency with the Dunwoody Village's traditional architectural character.

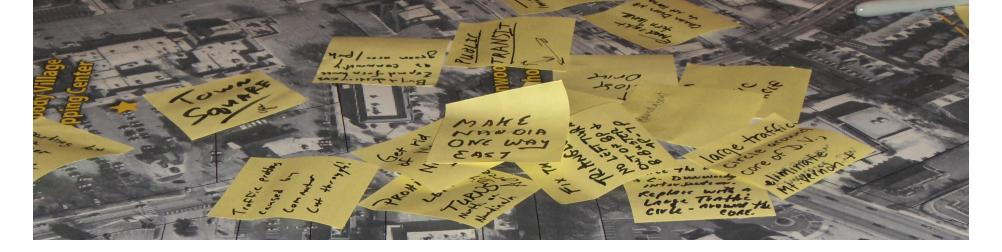
To enhance neighborhood retail areas the City of Dunwoody should consider:

- · A façade and landscape improvement program
- Updating Chamblee Dunwoody Road, Mount Vernon Road, and Dunwoody Village Parkway streetscape requirements to be consistent with those planned in the Circulation and Open Space Framework Plan
- Reviewing the City's site design standards to encourage any redevelopment to address the street by placing buildings at the back of sidewalk and placing parking behind or to the side, screened from public right-of-way
- Enhancing the pedestrian environment by requiring pedestrian sidewalks/paths from major roadways to developments and enhancing parking lot landscaping where appropriate
- Encouraging (or requiring) inter-parcel connectivity and limit curb cuts to major roadways (Mount Vernon Road, Chamblee Dunwoody Road, and Dunwoody Village Parkway) as part of significant development or redevelopment efforts
- Reviewing the City's sign ordinance to ensure that it promotes the installation of low, traditionally styled, highquality signage and does not serve as a disincentive that results in older, deteriorating signage remaining in place

# SMALL-SCALE OFFICE

One of the unique features of the Dunwoody Village is a collection of smaller-scale, Williamsburg-esqe office condos. These small scale office facilities are located along the southern edge of Mount Vernon Road between Ashford Dunwoody Road and Wickford Way, at the intersection of Mount Vernon Road and Dunwoody Village Parkway, and along Center Drive at the northernmost end of the Village. As office condos with multiple owners that reinforce the traditional character of the Village, these facilities are unlikely to change over the short- and mid-term. Similar to neighborhood/convenience retail areas, façade and landscape improvements should be undertaken as opportunities arise in keeping with the Village's traditional architectural character.

Maintaining small scale office opportunities is important to maintaining the unique, local character of the Dunwoody Village. However, pruning lower performing office buildings over time will help the Village Center be a more viable location for local businesses. As opportunities arise, low visibility, lower performing, stand-alone office (marked by lower rental rates and higher vacancies) should be removed from inventory over modest reinvestments that will only prolong consolidation and redevelopment.



# CIRCULATION & OPEN SPACE FRAMEWORK PLAN

The Circulation and Open Space Framework Plan outlines key community green/open space and pedestrian, bicycle, and roadway improvements developed in conjunction with the land use and market goals established during the planning process. Outlining transportation improvements in conjunction with land use goals and objectives is vital to maintaining and enhancing the character of the Dunwoody Village. The Circulation and Open Space Framework Plan places emphasis on community goals to create additional open space and gathering areas, enhance walkability, and expand potential for alternative transportation modes to be utilized in and around Dunwoody Village.

The following sections outline key green space initiatives and recommended roadway, pedestrian, bicycle, and multi-use trail enhancements.



#### Circulation and Open Space Framework Plan



#### DUNWOODY VILLAGE CIRCULATION & OPEN SPACE INITIATIVES



• "Town Green" - 1.5 to 2.0 acres

(NOTE: location and layout TBD)

• "Civic Green" - 0.5 to 1.0 acres

Farmhouse Green

 Chamblee Dunwoody Road & Womack Pocket Park (if property is renovated/redeveloped)

• The Shops at Dunwoody Plaza (private)



#### PEDESTRIAN PATHS / TRAILS:

• 12-15 feet wide

- Multi-use for pedestrians and recreational bikers (not bicycle commuters)
- Some in public right of way, some on private property (with easements)
- Potential trailheads connecting to adjacent neighborhoods (secured access to be further explored) (NOTE: Will require detailed discussions with affected neighborhoods and existing property owners to determine feasibility and exact locations)



#### STREETSCAPE IMPROVEMENTS:

- Sidewalks, decorative lighting, street trees, mast arms, aceess management, landscaping, and on-street bike routes (dedicated lane on Dunwoody Village Parkway)
- · Chamblee Dunwoody Road
- · Mount Vernon Road
- Dunwoody Village Parkway
- Ashford Center Parkway



#### INTERSECTION IMPROVEMENTS:

- Decorative Pedestrian Crossings
- · Pedestrian Signalization if warranted
- Operational Improvements (Chamblee Dunwoody Road signalization changes associated with change to Nandina Lane)



#### NEW VILLAGE CENTER STREET GRID:

- New roads to break up super block (associated with new development) (NOTE: location and layout TBD)
- Roads may be public or private (TBD)
- · Pedestrian oriented streets
- Operational Improvements



#### ACCESS MANAGEMENT IMPROVEMENTS:

- Operational & access improvements to Chamblee
   Dunwoods Board north of Mount Verson Board
- Dunwoody Road north of Mount Vernon Road

   Potential signal timing reduced / consolidated cu
- Potential signal timing, reduced / consolidated curb-cuts, interparcel connectivity enhancements, etc. (requires additional detailed study)



#### NANDINA LANE RECONFIGURATION:

- Conversion of Nandina to one-way (southbound)
- Requires adjustments to operation of Chamblee Dunwoody Road & Mount Vernon Road intersection (requires additional detailed study)
- Consider long-term removal

39



# OPEN SPACE

As detailed in the Village Center description in the last section, the centerpiece of the proposed, walkable Village Center is a Village Green Space. The primary goal is to create an appropriately scaled community gathering space as a focal point of the Village Center. The Planning Team's recommended 1.5 to 2 acre open space would accommodate community gatherings of up to 10,000 or more if adjacent streets are designed in a manner that they can be closed for larger events.

The Village Green should be visible from and/or linked to each of the Village's major streets (Mount Vernon Road, Chamblee Dunwoody Road, and Dunwoody Village Parkway) through sidewalks, multi-use trails, and significant landscaping. The Green should also be linked to the Farmhouse via a pedestrian path and enhanced landscaping.

The most significant design challenge for the space is to create an open space that is large enough to accommodate community gatherings without feeling oversized and empty when not in use. To best meet this challenge, the green space must be designed in conjunction with surrounding redevelopment and utilize a variety of materials to create sub-areas. The space should include a combination of hardscape plazas and open green spaces to provide variety and flexibility to comfortably accommodate a variety of groups (from small gatherings to large events). The space is intended to be passive in nature and not a location for active recreation (playgrounds, athletics, etc.).

Other open space opportunities within the Dunwoody Village include maintaining and enhancing green spaces and mature trees that surround the Farmhouse, a more formal "civic green space" of ½ to 1 acre that should accompany the development or relocation of any civic facility into the Dunwoody Village, and encouraging enhanced community green space as a part of major, private redevelopment/improvement projects.

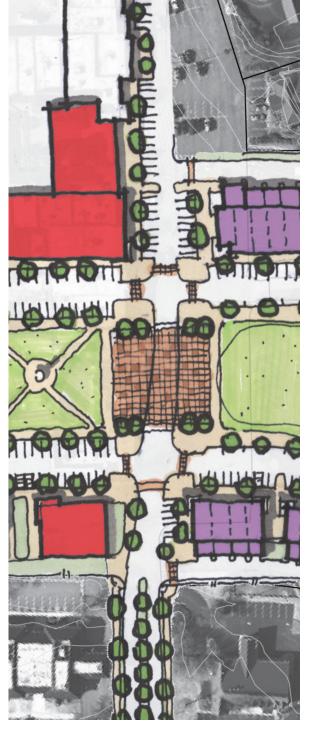
Any public or private redevelopment or major improvement property improvement project should be accompanied with landscape and green space improvements. The Planning Team recommends that the district's zoning classification be reviewed to encourage the creation and enhancement of smaller green spaces, provide street trees along major roadways and entries, and provide landscape islands and trees within surface parking areas. Generally these smaller open spaces should be used to create focal points within redevelopment projects and be bordered on at least one side by a public roadway where reasonable to enhance visibility and safety. If the City were to reposition the current Library/Arts Center site at Chamblee Dunwoody Road and Womack/Ashford Center Parkway, a landscaped area or pocket park should be considered on the northeast corner of the intersection as a gateway into the Dunwoody Village.

#### NEW ROADWAYS

Integral to enhancing the character of and enhancing walkability in the Dunwoody Village is breaking up the superblock bounded by Mount Vernon Road, Chamblee Dunwoody Road, and Dunwoody Village Parkway. The Dunwoody Village Master Plan calls for two new east-west connections between Chamblee Dunwoody Road and Dunwoody Village Parkway and one north-south connection through the block. These internal roadways should be local, low-speed streets with shared bicycle lanes on-street, on-street parking, landscape buffers (4-6 feet recommended), street trees, lighting and wide pedestrian sidewalks (8-12 feet in width recommended).

The internal street grid is proposed to enhance access, visibility, and walkability providing multiple options for cars, bicycles, and pedestrians alike. These roadways would also alleviate some pressure on the Mount Vernon Road/Chamblee Dunwoody Road intersection and Chamblee Dunwoody Road between Mount Vernon Road and Dunwoody Village Parkway as they would provide options for low circulation. Internal streets will reduce the opportunity to have the "back side" of buildings exposed to public right-of-way and reduce expanses of surface parking lots.





## TRAFFIC ENHANCEMENTS

Congestion at the Mount Vernon Road and Chamblee Dunwoody Road intersection, in combination with Nandina Lane, inhibits travel to and through the Village on a daily basis. Particularly during afternoon peak hours, delay at the intersection can back traffic for expansive distances in all directions. Roadway expansions to improve capacity are not supported by the community and would not reinforce the character of the Dunwoody Village. Adjustments are necessary however, to better manage the intersection and enhance safety.

While some inventive concepts have been proposed for the intersection (two-lane roundabouts, bypasses, etc.), the Planning Team recommends a series of more modest improvements including modifications to Nandina Lane and enhanced traffic signal timing of this and all nearby traffic signals.

Left-turn movements onto and out of Nandina Lane create operational and safety conflicts due to their close proximity to the major intersection of Chamblee Dunwoody Road and Mount Vernon Road. This project would involve operationally changing



the intersections at both ends of Nandina Lane to become right-in-right-out only. This may result in needing turn lane reconfigurations at the primary intersection (to be determined by a future traffic study); however, the net result will yield better efficiency and safety within the proximity of this area. This project will also include replacing the existing span wire signals with decorative mast arms and utilizing countdown pedestrian signals and well-marked crosswalks. Over the long-term, Nandina Lane should be further studied as a one-way southbound street or eliminated.

One of the largest challenges for the Dunwoody Village is the high volume of traffic passing through the area along the major corridors of Mount Vernon Road and Chamblee Dunwoody Road. Much of this traffic is regional in nature resulting from traffic patterns that extend well beyond the district. It is unlikely that this traffic can be rerouted, so improving operational efficiency along the major corridors is paramount. Operational improvements can be made by evaluating signal timings and linking signals together to coordinate timings and achieve positive progression. This effort would involve a more detailed traffic study that evaluates the network of signals in and around the Dunwoody Village.

As part of any improvement project, access management along Chamblee Dunwoody Road and all major roadways in the Dunwoody Village area should be considered. Part of the traffic congestion problem in the area is the number of curb cuts and potential conflict points along major roadways. Any future redevelopment and/or streetscape project should consider reducing the number of curb cuts and directing as many turning movements as possible into a limited number of intersections and access points.





To enhance walkability within Dunwoody Village pedestrian facilities (sidewalks, ramps, crossings) and the pedestrian environment (storefronts, lighting, benches, trash receptacles, etc.) must both be improved. Four significant streetscape (sidewalk, landscape, and lighting) projects are recommended for the Dunwoody Village.



Enhancements to Dunwoody Village Parkway are under design as part of a Transportation Enhancement Grant awarded to the City. Elements of the plan include:

- Reduction in the number of travel lanes from 2 in each direction to 1 northbound and 1 southbound lane
- On-street bicycle lanes
- Landscape buffers 6-feet in width between travel lanes and sidewalks
- Street trees, pedestrian lighting, and roadway lighting within the landscape buffer
- 6-foot wide sidewalks in the short-term with potential to widen to 12-feet as part of future improvements and/or redevelopment
- Flexibility to add on-street parking on the west side of Dunwoody Village Parkway as part of future park development and/or private redevelopment

With future enhancements, efforts should be made to reduce the number of curb cuts along the roadway to limit potential conflict points between motorized vehicles, bicycles, and pedestrians.

Streetscape improvements should be undertaken for Chamblee Dunwoody Road in two phases. The first phase would improve the roadway from Mount Vernon Road to Roberts Drive (approximately 2,700 linear feet). Elements should include:

- Landscape buffers no less than 4 feet in width and preferably 6 feet in width between the roadway and pedestrian sidewalks
- Street trees, pedestrian lighting, and roadway lighting within the landscape buffer
- Wider sidewalks no less than 8 feet in width, preferably 10-12 feet wide
- Off-street bicycle facility on one side of the street (5-foot dedicated path or as part of a 12-foot multi-use path)
- Intersection enhancements including new mast arm traffic signals, pedestrian signals, high visibility crosswalks, and ADA Ramp improvements where necessary
- Removal of overhead utility lines and replace with underground utility lines
- Access management considerations to reduce and limit curb cuts and potential conflict areas along the corridor



The second phase of streetscape improvements along Chamblee Dunwoody Road (approximately 1,300 linear feet) should include the area from Mount Vernon Road south to Ashford Center Parkway.

- · Landscape buffers no less than 4 feet in width between the roadway and pedestrian sidewalks
- · Street trees, pedestrian lighting, and roadway lighting within the landscape buffer
- · Wider sidewalks no less than 8 feet in width,
- · Off-street bicycle facility on one side of the street as part of a 12-foot multi-use path
- · Intersection enhancements including new mast arm traffic signals, pedestrian signals, high visibility crosswalks, and ADA Ramp improvements where necessary
- · Removal of overhead utility lines and replace with underground utility lines

Multi-modal improvements along Mount Vernon Road from Ashford Dunwoody Road to Wickford Way (approximately 3,500 linear feet) should include:

- · Landscape buffers no less than 4 feet in width and preferably 6 feet in width between the roadway and pedestrian sidewalks
- · Street trees, pedestrian lighting, and roadway lighting within the landscape buffer
- Wider sidewalks no less than 8 feet in width, preferably 10-12 feet wide adjacent to the proposed Village Center
- · Off-street bicycle facility on one side of the street (5-foot dedicated path or as part of a 12-foot multi-use path on the north side of Mount Vernon Road)
- Intersection enhancements including new mast arm traffic signals, pedestrian signals, high visibility crosswalks, and ADA Ramp improvements where necessary
- · Removal of overhead utility lines and replace with underground utility lines
- · Access management considerations to reduce and limit curb cuts and potential conflict areas along the corridor

Finally, pedestrian and bicycle enhancements on Ashford Center Parkway and/or Womack Road should consider:

- · On-street bicycle lanes or shared lanes
- Landscape buffers no less than 4 feet in width between the roadway and pedestrian sidewalks
- Street trees, pedestrian lighting, and roadway lighting within the landscape buffer
- · Wider sidewalks along both sides of major roadways no less than 6 feet in width
- · Intersection enhancements including new mast arm traffic signals, pedestrian signals, high visibility crosswalks, and ADA Ramp improvements where necessary
- · Extension of the Ashford Center Parkway median when and where feasible

# **BICYCLE ROUTES**

A combination of on-street and off-street bicycle routes will be necessary to enhance the ability of bicycles to traverse the area more easily. On-street bicycle routes should be included on Dunwoody Village Parkway and as part of new internal street grid through the Village Center. On-street bicycle lanes should be 5 feet in width to meet AASHTO standards.

Off-street bicycle lanes should be considered as part of multi-use trails on one side of Mount Vernon Road and Chamblee Dunwoody Road due to high traffic volumes, the number of conflicts, and other safety concerns. Multi-use paths are typically 12-15 feet in width on one side of a street and accommodate pedestrians (runners/joggers), recreational bicyclists, and sometimes electric carts and/or other devices. Alternatively, dedicated off-street bicycle lanes between landscape buffers and pedestrian sidewalks should be 8 feet or greater in width.

These bicycle routes should be tied into a Citywide network of bicycle facilities in coordination with the City's Comprehensive Transportation Plan.



# MULTI-USE PATHS/TRAILS

To further enhance connectivity to and from the Dunwoody Village, multi-use paths and trails should be considered, if desired by adjacent neighborhoods, to connect surrounding single-family neighborhoods to the Village. These paths would be 12-15 feet wide to accommodate pedestrians, bicycles, and potentially electric-carts (golf carts) or other devices. Trails would require cooperation within the neighborhoods to determine a proper location and property easements for the City to assist with construction of the path. The City of Dunwoody should consider a policy regarding these paths if individual neighborhoods desire their implementation.

Based upon public involvement in the Dunwoody Village Master Plan, potential priority locations for off-street multi-use trails include:

- A dedicated east-west path through the Village Center in conjunction with the realization of one of the recommended east-west roadways
- Potential links to adjacent neighborhoods (if the neighborhoods desire them and assist in defining the appropriate path):
  - The Branches,
  - Wynterhall,
  - Wyntercreek, and
  - Vernon North.



# SPECIAL LAND USE PERMIT APPLICATION

My Commission Expires:



41 Perimeter Center East | Dunwoody, GA 30346 Phone: (678) 382-6800 | Fax: (770) 396-4828

* Applicant Info	rmation:	* *	8 350 8
Company Name:	SunTrust Bank c/o Davi	d Blumenthal	
Contact Name:	Vickie B. Chung, Smith, G	ambrell & Russell, LLP	
Address: 1230	Peachtree St., NE, Ste. 31	00, Atlanta, GA 30309	
Phone: 404-815-36	20 Fax: 404-685-6	6920 Email: dw	ebb@sgrlaw.com
	ference date (required):		
	ation: 🗆 Check here if same	as applicant	
Owner's Name: Di	rk Laukien		
Owner's Address: 2	4 Waterway Ave., Ste. 225,	The Woodlands, TX 7738	0
Phone:	Fax:	Email:	
* Property Infor	mation:		
Property Address:	5490 Chamblee Dunwoody R	d.	Parcel ID: 18-366-01-013
Zoning Classificatio	n: <u>C-1</u>		
Requested Use of the	ne Property: Bank with Drive	-thru	
* Applicant Affic	lavit:		
are determined to be ne Zoning Ordinance. I cer and associated actions.	cessary, I understand that I am r tify that I, the applicant (if different	esponsible for filing additional ent), am authorized to act on t	s correct and complete. If additional materials materials as specified by the City of Dunwoody he owner's behalf, pursuant to this application
	SunTrust Bank c/o Davi		07101110
Applicant's Signatur	re:		Date: 07/01/16
Notary:			B WILLIAM
Sworn to and subsc	ribed before me this	Day of	TOS JUSTION EL TO
Notary Public:	ISTEN B. WILLIAMS		S & SUMP &
Signature:	in B. William	0	AUBLO A
My Commission Exp	ires: Odeber 8,20,	17	OTOBER
W Owner Affidavi	t:		Will COUNTY
are determined to be new Zoning Ordinance. I ce associated actions.	cessary, I understand that I am rertify that the applicant(s) (if different	esponsible for filing additional i	s correct and complete. If additional materials materials as specified by the City of Dunwoody n my behalf, pursuant to this application and
Property Owner's Na	<sub>ame:</sub> Dirk Laukien		
	gnature:		Date:
₭ Notary:			
Sworn to and subscr	ribed before me this	Day of	, 20
Notary Public:			

# Additional Property Owner(s) Notarized Certification

Property Owner (If Applicable):



41 Perimeter Center East | Dunwoody, GA 30346 Phone: (678) 382-6800 | Fax: (770) 396-4828

I hereby certify that to the best of my knowledge, this special land use application form is correct and complete. If additional materials are determined to be necessary, I understand that I am responsible for filing additional materials as specified by the City of Dunwoody Zoning Ordinance. I certify that the applicant(s) (if different) are authorized to act on my behalf, pursuant to this application and associated actions.

	(III)		
Owner Name: Dick	lydukigh k		
Signature:	Mun		Date: 5/26/2016
Address: zy warand	7 HUE Soute ZZS The woodle	es TX 77380	
Phone: 832-8/3-7313	Fax:	Email:	N.
Sworn to and subscri	Fax: ibed before me this2	Leth Day of	May , 2016
Notary Public:	gather M. Hoje	4	
	)	CYNTE	HIA M KOJAK
			i. Exp. Oct. 29, 2018 No. 152845-8
♣ Property Owner	· (If Applicable):		
			Date:
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Notary Public:	bed before the this	Day of	, 20
Notary Public.			
Property Owner	(If Applicable):		
Owner Name:			
Signature:			Date:
Address:			
	Fax:		
Sworn to and subscri	ibed before me this	Day of	, 20
Notary Public:			
3			

# Campaign Disclosure Statement

Applicant / Owner, SunTrust Bank

Signature: 🌽



41 Perimeter Center East | Dunwoody, GA 30346 Phone: (678) 382-6800 | Fax: (770) 396-4828

Have you, within the two years immediately preceding the filing of
this application, made campaign contributions aggregating \$250.00
or more to a member of the City of Dunwoody City Council or a
member of the City of Dunwoody Planning Commission?

□ YES ¥NO

Date:

Address	David Blumenthal 43	17 Park Orine, Sui	it 400, Norcro	30093
	If the answer abo	ve is yes, please complete the	e following section:	
Date	Government Official	Official Position	Description	Amount
	***************************************			
				100 100 100 100 100 100 100 100 100 100
				71.
				1000

# Campaign Disclosure Statement

Applicant / Owner:

Signature:



41 Perimeter Center East | Dunwoody, GA 30346 Phone: (678) 382-6800 | Fax: (770) 396-4828

Date: 5/26/2016

Have you, within the two years immediately preceding the filing of this application, made campaign contributions aggregating \$250.00 or more to a member of the City of Dunwoody City Council or a member of the City of Dunwoody Planning Commission?

U YES NO

Date	Government Official	Official Position	Description	Amount
_				

# Campaign Disclosure Statement



41 Perimeter Center East | Dunwoody, GA 30346 Phone: (678) 382-6800 | Fax: (770) 396-4828

Have you, within the two years immediately preceding the filing of			
this application, made campaign contributions aggregating \$250.00	☐ YES	MO	
or more to a member of the City of Dunwoody City Council or a	U IES	a NO	
member of the City of Dunwoody Planning Commission?			
Applicant / Outrate Dennis J. Webb. Jr. Smith Gambrell & Ru	ssell 11	Р	

Applicant Owner.	24.536.5 93.53
Signature:	Date: 7-1-16
Address: 1230 Peachtree St., NE, Ste. 3100, Atlanta, GA 30309	

If the answer above is yes, please complete the following section:

Date	Government Official	Official Position	Description	Amount
-				
	,			

# Campaign Disclosure Statement



41 Perimeter Center East | Dunwoody, GA 30346 Phone: (678) 382-6800 | Fax: (770) 396-4828

Have you, within the two years immediately preceding the filing of this application, made campaign contributions aggregating \$250.00 or more to a member of the City of Dunwoody City Council or a member of the City of Dunwoody Planning Commission?	
* Applicant / Owner: Vickie B. Chung, Smith, Gambrell & Russell, LLP	
Signature:Date: 7 11 16  Address: 1230/Peachtree St., NE, Ste. 3100, Atlanta, GA 30309	

If the answer above is yes, please complete the following section:

Date	Government Official	Official Position	Description	Amount	

# Campaign Disclosure Statement

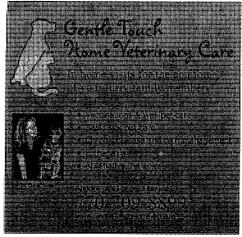


41 Perimeter Center East | Dunwoody, GA 30346 Phone: (678) 382-6800 | Fax: (770) 396-4828

Have you, within the two years immediately preceding the filing	OI
this application, made campaign contributions aggregating \$250	.00
or more to a member of the City of Dunwoody City Council or a	
member of the City of Dunwoody Planning Commission?	
* Applicant / Owner: Smith, Gambrell & Russell, LLP	

YES NO

Signatu	re:		Date:	b					
Address	: 1230 Peachtree St., NE, St	e. 3100, Atlanta, GA 30309	- 1						
If the answer above is yes, please complete the following section:									
Date	<b>Government Official</b>	Official Position	Description	Amount					
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# Habitat ReStore THE STORE THAT BUILDS HOMES AND Shop Dur Wide Selection of tems for Your Hame to our se it of Furniture Alpiantes e Atme Decer Great Suilding Supplies Great Cause! - hoghmo at The 344400 olehor marinerinte www.suburbanatlantarestores.org 1740 CeKalb Technology Plwy. Doraville, GA 30340 do to excessed was ald P. End M. Shanka Jaho Nasij

# UPCOMING Mark your calendars

Join the Lunch & Learn Programs of PALS on Mondays beginning June 13 – July 25, 2016 from 10 a.m. – 3 p.m. at Dunwoody Baptist Church.

Classes offered are: the Constitution, Beautiful Geological Marvels, Mahjongg, Estate Planning, A History of the World in Six Glasses, Travel, Bridge, Current Events, and Local Parks and Gardening. Brochures are available at Dunwoody Baptist or by calling PALS at 770-698-0801. Enjoy great catered lunches, available with reservations.

Registration is underway for the North DeKalb Blaze Track & Field Club's 2016 Outdoor Season.

Open to boys and girls ages 5 to 18, Blaze is a well-organized club that competes in AAU and USATF sanctioned events, including the National Junior Olympic Games. Events include sprints, middle distance running, race walking, jumpthrowing, relays and ina. multi-events. Athletes learn proper technique, competition strategies, fitness tips, exercise routines, good nutrition, teamwork and sportsmanship. Practice is at North DeKalb Stadium on Chamblee-Dunwoody Road and meets are across metro-Atlanta, with regional and national meets at the end of the summer season.

Email northdekalbblazetc@bell-south.net, call Coach Holmes at 678-472-3827 or visit league-lineup.com/ blazetrack for a registration form.

The Friends of the Dunwoody Library book sale begins June 2 and continues through June 6 at the Dunwoody Library, 5339 Chamblee-Dunwoody Road, 770-512-4640. On Thursday, June 2, there will be a 1 p.m. special preview shopping for members of "Friends". Then, the sale is open to the public from 4-8 p.m. On

Friday and Saturday, June 3-4, the sale will be open 10 a.m. to 5 p.m. each day and closed on Sunday, June 5. Monday, June 6 from 10 a.m. to 8 p.m., will be "Bag Day" – fill a large grocery bag for \$6.

The sale will feature thousands of books-most priced from \$.25 to \$2.00. The sale includes 25,000 items: current hardback fiction, paperbacks, children's books, videos, CDs, DVDs, audio books, vinyl records, magazines, puzzles, and collectible books. And, there is a large selection of history, literature, travel, religion, self-help, home improvement, cookbooks, and large print books. All proceeds benefit the Dunwoody Library and the DeKalb Library System.

Be a Friend of the Dunwoody Library. Get tons of books at bargain prices, and support your library. Memberships start as low as \$5 per year.

1548 Mt. Vernon Road, 770-394-0875.

Please join Choral Guild of Atlanta for its annual Summer Sing on Saturday, June 11 at 10 a.m. Singers of all voice parts are invited as we welcome Atlanta composer Curtis Bryant as guest clini-

are scheduled June 8 at 3 and 7

p.m. When the street urchin, Al-

addin, vies for the attention of the

beautiful princess, Jasmine, he

uses a genie's magic power to be-

come a prince in order to marry her.

lago, Jafar, the Genie and more are

here in this musical adventure filled

with magic, mayhem and the power

of love. Donations at the door will

support Performing Arts at Dun-

woody United Methodist Church,

cian for this free event.

Come at 9:30 to meet the director and chorus members and enjoy refreshments.

St. Luke's Presbyterian Church, 1978 Mount Vernon Road. For more information, email info@cgatl.org or call 404-223-6362.

The Marcus Jewish Community Center of Atlanta (MJCCA) is pleased to welcome the community to the Dunwoody Pool Day at the J, on Sunday, June 5, 12-2

This pool party is open to all and free of charge. Kids and families can enjoy the MJCCA's beautiful Sherwin Glass Aquatic Center and Barbara and Ed Mendel Splash Park. The MJCCA is located at 5342 Tilly Mill Road, Dunwoody. Food and drinks will be available for purchase. For information, contact Rabbi Brian Glusman at 678-812-4161 or rabbi.glusman@atlantaicc.org.

Dunwoody Pool Day at the J highlights include games and activities, free prizes, face painting, music and dancing

with DJ Sammy Rosenberg, free ice pops, splash park and lunch available for purchase.

Two peformances of Aladdin, KIDS, the musical, presented by the Dunwoody United Methodist Church,

Emory Saint Joseph's Hospital will host the first annual "Run for Mercy 5K/1K" a family-friendly Peachtree Qualifier event, at 8 a.m., June 12. The race benefits Mercy Care, a network of 15 clinics providing health care services to those living in homelessness and poverty. Proceeds will also be used for the purchase of medical equipment for the new

Mercy Care Chamblee clinic.
The "Run for Mercy 5K/1K" will kickoff in the Penimeter Place Shopping Center, 100 Penimeter Center

To register for the "Run for Mercy 5K/1K," visit: emoryhealthcare. org/runformercy5k/.

## PUBLIC NOTICE

SunTrust Bank intends to submit a Special Land Use Permit (SLUP) Application to the City of Dunwoody for development of a SunTrust Bank Branch at 5490 Chamblee Dunwoody Road. A SLUP application will be required to increase the maximum number of allowable parking spaces. The Applicant will be holding a neighborhood meeting to discuss the proposed SLUP application and to answer questions that you may have regarding the proposed development. Specific details regarding the Special Land Use Permit Application and Applicant-initiated meeting are below.

CASE NUMBER:

APPLICANT NAME:
JURISDICTION:
ZONING:
SLUP REQUEST:
STREET LOCATION:
PROPOSED DEVELOPMENT:
MEETING TIME:

MEETING LOCATION:

TBD (this will be provided at the time the application is filed with the City)

Travis Pruitt and Associates, Inc. on behalf of SunTrust Bank City of Dunwoody

C-1 (Local Commercial) (Dunwoody Village Overlay District) Increase the maximum number of parking spaces

5490 Chamblee Dunwoody Road New SunTrust Bank Branch

June 22, 2016 from 6:00 PM to 8:00 PM

5486 Chamblee Dunwoody Road, Suite 8, Dunwoody, GA 30338



DUNWOODY COMMUNITY MEETING HELD ON JUNE 22ND FROM 6:00 TO 8:00 PM REGARDING THE 5490 CHAMBLEE DUNWOODY ROAD PROPERTY UNDER CONTRACT TO BE ACQUIRED BY SUNTRUST BANK FOR THE PURPOSE OF BUILDING A SUNTRUST BANK BRANCH

NAME OF PERSON ATTENDING Mary Cooper ADDRESS OF PERSON ATTENDING 5145 Meadow creek Dr.

comments: Many people in the Community do not want a bank in this space. If SunTrust truly wants to be a part of the community - trecommend working with the community. Maybe they can to help a resturant get financing to go into the Spot. This is SunTrust's chance to be a superstan and listen to the community. The trule is going to be a lot of tacklash about a bank going there.

DUNWOODY COMMUNITY MEETING HELD ON JUNE 22<sup>ND</sup> FROM 6:00 TO 8:00 PM REGARDING THE 5490 CHAMBLEE DUNWOODY ROAD PROPERTY UNDER CONTRACT TO BE ACQUIRED BY SUNTRUST BANK FOR THE PURPOSE OF BUILDING A SUNTRUST BANK BRANCH

NAME OF PERSON ATTENDING Row Wood
ADDRESS OF PERSON ATTENDING 12 90 Mile Fost Dr
COMMENTS: TAM OFFORED to the Proposed VARIANCES.
Kan Lina
Ditto: Maggie Seninara Wasford

# SIGN IN SHEET

DUNWOODY COMMUNITY MEETING HELD ON JUNE 22ND FROM 6:00 TO 8:00 PM REGARDING THE 5490 CHAMBLEE DUNWOODY ROAD PROPERTY UNDER CONTRACT TO BE ACQUIRED BY SUNTRUST BANK FOR THE PURPOSE OF BUILDING A SUNTRUST BANK BRANCH

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			CONTRACTOR AND ADMINISTRATION OF THE PROPERTY	<u></u>	
***************************************	<u> </u>				

Promenade, Suite 3100 1230 Peachtree Street, N.E. Atlanta, Georgia 30309-3592

Main: 404 815-3500 Fax: 404 815-3509 www.sgrlaw.com



SMITH, GAMBRELL & RUSSELL, LLP

Attorneys at Law

Dennis J. Webb, Jr.

Direct Tel: (404) 815-3620 Direct Fax: (404) 685-6920

djwebb@sgrlaw.com

July 25, 2016

# Via Hand Delivery

John Olson, City Planner 41 Perimeter Center East, Ste. 250 Dunwoody, GA 30346

Re: SunTrust Dunwoody - SLUP-16-091 / Add Variance

Dear John:

Following up on our meeting of July 19, 2016, I submit the attached to be included in the above-referenced file(s):

- 1. Color Rendering of Survey;
- 2. Color Rendering of Landscape Plan;
- 3. Floor Plan and Elevations (Elevations in Black and White and Color);
- 4. Lighting Submittal;
- 5. Sketches Showing Vehicle Turning for (a) the Proposed Configuration of the Site and (b) for the Site With the Drive Thru Lanes Meeting the Setback Requirements;
- 6. A Revised Site Plan That Better Shows the Setback Lines for the Proposed Configuration;
  - 7. Revised Statement of Intent for the SLUP (Sidewalk SLUP removed).

As you know, the property at issue is .75 acres and zoned C-1. It contains a vacant, one-story, 4,200 square foot building that until recently housed the Old Hickory House Restaurant. The property contains 57 parking spaces, 26 of which are between the front of the building and Chamblee Dunwoody Road. The site has 2 curb cuts. It contains roughly 93% impervious surface. The front of the building (east) sits roughly 65' off of Chamblee Dunwoody. In the rear (west), the building is at its closest point 7.9' from the property line, so it is well into the rear yard setback. The western edge of the property is adjacent to a retaining wall of roughly 5-6 feet. West of that retaining wall is a 24' private driveway and then a parking field. The current property does not conform to the Dunwoody Village Overlay in any way.



John Olson, **City Planner** July 25, 2016 Page 2

SunTrust currently has a bank branch in the area on Mt. Vernon Road. The hope is to relocate that branch to the Old Hickory House site; it is closing current location and reopening it on Chamblee Dunwoody, so it is not adding a new bank to the area. The new location will contain a single-story, 3,500 square foot building designed to the Overlay's architectural standards. Parking will be reduced from 57 spaces to 22, and all spaces will be located behind the building. The site will have only 1 curb cut. The front of the building will be setback only 5' from the front property line and 10' from the front sidewalk. The imperious surface will be reduced from 93% to 53%, which is well below the maximum allowed in the C-1 zoning district. Further, the building will conform to all required setbacks while the drive-thru canopy will be located 12' from the rear property line, as opposed to 7.9' for the existing building.

To allow for construction on the new site, SunTrust has asked, among other things, to reduce the setback for the drive-thru lanes from 25' to 10', reduce the rear yard setback from 30' to 12' to allow for drive-thru canopies, and to increase the parking spaces from the 11 allowed under the Overlay to 22 spaces. The Staff mentioned that if SunTrust did not ask to increase the parking, it may not need the setback variances. SunTrust took a look at that issue.

As a preliminary matter, I note that SunTrust will have 9 employees at the new location, which is the same number it now has at the current location. So, under the Overlay, it would be limited to only 2 customer parking spaces, and 1 of those must be a handicap space. The current location has 23 parking spaces. Hence, the parking maximum under the Overlay is unfeasible and presents a hardship in this case.

Nonetheless, SunTrust diagramed a scenario in which it removed the westernmost row of parking from the site plan originally submitted. That left it with 12 spaces which, as indicated above, is not enough. More importantly, however, the change negatively impacted circulation on the site. As you will see, moving the drive-thru lanes and the drive-thru canopy out of the setbacks pinched the turning radius, so a car turning into the easternmost of the 2 drive-thru lanes cannot maintain its lane. Instead, it will cross into the second, western lane, creating a hazard. The change also left less area for drive-thru stacking. As another diagram shows, these problems do not exist with the drive-thru configuration proposed.

Thanks,

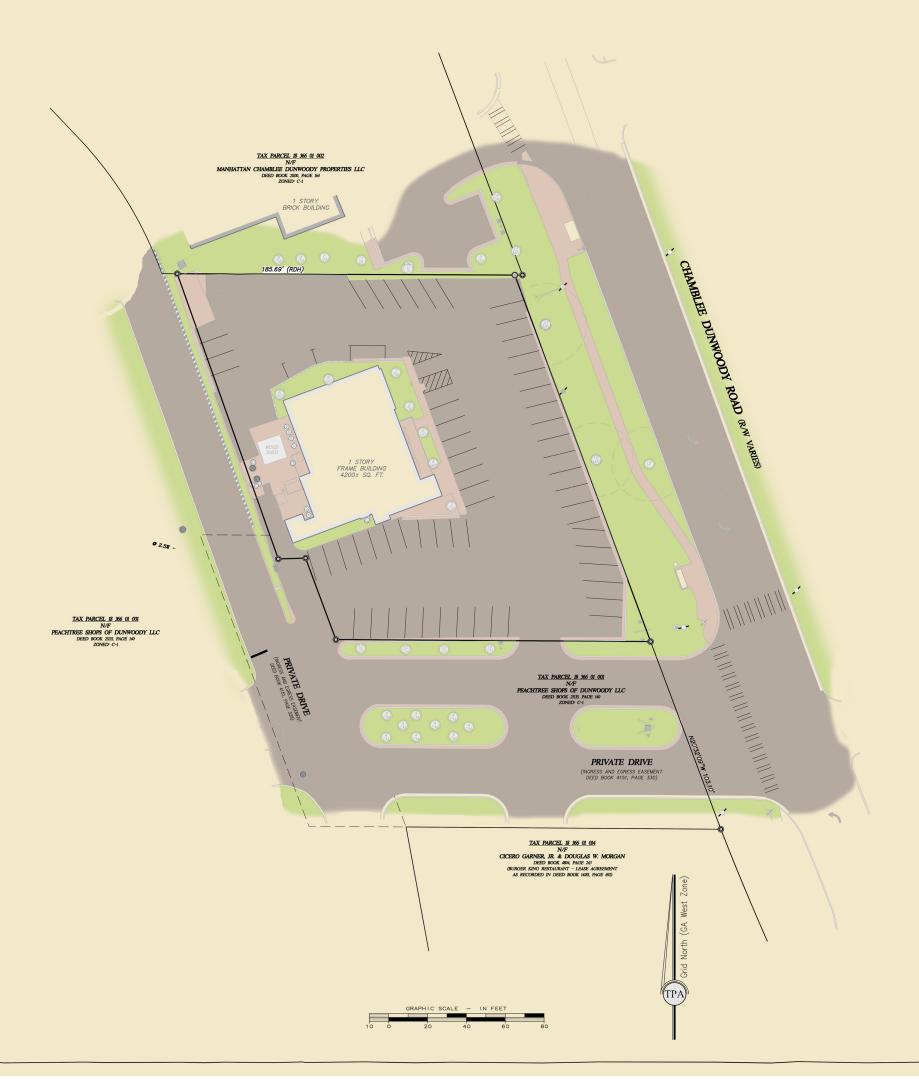
Dennis J. Webb, Jr.

Attorney

DJW/1177 Enclosures







**EXISTING SITE** 



ARCHITECTURE DEPLANNING DINTERIORS 5887 GLENRIDGE DRIVE SUITE 375 ATLANTA, GEORGIA 30328 TEL: 404.233.8110 FAX: 404.233.7229

woody Rd., Dunwoody, GA 30338

SunTrust New Banking Center

GENERAL NOTES

LEGEND

Date Description

06/21/2016 ISSUE FOR REVIEW

Project Name

Seal / Signature

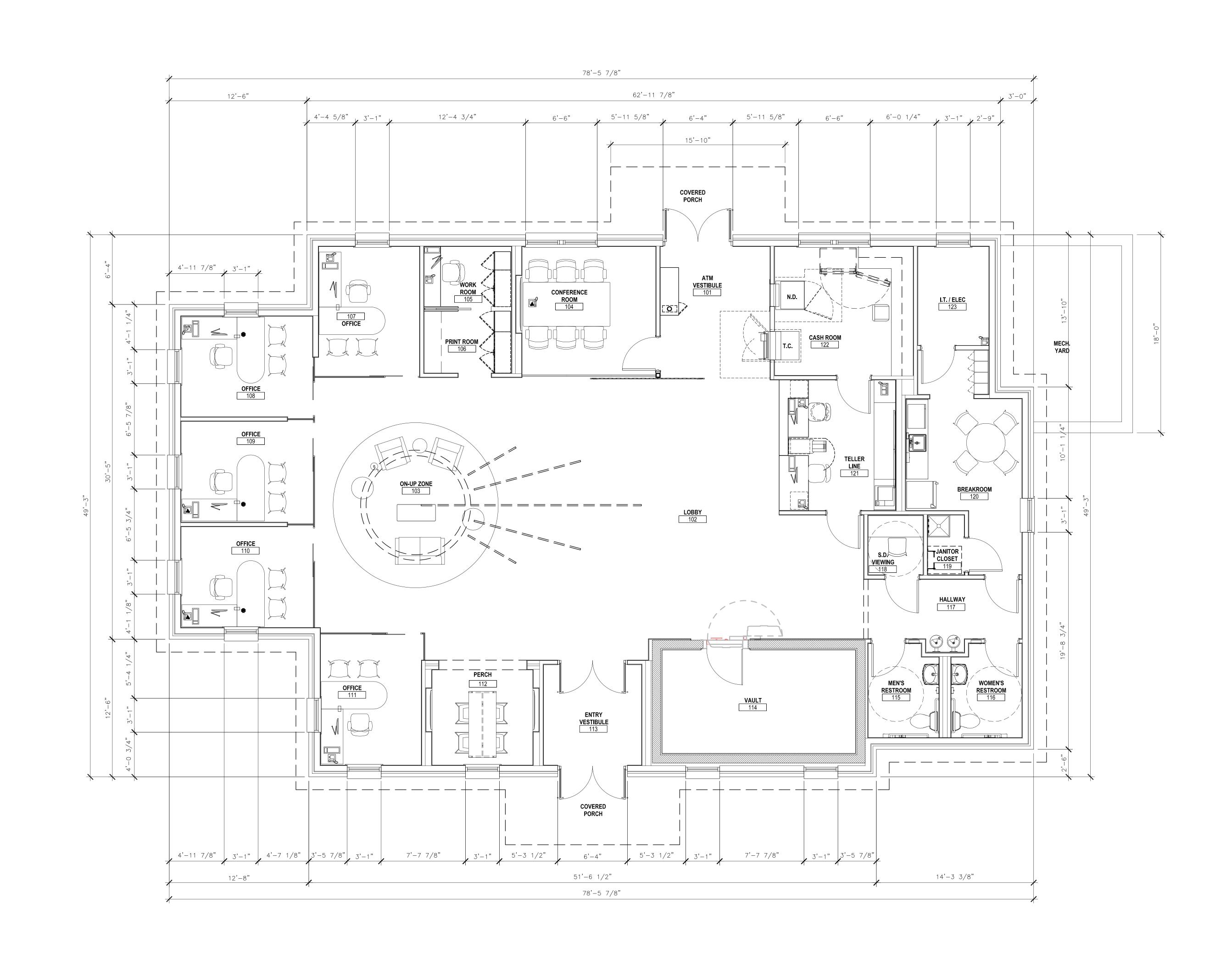
Project Number

SUNTRUST BANK - DUNWOODY

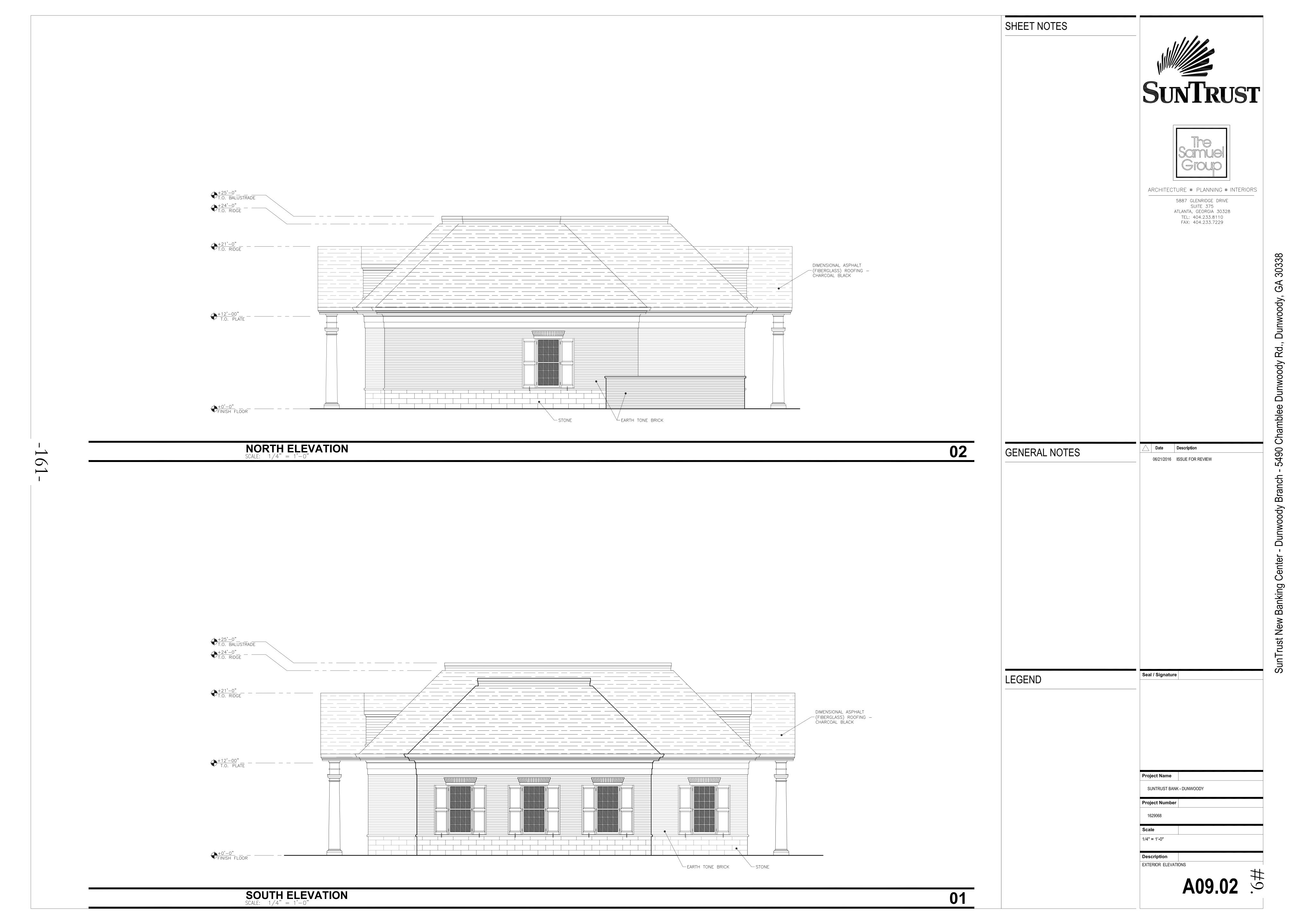
1629068

1/4" = 1'-0" Description

CONSTRUCTION FLOOR PLAN









5887 GLENRIDGE DRIVE SUITE 375 ATLANTA, GEORGIA 30328 TEL: 404.233.8110 FAX: 404.233.7229

GENERAL NOTES

02

Date Description 06/17/2016 ISSUE FOR REVIEW

LEGEND

Seal / Signature

**Project Name** 

SUNTRUST BANK - DUNWOODY

Project Number

1/4" = 1'-0"

EXTERIOR ELEVATIONS

A09.01



DIMENSIONAL ASPHALT (FIBERGLASS) ROOFING -CHARCOAL BLACK SUNTRUST +0'-0" FINISH FLOOR

CHAMBLEE DUNWOODY ROAD ENTRANCE (EAST) ELEVATION
SCALE: 1/4" = 1'-0"

01

NORTH ELEVATION SCALE: 1/4" = 1'-0" 02

+25'-0"\_ T.O. BALUSTRADE +24'-0"\_ T.O. RIDGE DIMENSIONAL ASPHALT
(FIBERGLASS) ROOFING CHARCOAL BLACK +12'-00" T.O. PLATE -+0'-0" FINISH FLOOR EARTH TONE BRICK STONE

SCALE: 1/4" = 1'-0"

01

July 25, 2016

BOLDEN-WILLIAMS

The Samuel Group 5887 Glenridge Drive, Suite 375 Atlanta, Georgia 30328

Attention: Bart Sargent

RE: SunTrust – Dunwoody Village, Dunwoody, GA

BWA Project No.: 16/8742

Bart.

We have reviewed the current photometric study (attached) for the above project and offer the following comments. The areas on site that exceed the Dunwoody Village Overlay requirements are in direct relation to the National and State standards for Banking and Finance (see attached) as well as IESNA-G-1 (Illuminating Engineers Society of North America.) Areas that exceed the foot-candle (fc) requirements are the drive-thru canopy which contains an ATM and Teller Connect unit. These require a minimum of 10 fc at five feet from the unit and 2 fc at fifty feet. This does not meet the ordinance but is required by Georgia Code and IESNA. The Teller Connect unit also has a requirement for 40-50 fc at the front of the machine for proper operation per the attached manufacturer's instructions.

Due to the security involved with bank buildings, the IESNA recommends a minimum of 3 fc in the parking areas for a facility like this.

With the requirements listed above, it will not be possible to meet the Dunwoody Village lighting requirements. Bank buildings require more lighting than a typical retail building in certain areas. We also recognize that the light levels at the front door are high. We will reduce lighting level along the front (street side) of the building to meet the ordinance.

Best regards,

Jeff Williams

Jeff Williams Bolden-Williams & Associates, Inc.

# **Bart Sargent**

To: Bart Sargent

**Subject:** NCR Teller Connect Ambient Lighting Requirments

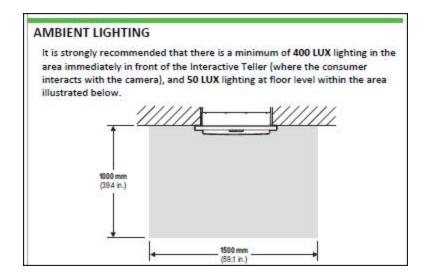
From: McKee, Dan [mailto:Dan.McKee@ncr.com]
Sent: Thursday, March 24, 2016 9:01 AM

To: Manookian. Vickie

Subject: RE: Action Item Follow Up

#### Hello Vickie -

Jim asked that I reach out to you to discuss the lighting requirements around the Teller Connect ITMs. Attached is the Site Prep Guide. If you go to page 5-1, on the bottom of the right-most column is the following graphic with verbiage. Calling for 400 LUX in front of the machine is intended to help the camera. That minimum level of light should be sufficient for the camera to get a good image of the consumer's face to aid in proving identification.



The 6632 & 6638 Site Prep Guides have the same graphic and information, and also on page 5-1 of their respective Site Prep Guides.

If you have any questions, please do not hesitate to contact me.

# Regards, Dan



Dan McKee Solution Manager NCR Corporation tel.: +1 (678) 808-5663

address: 3097 Satellite Blvd., Duluth GA 30096

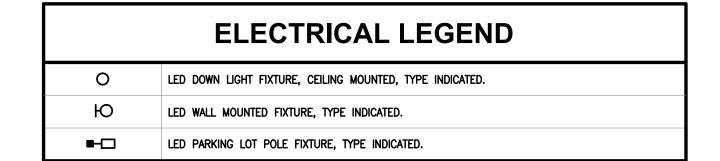
dan.mckee@ncr.com | www.ncr.com

	LIGHT FIXTURE SCHEDULE								
TYPE DESCRIPTION MANUFACTURER CATALOG NUMBER MOUNTING					LAMP TYPE	LUMENS	WATTAGE	NOTES	
OA1	L.E.D. PARKING AREA — SINGLE HEAD	LITHONIA	(1) CSX1-LED-60C-700-40K-T4M-120-SPA-FINISH	POLE	LED	16,479 lm	134 W	2, 3	
OA2	L.E.D. PARKING AREA — TWIN HEAD	LITHONIA	(2) CSX1-LED-60C-700-40K-T4M-120-SPA-FINISH	POLE	LED	32,958 lm	268 W	2, 3	
OB1	L.E.D. WALL PACK	LITHONIA	CSXW-LED-30C-700-30K-T3M-120-FINISH	WALL	LED	7,981 lm	69 W	1, 3	
OD1	6" L.E.D. DOWNLIGHT	LITHONIA	EVO-30/25-6AR-MD-LSS-120	RECESSED	LED	2,612 lm	29.5 W	_	
OD2	6" L.E.D. DOWNLIGHT	LITHONIA	EVO-40/40-6AR-MD-LSS-120	RECESSED	LED	4,046 lm	48.1 W	-	

# NOTES:

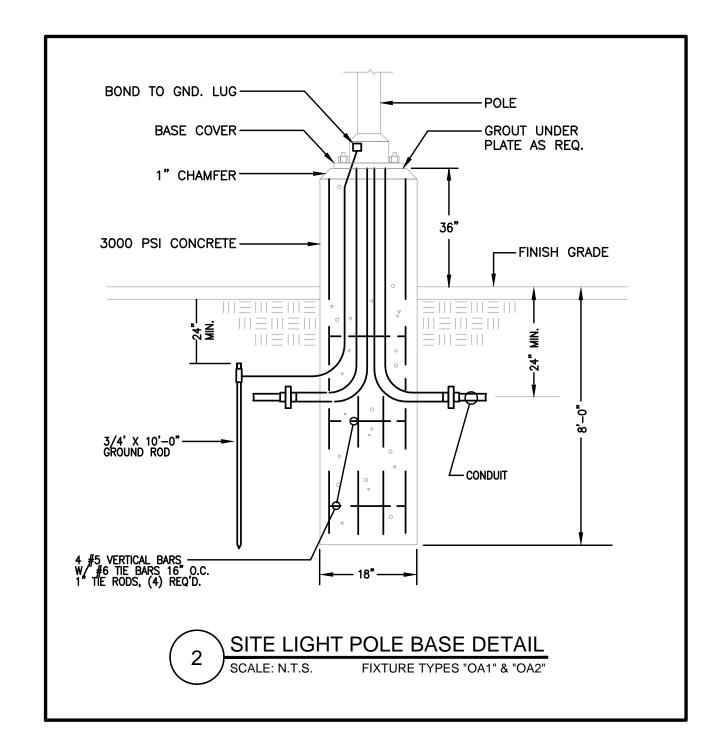
- 1. COORDINATE MOUNTING HEIGHT AND LOCATION WITH THE ARCHITECT PRIOR TO ROUGH-IN.
- 2. PROVIDE A 25'-0" SQUARE STEEL POLE, LITHONIA #SSS-25-4C-DM19AS-FINISH.
- 3. COORDINATE FINISH WITH ARCHITECT PRIOR TO ORDERING.

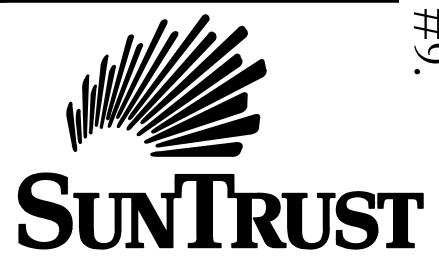
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# **ELECTRICAL GENERAL NOTES**

- DRAWINGS ARE DIAGRAMMATIC ONLY. EXACT LOCATIONS, MOUNTING HEIGHTS OF EQUIPMENT AND ROUTING OF RACEWAYS SHALL BE COORDINATED WITH THE EQUIPMENT REQUIREMENTS AND FIELD CONDITIONS. REFERENCE COMPLETE CONSTRUCTION DOCUMENTS (ARCHITECTURAL, MECHANICAL, PLUMBING, PATTERSON DENTAL EQUIPMENT, ETC.) PRIOR TO COMMENCING WORK FOR ADDITIONAL INFORMATION AND REQUIREMENTS. ANY DISCREPANCIES SHALL BE IMMEDIATELY BROUGHT TO THE OWNER'S/ARCHITECT'S ATTENTION BEFORE PROCEEDING WITH WORK.
- THE ELECTRICAL CONTRACTOR SHALL FURNISH AND INSTALL ALL INCIDENTAL ACCESSORIES NECESSARY TO MAKE THE ELECTRICAL WORK COMPLETE AND READY FOR OPERATION. CONTRACTOR SHALL PROVIDE CONNECTIONS TO OWNER, CONTRACTOR, OR OTHER PARTY'S EQUIPMENT AND DEVICES, UNLESS OTHERWISE NOTED. ON THE DAY OF DENTAL EQUIPMENT INSTALLATION, THE ELECTRICIAN MUST BE ON SITE TO MAKE FINAL CONNECTIONS WHERE NECESSARY.
- THE CONTRACTOR SHALL VISIT THE SITE OF THE PROPOSED PROJECT TO INSPECT THE EXISTING CONDITIONS AND DETERMINE THE SCOPE OF HIS WORK AND THE EXTENT OF DEMOLITION. THE SITE INSPECTION SHALL BE MADE PRIOR TO SUBMITTING BID FOR THE PROPOSED PROJECT. NO COMPENSATION WILL BE ALLOWED FOR FAILURE TO INSPECT THE SITE. CONTRACTOR SHALL INFORM ARCHITECT PRIOR TO BIDDING OF DISCREPANCIES WHICH EXIST BETWEEN DRAWINGS AND ACTUAL FIELD
- WIRE AND CONDUIT SIZE INDICATED ON HOMERUNS SHALL BE CONTINUOUS THROUGH CIRCUIT.
- 5. A GROUNDING CONDUCTOR SHALL BE INCLUDED IN EACH RACEWAY OR CABLE, SIZED IN ACCORDANCE WITH THE NATIONAL ELECTRICAL CODE.
- 6. TERMINATIONS (LUGS, TERMINAL BLOCKS, ETC.) IN CIRCUIT BREAKERS, DISCONNECT SWITCHES, LIGHTING CONTACTORS, RELAYS, PANEL BOARDS, TIME SWITCHES, ETC. SHALL BE RATED FOR 75C IN TEMPERATURE. IF TERMINATIONS IN EQUIPMENT SUCH AS EXHAUST FANS, WATER HEATERS, AIR CONDITIONING UNITS, ETC. ARE RATED FOR 60C ONLY, THEN CONDUCTORS MUST BE DE-RATED AND USED IN COMPLIANCE WITH TABLE 310-16 OF CURRENT N.E.C. AND SIZED FOR THE 60C COLUMN.
- BRANCH CIRCUIT CONDUCTORS SHALL NOT BE SMALLER THAN NO. 12 AND WHERE BRANCH CIRCUIT CONDUCTOR RUNS FROM SOURCE (PANEL) TO THE LAST DEVICE ON THE CIRCUIT EXCEEDS 100 FT. IN LENGTH, THE CONDUCTORS SHALL BE NO. 10 MINIMUM AND FOR THE ENTIRE LENGTH OF THE CIRCUIT. FOR RUNS OVER 200 FT. IN LENGTH THE CONDUCTOR SHALL BE NO. 8 MINIMUM AND FOR THE ENTIRE LENGTH OF THE CIRCUIT. THE ABOVE APPLIES TO 120 VOLT CIRCUITS. ONLY.
- 8. ALL ELECTRICAL WORK SHALL BE IN ACCORDANCE WITH OSHA, THE NATIONAL ELECTRICAL CODE AND LOCAL GOVERNING AUTHORITIES.







# ARCHITECTURE ■ PLANNING ■ INTERIORS

5887 GLENRIDGE DRIVE SUITE 375 ATLANTA, GEORGIA 30328 TEL: 404.233.8110 FAX: 404.233.7229

BOLDEN-WILLIAMS & ASSOCIATES, INC.
3066 HIGHWAY 29 SOUTH, LAWRENCEVILLE, GEORGIA 30044
(T) 770.279.0413 (F) 770.925.4263

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ELECTRICAL SITE PLAN



# CSX1 LED ED Area Luminaire





# **Specifications**

EPA:  $(0.07 \, m^2)$ 

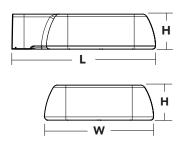
23-1/2" Length: (59.7 cm)

18-1/2" Width:

(46.9cm) 5-7/8"

Height: (14.9 cm) Weight 37 lbs

(max):



# Catalog Notes Туре

#### Introduction

The Contour® Series luminaires offer traditional square dayforms with softened edges for a versatile look that complements many applications.

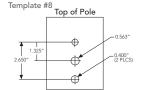
The CSX1 combines the latest in LED technology with the familiar aesthetic of the Contour® Series for stylish, high-performance illumination that lasts. It is ideal for replacing traditional metal halide in area lighting applications with typical energy savings of 65% and expected service life of over 100,000 hours.

# Ordering Information

### EXAMPLE: CSX1 LED 60C 1000 40K T3M MVOLT SPA DDBXD

CSX1 LED	60C									
Series	LEDs	Drive current	Color temperature	Distribution	Voltage	Mounting		Options	Finish (rea	quired)
CSX1 LED	60C 60 LEDs	700 700 mA 1000 1000 mA (1 A)	<b>40K</b> 4000 K <b>50K</b> 5000 K	T2M Type II T3M Type III T4M Type IV T5M Type V TFTM Forward throw	MVOLT 1 120 1 208 1 240 1 277 1 347 480	RPA Rc m WBA W Shipped Separatel SPUMBA Sc ur m ac RPUMBA Rc UIT m ac KMA8 DDBXD U M. m br.	quare pole nounting ound pole nounting Vall bracket ly 2 quare pole niversal nounting daptor 3 ound pole niversal nounting daptor 3 last arm nounting racket adapor (specify nish) 4	Shipped installed PER NEMA twist-lock receptacle only (n DCR Dimmable and controllable via RO controls) <sup>5</sup> DMG 0-10V dimming driver (no control HS House-side shield <sup>2</sup> SF Single fuse (120, 277, 347V) <sup>7</sup> DF Double fuse (208, 240, 480V) <sup>7</sup> DS Dual switching <sup>8,9</sup> 2ELED Emergency LED secondary source modules) battery pack (-20°C min temperature) BL30 Bi-level switched dimming, nomir BL50 Bi-level switched dimming, nomir Shipped separately <sup>2</sup> VG Vandal guard BS Bird-deterrant spikes	AM® (no DNAXD  DWHXD  DDBTXD  DBLBXD  DNATXD  2. 2. 2. 2. 2. 3. 30% 9.10  DWHGXE	Dark bronze Black Natural aluminum White Textured dark bronze Textured black Textured natural aluminum Textured white

Drilling



DLL127F 1.5 JU DLI 347F 1.5 CUL JU DLL480F 1.5 CUL JU SCII KMA8 DDBXD U

Photocell - SSL twist-lock (120-277V) 1 Photocell - SSI twist-lock (347V) 11 Photocell - SSL twist-lock (480V) 11 Shorting cap 11 Mast arm mounting bracket adaptor (specify finish) 4 Round and square pole universal mount-ing bracket adaptor (specify finish) PUMBA DDBXD U\* CSX1HS U House-side shield (includes 2 shields) CSX1VG U Vandal guard accessory

For more control options, visit DTL and ROAM online

Bird-deterrent spikes accessory

CSX1 shares a unique drilling pattern with the AERIS™ family. Specify this drilling pattern when specifying poles.

2 at 90° \* Single unit DM29AS DM28AS 2 at 180° DM39AS 3 at 90° \* DM49AS 4 at 90° DM32AS 3 at 120° \*\*

#### Example: SSA 20 4C DM19AS DDBXD

Visit Lithonia Lighting's POLES CENTRAL to see our wide selection of poles, accessories and educational tools.

\*Round pole top must be 3.25" O.D. minimum

## Tenon Mounting Slipfitter\*\*

Tenon O.D.	Single Unit	2 at 180°	2 at 90°	3 at 120°	3 at 90°	4 at 90°
2-3/8"	AST20-190	AST20-280	AST20-290	AST20-320	AST20-390	AST20-490
2-7/8"	AST25-190	AST25-280	AST25-290	AST25-320	AST25-390	AST25-490
4"	AST35-190	AST35-280	AST35-290	AST35-320	AST35-390	AST35-490

#### NOTES

- MVOLT driver operates on any line voltage from 120-277V (50/60 Hz). Specify 120, 208, 240 or 277 options only when ordering with fusing (SF, DF options).
- Also available as a separate accessory; see Accessories information at left.
- 1.5 G vibration load rating per ANCI C136.31.
- Requires "SPA" mounting option. Must be ordered as a separate accessory; see Accessories information. For use with 2-3/8" mast arm (not
- Specifies a ROAM® enabled luminaire with 0-10V dimming capability; PER option required. Not available with 347 or 480V, Add'l hardware and services required for ROAM® deployment; call 1-800-442-6745.
- Not available with 347 or 480V.
- Single fuse (SF) requires 120, 277 or 347 voltage option. Double fuse (DF) requires 208, 240 or 480 voltage option.
- Provides 50% dimming capability via two independent drivers, each operating half the luminaire. Available with MVOLT and two light engines only. N/A with PER, DCR, DMG or 2ELED.
- Requires an additional switched line.
- Dimming driver standard. MVOLT only. Not available with DCR.
- Requires luminaire to be specified with PER option. Ordered and shipped as a separate line item



CSX1BS U

## **Performance Data**

#### **Lumen Output**

Lumen values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts. Contact factory for performance data on any configurations not shown here.

	Drive Current (mA)	System Watts	Dist. Type	40K				50K					
				Lumens	В	U	G	LPW	Lumens	В	U	G	LPW
60C	700 mA	134	T2M	15,590	3	0	3	116	15,687	3	0	3	117
			T3M	16,502	3	0	3	123	16,605	3	0	3	124
			T4M	16,479	2	0	3	123	16,582	3	0	3	124
			T5M	16,539	4	0	2	123	16,643	4	0	2	124
			TFTM	16,710	2	0	3	125	16,814	2	0	3	125
	1000 mA	209	T2M	21,048	3	0	3	101	21,180	3	0	3	101
			T3M	22,279	3	0	3	107	22,418	3	0	4	107
			T4M	22,248	3	0	4	106	22,387	3	0	4	107
			T5M	22,330	5	0	3	107	22,469	5	0	3	108
			TFTM	22,560	3	0	3	108	22,701	3	0	4	109

## Lumen Ambient Temperature (LAT) Multipliers

Use these factors to determine relative lumen output for average ambient temperatures from 0-40  $^{\circ}$ C (32-104  $^{\circ}$ F).

Amb	Ambient				
0°C	32°F	1.02			
10°C	50°F	1.01 1.00			
20°C	68°F				
25°C	77°F	1.00			
30°C	86°F	1.00			
40°C	104°F	0.99			

## **Projected LED Lumen Maintenance**

Data references the extrapolated performance projections for the CSX1 LED 60C platform in a 25°C ambient, based on 10,000 hours of LED testing (tested per IESNA LM-80-08 and projected per IESNA TM-21-11).

To calculate LLF, use the lumen maintenance factor that corresponds to the desired number of operating hours below. For other lumen maintenance values, contact factory.

Operating Hours	0	25,000	50,000	100,000
Lumen Maintenance Factor	1.0	0.94	0.90	0.83

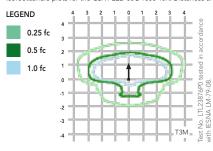
#### **Electrical Load**

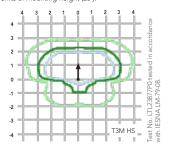
				Current (A)							
	mber LEDs	Drive Current (mA)	System Watts	120V	208V	240V	277V	347V	480V		
		700	134W	1.321	0.756	0.659	0.580	0.462	0.337		
60C	1000	209W	2.068	1.198	1.056	0.943	0.764	0.605			

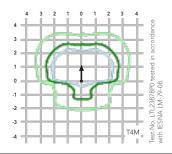
## **Photometric Diagrams**

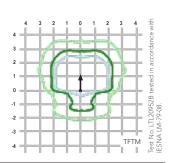
To see complete photometric reports or download .ies files for this product, visit Lithonia Lighting's CSX1 homepage.

Isofootcandle plots for the CSX1 LED 60C 1000 40K. Distances are in units of mounting height (20').









#### **FEATURES & SPECIFICATIONS**

#### INTENDED USE

The Contour Series LED area luminaire is ideal for streets, walkways, parking lots, and surrounding areas that call for high-performance LED lighting in a transitional dayform.

#### CONSTRUCTION

Single-piece die cast housing has a unique flow-through design that allows for optimized thermal management through convective cooling. A metallic screen covers the top of the housing, preventing debris build-up while allowing natural cleaning of the heat sinks. Modular design allows for ease of maintenance and future light engine upgrades. The LED driver and electronics are thermally isolated from the light engine(s), ensuring long life. Housing is completely sealed against moisture and environmental contaminants.

#### FINISH

Exterior parts are protected by a zinc-infused Super Durable TGIC thermoset powder coat finish that provides superior resistance to corrosion and weathering. A tightly controlled multi-stage process ensures a minimum 3 mils thickness for a finish that can withstand extreme climate changes without cracking or peeling.

#### OPTICS

Precision-molded acrylic lenses provide optimal luminaire spacing and improved uniformity. Lenses are indexed to the circuit board to ensure consistent optical alignment and delivering repeatable photometric performance. Light engines are available in standard 4000 K (70 CRI) or optional 5000 K (67 CRI) configurations. The CSX1 has zero uplight and qualifies as a Nighttime Friendly product, meaning it is consistent with the LEED® and Green Globes criteria for eliminating wasteful uplight.

#### ELECTRICAL

Light engines consist of 60 high-efficacy LEDs mounted to metal-core circuit boards to maximize heat dissipation and promote long life (100,000 hrs at  $40^{\circ}$ C, L70). Class 1 electronic driver

designed to have a power factor >90%, THD <20%, with an expected life of 100,000 hours with <1% failure rate. Easily-serviceable surge protection device meets a minimum Category C Low operation (per ANSI/IEEE C62.41.2).

#### INSTALLATION

Integral arm provides easy installation to a pole and assists in alignment and leveling. Secure connection withstands up to 3.0 G vibration load rating per ANSI C136.31. The CSX1 utilizes the AERISTM series pole drilling pattern for SPA and RPA options; wall mounting bracket also available. Available mast arm adapter accessory accepts horizontal tenons up to 2-3/8" O.D.

#### LISTINGS

CSA Certified to U.S. and Canadian standards. Light engines and luminaire are IP66 rated. U.S. Patent No. D632830. U.S. Patent No. D653,382 S.

DesignLights Consortium® (DLC) qualified product. Not all versions of this product may be DLC qualified. Please check the DLC Qualified Products List at <a href="https://www.designlights.org">www.designlights.org</a> to confirm which versions are qualified.

#### WARRANTY

Five year limited warranty. Full warranty terms located at www.acuitybrands.com/ CustomerResources/Terms and conditions.asox

**Note:** Actual performance may differ as a result of end-user environment and application. All values are design or typical values, measured under laboratory conditions at 25 °C. Specifications subject to change without notice.



O.C.G.A. § 7-8-3

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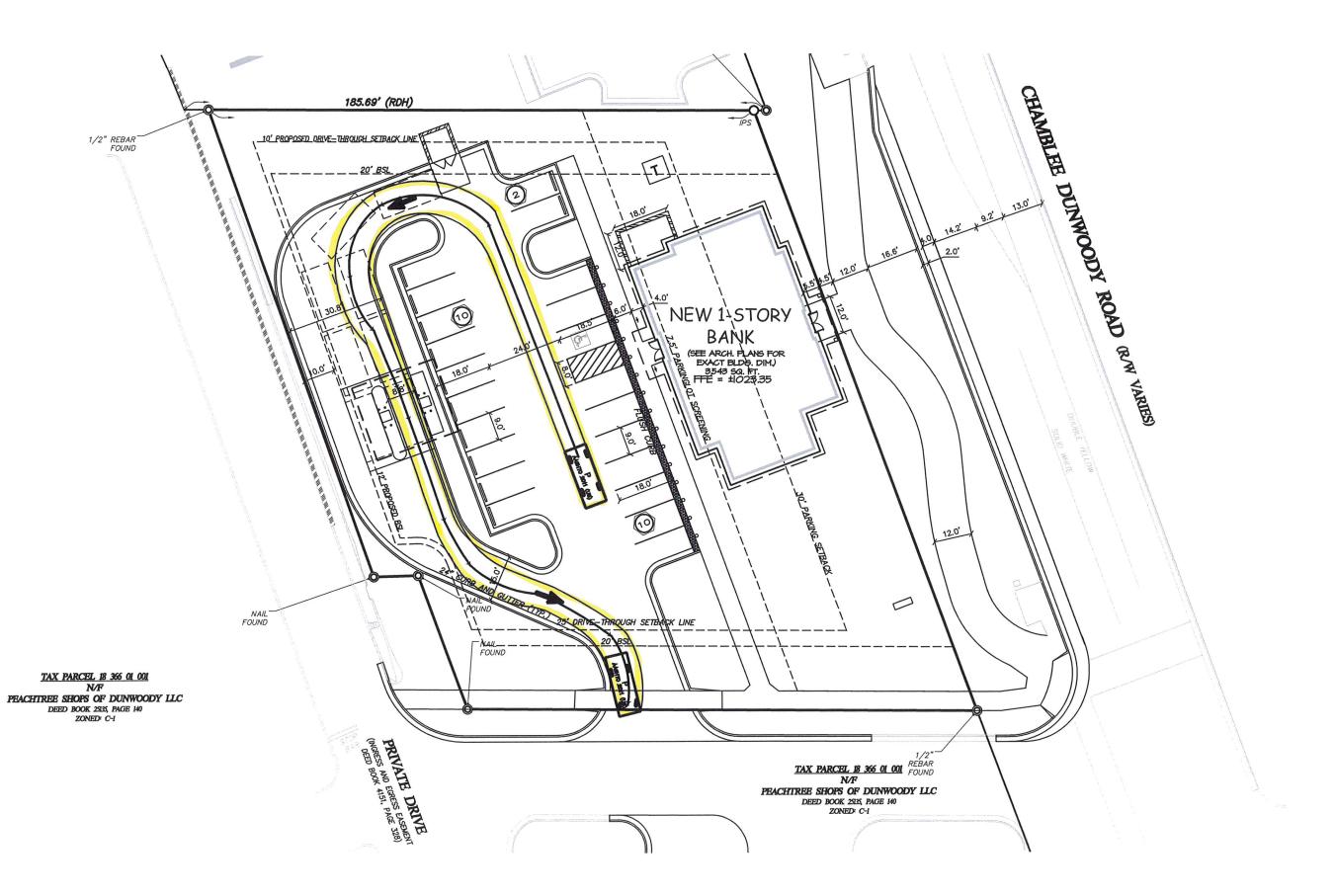
\*\*\* Current Through the 2016 Regular Session \*\*\*

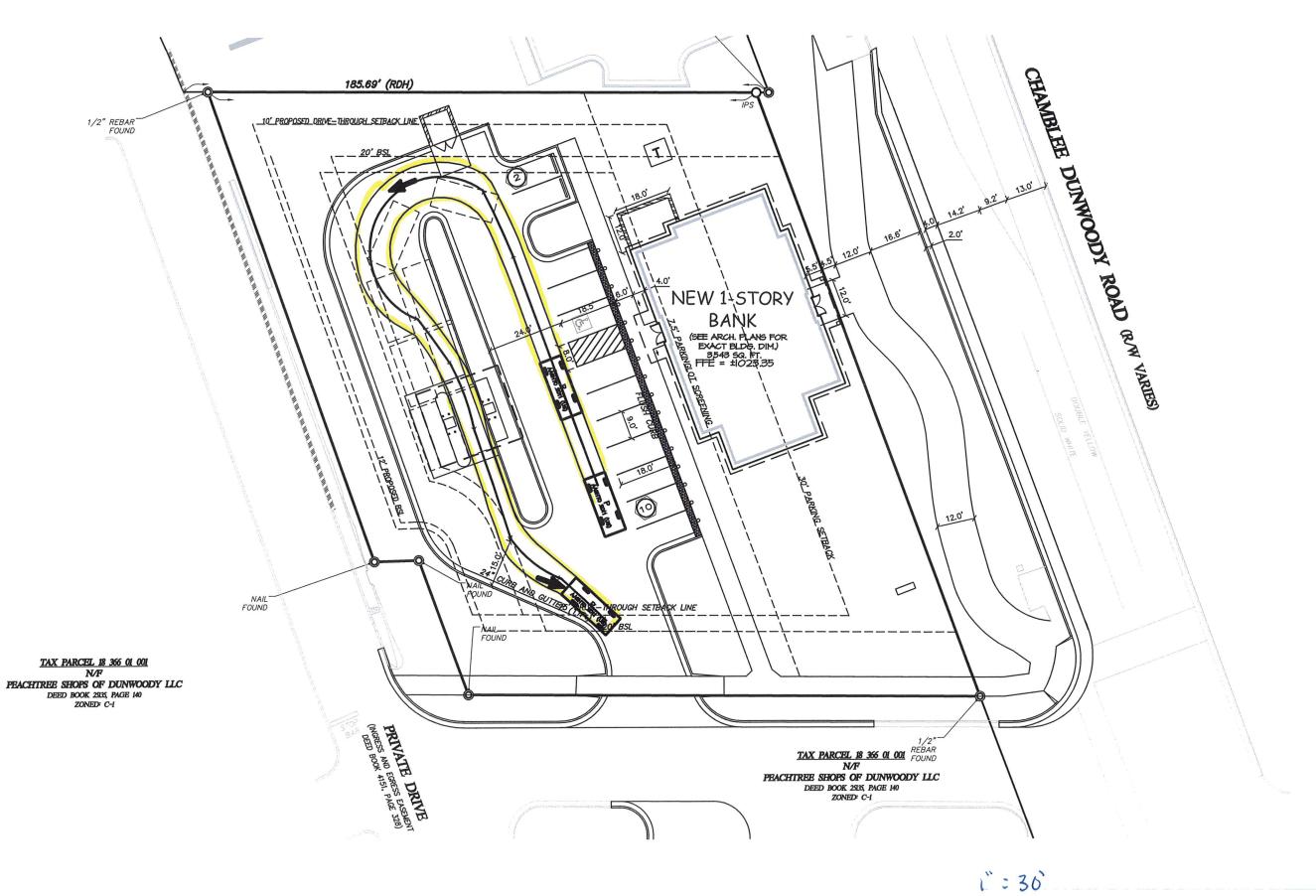
TITLE 7. BANKING AND FINANCE CHAPTER 8. SAFE USE OF REMOTE SERVICE TERMINALS

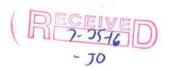
O.C.G.A. § 7-8-3 (2016)

- § 7-8-3. Date of compliance; provision of adequate lighting
- (a) Each operator of a remote service terminal installed on or after July 1, 1993, shall comply with the provisions of this chapter commencing on the date the remote service terminal is installed. Compliance with the provisions of this chapter by operators as to remote service terminals existing as of July 1, 1993, shall be optional until July 1, 1994, and mandatory thereafter. This Code section shall apply to an operator of a remote service terminal only to the extent that the operator controls the access area or defined parking area to be lighted.
- (b) If an access area or defined parking area is not controlled by the operator of the remote service terminal, and if the person who leased the remote service terminal site to the operator controls the access area or defined parking area, the person who controls the access area or defined parking area shall comply with the provisions of this chapter as to any remote service terminals installed on or after July 1, 1994, commencing on the date the remote service terminal is installed and as to any remote service terminal existing as of July 1, 1993, commencing no later than on July 1, 1994.
- (c) The operator, owner, or other person responsible for the remote service terminal shall provide lighting during the hours of darkness with respect to an open and operating remote service terminal and any defined parking area, access area, and the exterior of an enclosed remote service terminal installation according to the following standards:
- (1) There shall be a minimum of ten candlefoot power at the face of the remote service terminal and extending in an unobstructed direction outward five feet;
- (2) There shall be a minimum of two candlefoot power within 50 feet from all unobstructed directions from the face of the remote service terminal. In the event the remote service terminal is located within ten feet of the corner of the building and the remote service terminal is generally accessible from the adjacent side, there shall be a minimum of two candlefoot power along the first 40 unobstructed feet of the adjacent side of the building; and
- (3) There shall be a minimum of two candlefoot power in that portion of the defined parking area within 60 feet of the remote service terminal.

**HISTORY:** Code 1981, § 7-8-3, enacted by Ga. L. 1993, p. 917, § 12. Title Note Chapter Note







# FIRST AMENDED STATEMENT OF INTENT

and

Other Material Required by
City of Dunwoody Zoning Ordinance
For the
Special Land Use Permit Application
Application No. SLUP-16-091

of

SunTrust Dunwoody

for

±0.75 Acres of Land located in Land Lot 366, 18th District

5490 Chamblee Dunwoody Road Dunwoody, Georgia 30338

Submitted for Applicant by:

Dennis J. Webb, Jr.
Vickie B. Chung
Smith, Gambrell & Russell, L.L.P.
Promenade, Suite 3100
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Atlanta, Georgia 30309
404-815-3500

# I. INTRODUCTION

This Application seeks one Special Land Use Permit ("SLUP") in order to redevelop +0.75 acres of land located in Land Lot 366, 18th District of DeKalb County (the "Subject Property"), which is located in the Dunwoody Village Overlay District. The Applicant seeks to transform an old, vacant restaurant (formerly Hickory House) into a SunTrust Bank with an isolated drive-through, which will be a relocation of the current branch located at 1710 Mount Vernon Road. The existing site currently contains a ± 4,200 square foot vacant restaurant building, 57 parking spaces between the building and Chamblee Dunwoody Road, and an impervious coverage of approximately 94%. The Applicant will completely redevelop the site, adding a new 3,700 square foot building, rear drive-through with canopy, additional landscaped area, expanded sidewalk, bike lane and bicycle parking, and reducing impervious area and total parking, all of which will bring the Subject Property into greater compliance with the regulations and overall goals of the Dunwoody Village Overlay.

The proposed use is completely consistent with the underlying zoning and Comprehensive Land Use Plan. The Subject Property is zoned C-1, which allows banks with drive-through services as of right. The Comprehensive Plan designates the Subject Property as being located in the "Commercial" future land use category, which encompasses non-industrial business uses, such as banking. In addition to bringing a currently non-compliant site into greater compliance with the Dunwoody Village Overlay requirements, the proposed is expected to generate less traffic than other allowable C-1 uses. The SLUP requested in this Application is to increase the allowed parking in Section 27-97(g)(1) from 3 spaces per 1,000 square feet of floor area (11 total spaces for 3,700 square feet of floor area) to 22 total spaces.

<sup>&</sup>lt;sup>1</sup> Concurrently with this SLUP application, the Applicant requests three variances: (1) to reduce the 30 foot rear building setback; (2) to reduce the 25 foot side and rear yard drive-through setbacks, and; (3) to increase the maximum allowed footcandles at the property line and in parking areas.

This SLUP is requested because the Applicant, by constructing a new building, is required to fully comply with the Dunwoody Village Overlay requirements per Section 27-97(c)(1) of the Code.

## II. IMPACT ANALYSIS

The criteria considered by the City in granting a SLUP are listed in Article V, Division 3, Section 27-359 of the Code. The requested SLUP should be granted because all applicable criteria are met.

A. Whether the proposed use is consistent with the policies of the comprehensive plan.

The Subject Property is located within the Dunwoody Village Character Area of the Comprehensive Plan and within the Dunwoody Village Overlay under the Code. The City's Future Land Use Map designates the Subject Property as "Commercial." The Subject Property is not currently in compliance with the Overlay requirements, but the proposed redevelopment will bring the Subject Property closer to compliance and advance the goals of the Overlay. Furthermore, the proposed redevelopment is entirely consistent with these designations, which seek to provide a mix of office, retail and residential uses that use high quality building materials and adhere to the architectural standards of the district. In particular, the proposed redevelopment will further the following stated policies of the Comprehensive Plan:

- (a) Promoting redevelopments that abide by the architectural standards of Dunwoody Village Promoting, using "high quality building materials";
- (b) Promoting redevelopments "focused on pedestrian and bicycle amenities"; and
- (c) Promoting "a mix of office, retail and residential."

Furthermore, the proposed redevelopment is consistent with and will further the goals of the Dunwoody Village Master Plan, in particular:

- (a) Making the area "safer and more accommodating for pedestrians"; and
- (b) Encouraging "high-quality redevelopment of the underutilized areas"; and
- (c) Creating a "more walkable, vibrant Village Center with area appropriate goods and services."
- B. Whether the proposed use complies with the requirements of this zoning ordinance.

The Subject Property is zoned C-1. That zoning district allows for banks with drivethroughs as of right. With the exception of the three variances requested by the Applicant, the proposed development is wholly compliant with the underlying zoning.

The Applicant requests a SLUP to increase the allowed parking in Section 27-97(g)(1) from 3 spaces per 1,000 square feet of floor area (11 total spaces for 3,700 square feet of floor area) to 22 total spaces. Without this increase, the new bank would not have adequate parking on site. Although the City has progressively been moving towards being more walkable, the reality is that commercial uses need adequate parking in order to succeed. Synovus Bank of Georgia, located directly adjacent to the Subject Property, has 34 parking spaces, one and a half times what the Applicant is requesting. The Subject Property currently contains 57 parking spaces, all of which are located in between the existing building and Chamblee Dunwoody Road. The parking associated with the proposed development will all be located in the rear of the new bank building, in line with Dunwoody Village Overlay requirements.

C. Whether the proposed site provides adequate land area for the proposed use, including provision of all required open space, off-street parking and all other applicable requirements of the subject zoning district.

There is more than adequate land area to accommodate the proposed use. The Subject Property is + 0.75 acres. The existing impervious coverage on the Subject Property is 95%,

significantly higher than the 80% allowed under the C-1 zoning district. The proposed development will have an impervious coverage of 58.1%, bringing the site into compliance with C-1 regulations. The Applicant will comply with all open space requirements of the Code.

- D. Whether the proposed use is compatible with adjacent properties and land uses, including consideration of:
  - a. Whether the proposed use will create adverse impacts upon any adjoining land use by reason of noise, smoke, odor, dust or vibration generated by the proposed use;

No. The Subject Property is adjoined by other commercial uses and will not create any adverse impacts due to noise, smoke, odor, dust or vibration.

b. Whether the proposed use will create adverse impacts upon any adjoining land use by reason of the hours of operation of the proposed use;

No. Hours of use will mirror those of the existing businesses in the Dunwoody Village commercial core.

c. Whether the proposed use will create adverse impacts upon any adjoining land use by reason of the manner of operation of the proposed use;

No. Bank uses have existed near the Subject Property for many years with no adverse impact on the surrounding properties.

d. Whether the proposed use will create adverse impacts upon any adjoining land use by reason of the character of vehicles or the volume of traffic generated by the proposed use;

No. The proposed project will generate considerably less traffic than other allowable uses under the C-1 zoning district. The Institute of Traffic Engineers (ITE) Trip Generation

Manual (Ninth Edition) (Code 9120)(subspecialty Drive-in Bank) was used to calculate vehicle trips for this development. For a bank with 10 employees, an average of an average of 25 trips is anticipated for the morning weekday peak hour and an average of 47 trips is anticipated for the evening weekday peak hour. In contrast, a 3,700 square foot fast food restaurant with a drive-through window, which is also permitted as of right in C-1, would generate an average of 235 trips during the morning weekday peak hour and an average of 194 trips during the evening weekday peak hour.

e. Whether the size, scale and massing of proposed buildings are appropriate in relation to the size of the subject property and in relation to the size, scale and massing of adjacent and nearby lots and buildings;

Yes. The proposed building is appropriate in terms of its size, scale and massing compared to other buildings in the Dunwoody Village commercial core. It will comply with the Code's maximum height regulations and is well beneath the allowable square footage.

f. Whether the proposed plan will adversely affect historic buildings, sites, districts, or archaeological resources;

The Applicant knows of no historic buildings, sites, districts, or archaeological resources either on the Subject Property or located in the immediate vicinity that would suffer adverse impacts from the requested SLUP.

E. Whether public services, public facilities and utilities—including motorized and nonmotorized transportation facilities—are adequate to serve the proposed use.

The proposal for the site in question will have minimal effects on city streets, infrastructure or schools. As noted above, the expected traffic generation for the proposed use is much less than the anticipated traffic for other allowable uses in the C-1 district. There is

sufficient utility, water and sewer capacity to supply this site. The proposed redevelopment will have no effect on surrounding schools.

F. Whether adequate means of ingress and egress are proposed, with particular reference to non-motorized and motorized traffic safety and convenience, traffic flow and control and emergency vehicle access.

The proposed bank will be serviced by existing driveways on a private driveway off Chamblee Dunwoody Road. Ingress, egress and traffic flow for the proposed use will not a problem.

G. Whether adequate provision has been made for refuse and service areas.

Yes. See the attached site plan.

H. Whether the proposed building as a result of its proposed height will create a negative shadow impact on any adjoining lot or building.

No.

## III. PRESERVATION OF CONSTITUTIONAL RIGHTS

The Applicant respectfully submits that the Zoning Ordinance of the City of Dunwoody lacks adequate standards for the City Council to exercise its power to grant special land use permits. In essence, the standards are not sufficient to contain the discretion of the City Council and to provide the Courts with a reasonable basis for judicial review. Because the stated standards (individually and collectively) are too vague and uncertain to provide reasonable guidance to the City Council, the Zoning Ordinance violates the Fifth and Fourteenth Amendments of the Constitution of the United States in matters of zoning. The Zoning Resolution also violates Article I, Section III, Paragraph 1; and Article I, Paragraphs 1 and 2 of the Constitution of State of Georgia, 1983.

The City Council is granted the power to zone pursuant to Article IX, Section II, Paragraph 4 of the Constitution of the State of Georgia, 1983. It is a power which must be fairly exercised. Based on this element of fairness, the Zoning Ordinance of the City of Dunwoody violates Article IX, Section II, Paragraph 4 of the Constitution of the State of Georgia, 1983.

The Zoning Ordinance presently in effect is contrary to the best interest of the health and welfare of the citizens of the City of Dunwoody, and constitutes an arbitrary and capricious act. As a result, the Zoning Ordinance is in violation of Article I, Section I, Paragraphs 1 and 2 of the Constitution of the State of Georgia 1983; Article I, Section III, Paragraph 1 of the Constitution of the State of Georgia; and Article I, Section II, Paragraph 3 of the Constitution of the State of Georgia, 1983. Furthermore, the Zoning Ordinance violates the due process clause and equal protection clauses of the Fifth and Fourteenth Amendments to the Constitution of the United States of America.

In addition, the Zoning Ordinance presently in effect is unconstitutional in that it renders this property unusable and destroys its marketability. Therefore, the Zoning Ordinance constitutes a taking of applicant's property without just and adequate compensation and without due process of law in violation of the Fifth and Fourteenth Amendments to the United States Constitutional and in violation of Article I, Section I, Paragraph 1 and Article I, Section III, Paragraph 1(a) of the Constitution of Georgia.

Any limitation on the time for presentation of the issues before the City Council who have the power to zone and rezone is a violation of the guarantees of free speech under Article I, Section I, Paragraph 5 of the Constitution of the State of Georgia, 1983 and the First Amendment of the Constitution of the United States of America. Further, said limitations are in violation of the right to petition and assemble, in violation of Article I, Section I, Paragraph IX of the

#9.

Constitution of Georgia, 1983 and the First Amendment of the Constitution of the United States

of America as well as the due process clauses of the Constitution of Georgia, 1983 and the

Constitution of the United States of America.

The Zoning Ordinance of the City of Dunwoody is unlawful, null and void in that its

adoption and map adoption/maintenance did not comply with the requirements of its

predecessor ordinance and/or the Zoning Procedures Law, O.C.G.A. § 36-66-1, et seq.

IV. CONCLUSION

For the foregoing reasons, the Applicant respectfully requests that the SLUP at issue be

approved. The Applicant also invites and welcomes any comments from Staff or other officials

of the City so that such recommendations or input might be incorporated as conditions of

approval of this Application.

This 22 day of July, 2016.

Respectfully submitted,

SMITH, GAMBRELL & RUSSELL, L.L.P.

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